

MODIFICATION APPLICATION

Section 75W Modification
Major Project MP 07_0072 Concept Approval
Orange Private Hospital Campus
Lot 1 DP 549856, Forest Road, Orange



Prepared for
James Richmark Pty Ltd
June 2014

Ref: MA3PJB13033

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INTRODUCTION

1.1 OVERVIEW

This Concept Plan Amendment (CPA) is submitted on behalf of the new owners of the subject land, James Richmark Pty Ltd for approval under Part 3A of the Environmental Planning and Assessment Act 1979 (the EP&A Act).

The proposal relates to the Orange Private Hospital Campus approved on Lot 1 DP 549856, Forest Road, Orange. The CPA seeks to amend the Approved Concept as approved via Major Project MP 07_0072.

The Orange Private Hospital Campus Concept Plan seeks to create a non-institutional health precinct that includes facilities ranging from a traditional private hospital, to accommodation for patients, carers and staff, and complementary amenities to support these users. The Approved Concept includes 4 separate precincts:

1. Private hospital precinct
2. Medi-motel precinct
3. Residential precinct
4. Hostel precinct

Each of the components is intended to complement each other and the adjacent public hospital (Orange Health Service).

A modification of the Approved Concept is proposed in response to circumstances that have occurred since the original Concept Approval was granted. In this time, the public hospital has been commissioned; and an aged care and retirement facility has commenced construction on the site to the south of the subject land.

The new owners are of the view that the synergy of this developing health services precinct would benefit not only by increased private hospital and health facilities but also by an increase in retail and accommodation opportunities. Accordingly, the CPA retains the structure of the four development precincts identified in the Approved Concept Plan, but seeks to alter/update the Concept as follows:

- Provide additional floor space to accommodate more private hospital beds and other medical services/facilities.
- Increase floor space for health services and consulting in close proximity to the recently established Orange Health Service (public hospital) as well as the proposed private hospital.

- Increase retail floor space to enable the creation of a neighbourhood scale shopping complex that will be of benefit not only to the private hospital but also serve the nearby public hospital and the aged care and retirement facility.
- Increase residential accommodation opportunities in line with the proposed increase in health facilities to not only cater for the accommodation needs associated with the private hospital campus but also to cater for the accommodation needs associated with the public hospital campus (which was not in existence when the original Concept was approved).

The CPA continues the key elements of the approved Concept Plan by supporting the need for a mixed use development that forms a cohesive, harmonious and interrelated whole about a nucleus of health services.

Applications to modify the approved projects for Precincts 1 and 2 have been lodged under separate cover.

Separate Project Applications will be made for the detailed design of individual buildings and spaces within Precincts 3 and 4.

This application to amend approved Concept Plan MP07_0072 is made under Section 75W of the EP&A Act.

The key issues that require further consideration as a result of the CPA include:

- Comparison to Approved Concept Plan
- Environmental planning instruments
- Other planning provisions
- Traffic and parking
- Urban design
- Heritage
- Flora and fauna
- Social impact

1.2 APPLICANT

James Richmark Pty Ltd
PO Box 7765
Gold Coast Mail Centre QLD 9726

Correspondence to be forwarded to the applicant's consultant:

Peter Basha Planning & Development
PO Box 1827
Orange NSW 2800

1.1 PROJECT TEAM

The project team engaged for this CPA comprises the following:

Site Owner:	James Richmark Pty Ltd
Project Architect:	TVS Architects
Landscape Architect:	McGregor Coxall
Town Planner:	Peter Basha Planning & Development
Traffic Engineer:	Terraffic Pty Ltd

1.2 DOCUMENTATION

This report is accompanied by the following documentation.

Annexure A

Architectural Drawings by TVS Architects

Annexure B

Traffic and Parking Assessment by Terraffic Pty Ltd

Annexure C

Landscaping Plans by McGregor Coxall

Annexure D

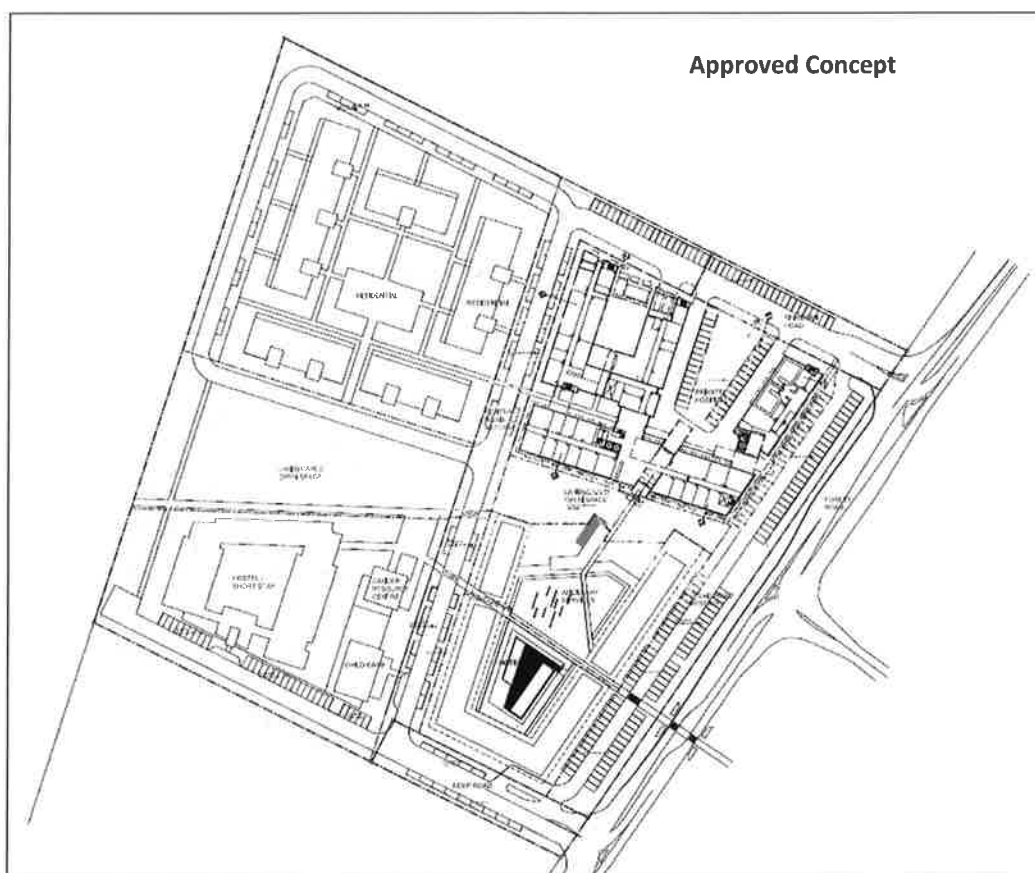
Confirmation of physical commencement

THE CONCEPT PLAN AMENDMENT

2.1 GENERAL

With reference to Part D in Schedule 1 of MP 07_0072, the Approved Concept includes 4 separate precincts:

1. Private hospital precinct
2. Medi-motel precinct
3. Residential precinct
4. Hostel precinct



The development includes:

- A part two/part three storey private hospital
- An 80 room medi motel and 9 consultation suites
- Short and medium term accommodation for hospital staff and students
- A future precinct for residential dwellings and or apartments

- A future precinct for a lodge/hostel for patients and families of patients
- Future child care facilities and cancer care facilities
- A gymnasium
- Ancillary retail activities (florist, newsagent and cafe restaurant)
- Internal roads and car parking
- Landscaping
- A four way signalised intersection

Project approval has been granted to Precincts 1 and 2 pursuant to MP08_0232 (Precinct 1 - Private Hospital); and MP08_0233 (Precinct 2 – Medi Motel & Ancillary Services).

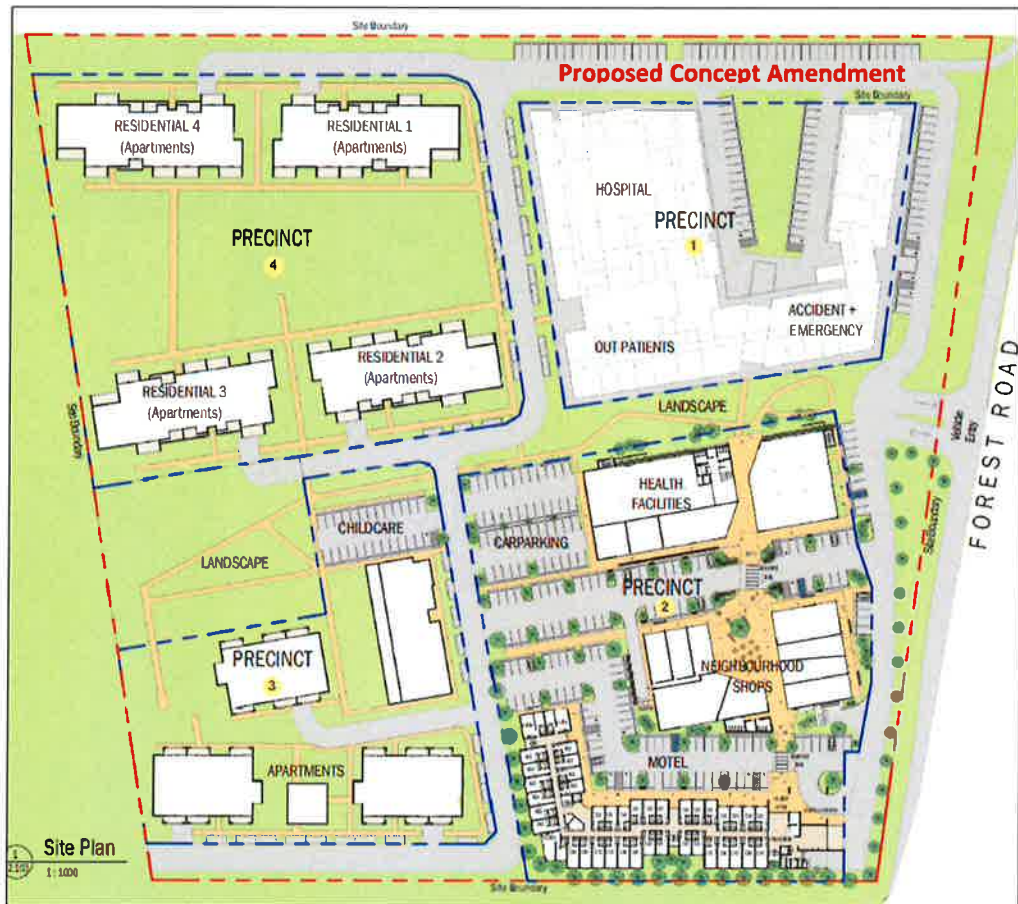
The Orange Private Hospital Campus approvals were due to lapse on 23 November 2013. However physical works in respect of MP 08_0233; MP 08_0232; and MP 07_0072 were physically commenced within the meaning of Section 95 of the EPA Act before the lapsing date (as confirmed in the letter from Orange City Council - refer Annexure D).

A modification of the Approved Concept is proposed in response to circumstances that have occurred since the original Concept Approval was granted. In this time, the public hospital has been commissioned; and an aged care and retirement facility has commenced construction on the site to the south of the subject land.

The new owners are of the view that the synergy of this developing health services precinct would benefit not only by increased private hospital and health facilities but also by an increase in retail and accommodation opportunities.

Accordingly, the CPA will:

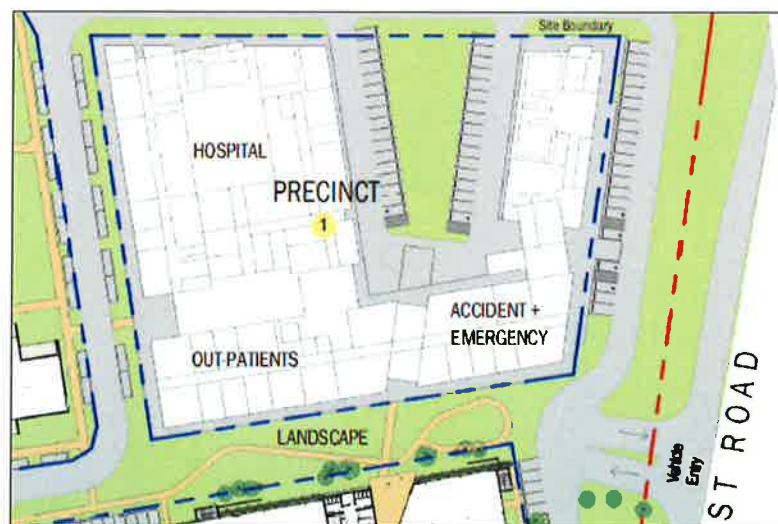
- Largely retain the structure of the four development precincts identified in the Approved Concept Plan, but seeks to alter/update the Approved Concept as explained in the following sections of this report.
- Continue the key elements of the approved Concept Plan by supporting the need for a mixed use development that forms a cohesive, harmonious and interrelated whole about a nucleus of health services.
- Articulate what James Richmark Pty Ltd is seeking to achieve for future development and sets the broad parameters for the development of the site.



2.2 SUMMARY OF PROPOSED CHANGES TO THE PRECINCTS

2.2.1 Precinct 1 – Private Hospital

The CPA seeks to increase the building height from a part 2 and 3 storey building to a part 3 and 4 storey building (plus lift overruns and plant). The intention is to increase floor area essentially over the original building footprint so that a larger number of private hospital beds and additional medical facilities can be provided.



The CPA also alters the parking layout to delete the row of spaces that are at the front of the site adjacent to Forest Road and provide a more generous landscaped strip.

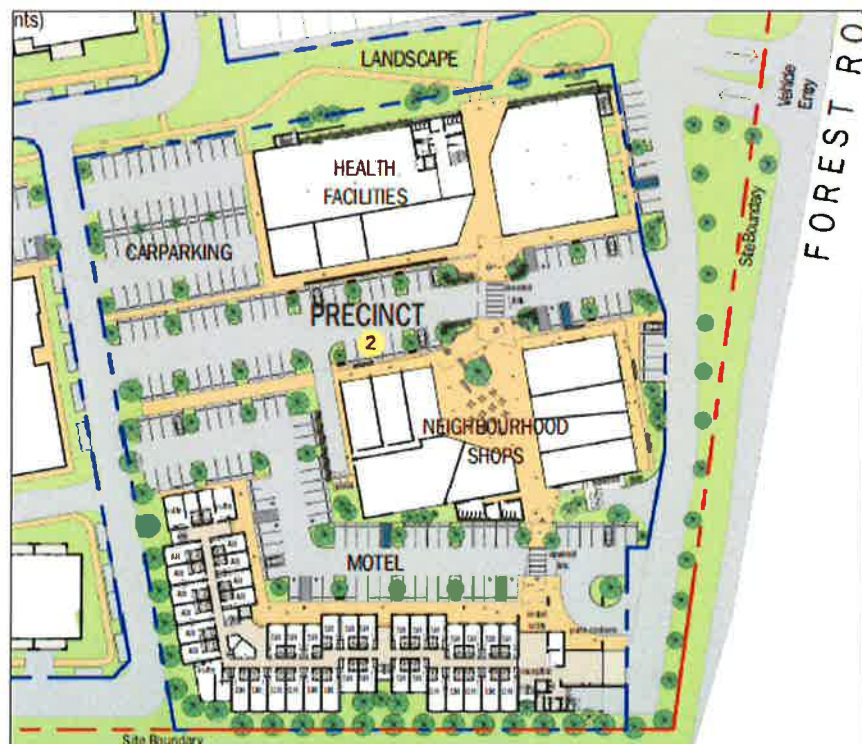
Proposed controls for Precinct 1:

- Maximum GFA of 14,000m²
- Building height – part 3 and 4 storeys (plus lift overruns, plant and the like)
- Preferred Use – hospital and associated medical facilities and services

2.2.2 Precinct 2 – Medi Motel, Health Facilities and Retail

The CPA seeks to reconfigure the building envelopes to provide:

- A separate building for the medi-motel which will maintain a similar number of accommodation rooms.
- A separate health facilities building which provides additional floor area for health related services.
- A separate building with increased floor area to provide for a broader range of ancillary retail and food and drink premises.



Proposed controls for Precinct 2:

- Maximum GFA of 10,000m²
- Preferred Use – Motel; health facilities including but not limited to rehabilitation, pathology, medical imaging, chemist, medical suites and the like; ancillary retail premises including but not limited to florist, cafe/restaurant, newsagent, hairdresser, shops, kiosks, take away food and drink premises and the like.

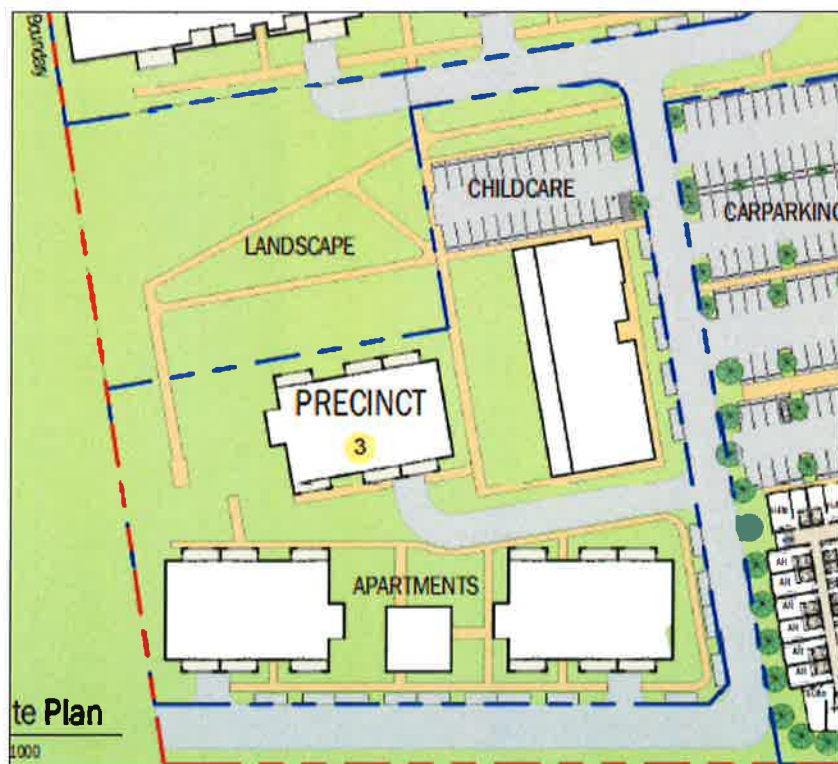
2.2.3 Precinct 3 – Childcare/Community and Residential

The Approved Concept for Precinct 3 provides for GFA of 4,500m² for hostel accommodation for patients/families; child care facilities; and a cancer centre.

The CPA alters the envisaged land uses to only include residential development and community/childcare facilities. GFA increases to 7,500m².

The intent is to provide:

- Childcare/community facilities that synergise with the broader medical precinct.
- A range of residential styles that can either be used as short term or long term accommodation by patients, carers, family members, medical employees and the like in a non-institutional environment.



The hostel is proposed to be deleted as this function would be fulfilled by:

- The motel component in Precinct 2. The owners consider that a hostel would cause an oversupply of short term accommodation and thus compete with the function and viability of the motel.
- The proposed increase in residential opportunities that are proposed for this precinct and Precinct 4. The aim is to encourage a range of residential accommodation styles that are considered less institutional than a typical hostel environment.

The cancer care centre would be removed from this precinct on the basis that the additional GFA proposed for health facilities in Precincts 1 and 2 provides ample space and resources to take up this function.

The proposed increase in GFA is justified on the grounds that:

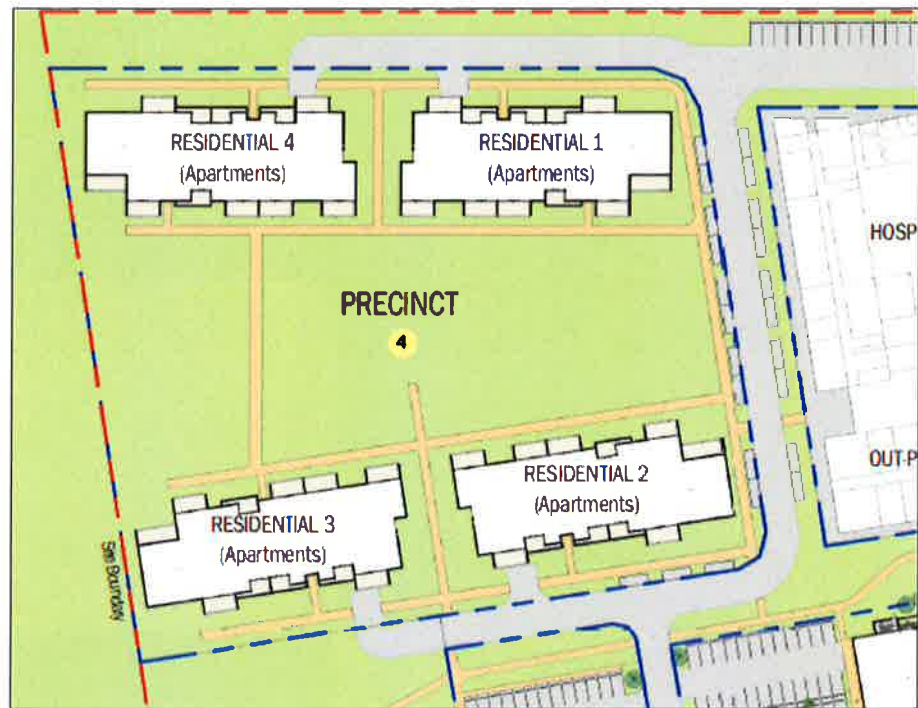
- The proposed GFA is not unrealistic or likely to lead to an overdevelopment of the precinct. In this regard:
 - Landscape and open space areas will remain generous.
 - A community/childcare facility may require GFA in the order of 1,000 to 1,500m² depending on use or the proposed number of childcare places.
 - Dwelling yield is subject to proposed dwelling size but a GFA of 6,000m² would enable say 60 x 2 bedroom units (each with an average GFA of 100m²). If larger dwellings are proposed then yield would reduce; and if smaller dwellings are proposed, then yield would increase.
 - The increase in the residential component in this precinct (and Precinct 4) moves in line with the increase in hospital and health facilities, associated not only with the proposed private hospital but also with the public hospital and the aged care facility which were not in existence when the original Concept was approved. It is expected that the broader health services precinct will benefit from the increase offering of residential accommodation opportunities that will eventuate as a result of the CPA.

Proposed controls for Precinct 3:

- Maximum GFA of 7,500m²
- Preferred Use – community/childcare facilities; residential.

2.2.4 Precinct 4 – Residential

The Approved Concept for Precinct 4 provides for GFA of 10,800m² for residential development. The original concept envisaged 114 apartments; or 31 town houses and 36 apartments; or 105 seniors living units (all subject to future project applications).



The CPA increases GFA to 17,000m². The proposed increase is justified on the grounds that:

- It provides for a broad range of dwelling styles that may include 1, 2, or 3 bedroom apartments that offer residential accommodation for health precinct staff; patients and their families; and downsizing or elderly people who seek a non-institutionalised style of living with the convenience of nearby health and medical services.
- The proposed GFA is not unrealistic or likely to lead to an overdevelopment of the precinct. In this regard:
 - Landscape and open space areas will remain generous.
 - Dwelling yield is subject to proposed dwelling size but a GFA of 6,000m² would enable say 170 x 2 bedroom units (each with an average GFA of 100m²). If larger dwellings are proposed then yield would reduce; and if smaller dwellings are proposed, then yield would increase.

- The increase in the residential component in this precinct (and Precinct 3) moves in line with the increase in hospital and health facilities, associated not only with the proposed private hospital but also with the public hospital and the aged care facility which were not in existence when the original Concept was approved. It is expected that the broader health services precinct will benefit from the increase offering of residential accommodation opportunities that will eventuate as a result of the CPA.

Proposed controls for Precinct 4:

- Maximum GFA of 17,000m²
- Preferred Use – residential

2.3 URBAN STRUCTURE

The CPA retains the structure of the four development precincts identified in the Approved Concept Plan and does not significantly alter the urban structure of the campus.

The network of publicly-accessible streets and open spaces is retained. The open spaces are defined by the streets and the non-institutional buildings which will contribute to a high degree of surveillance and public safety for patients, staff, visitors and carers who move around the site.

Significant areas of the site remain dedicated to open space, in the form of pedestrian walkways and areas for quiet relaxation or more active pursuits.

The on-grade car parking layout has been altered and parking space numbers increased as required. Parking will be distributed throughout the site to meet demand and will be integrated into the landscaping.

2.4 BUILT FORM

The CPA acknowledges that the built form for the campus must respond to the heritage significance of the Bloomfield Hospital site, on the opposite side of Forest Road. Accordingly it retains the generous setbacks and landscaped areas along the site frontage and between the proposed buildings. Whilst a minor increase in the height of the private hospital building is proposed, the buildings along this edge of the site will retain a reasonable scale and have a non-institutional character.

The CPA retains a strong sense of address to Forest Road, while delivering a health precinct with a clear central focus and gathering space.

The neighbourhood shops complex is positioned centrally to the private hospital, health facilities and medi-motel with strong pedestrian links. The complex is also easily accessible to the planned residential precincts in the western half of the campus. As such, a central focus and gathering space will be created, much like the “village common” that was envisaged in the Approved Concept. The expanded retail component will provide for a greater range of uses and services to complement the broader precinct.

The architect for the CPA has sought to maintain the architectural character developed for the original approvals, being modern and contemporary, which also compliments the architectural character of the public hospital constructed since the original approvals. The materials used and the articulation of the building facades are intended to relate to the human scale and deliver a rich pallet of colours and textures.

The CPA envisages that Precincts 3 and 4 will involve taller building forms. The CPA sets these buildings towards the rear of the site and provides for generous landscaped areas and open space in between. These attributes will underpin detailed planning and design of future buildings within these precincts. Future building forms and potential visual impacts will be assessed in greater detail as part of future applications for development within Precincts 3 and 4.

2.5 LAND USE

The CPA retains the four development precincts identified in the Approved Concept Plan and provides sufficient certainty in the location of each of the precincts. The CPA also provides for some flexibility to alter the layout of the precincts as opportunities arise or circumstances vary.

The CPA largely maintains the same land uses as the Approved Concept but does seek to increase residential and retail opportunities in line with the proposed increase in floor area for the private hospital and health facilities.

The most notable change to land use occurs in Precinct 3 where the CPA removes the hostel/cancer care component based on the following rationale:

- In the context of the motel component in Precinct 2, the owners consider that a hostel would cause an oversupply of short term accommodation and thus compete with the function and viability of the motel.
- The proposed increase in residential opportunities that are proposed for Precincts 3 and 4 aim to encourage a range of residential accommodation styles that are considered less institutional than a hostel environment.
- The cancer care centre would be removed from this precinct on the basis that the additional GFA proposed for health facilities in Precincts 1 and 2 provides ample space and resources to take up this function.

2.6 OPEN SPACE AND PUBLIC DOMAIN

The CPA includes a revised landscape concept plan. For continuity and to uphold the original design philosophy, the landscape architects that were engaged on the original concept have been retained.

The revised landscape concept maintains the original focus to create a non-institutional hospital precinct in a landscaped environment that responds to the heritage listed Bloomfield Hospital, located opposite the site on Forest Road. The landscape concept plan for the campus by McGregor Coxall is included in Annexure C.

Notwithstanding the changes brought about by the CPA, the original design philosophy for the landscaping of the campus is upheld. It encompasses ecologically sensitive design through water management and species and material selection across the entire campus. The landscape design seeks to create a place that encourages and enhances health, recuperation and healing and facilitates positive sensory experience, for the diverse users of the site.

In terms of parks and gardens, the revised concept maintains the approach of providing specific landscape zones to relate to the buildings and land use components within the campus. Due to certain design changes under the CPA, some landscape zones have been removed, replaced or reconfigured but generally the amount of landscaping and open remains generous, accessible and useable.

In regard to landscaping of vehicle areas, the revised landscape plan retains the key elements of the original concept, including:

- Landscaping along the Forest Road frontage to establish a harmonious interface with the heritage landscape associated with Bloomfield Hospital.
- Screening of the parking areas along the Forest Road frontage and within the campus.

The principles of water sensitive urban design have been applied across the site, in particular in the management of stormwater runoff from streets and hard surface areas. Landscape bioswales are strategically located to filter and retain as much water as possible within the site, while also collecting runoff building roof areas.

The revised landscape concept continues the original element of pedestrian links throughout the site that encourage patients, staff and carers to explore the numerous landscaped areas for quiet reflection. The majority of links will be wheelchair accessible and covered walkways and bridges between buildings have been included.

2.7 ACCESS AND TRANSPORT

The currently approved access arrangements that serve the site off Forest Road will be retained and comprise the following:

- A new 4-way signalised intersection on Forest Road that will connect the site to the main access driveway serving the Orange Base Hospital which is located on the eastern side of Forest Road. The new signals will include pedestrian crossings on each leg of the intersection to enhance pedestrian safety.
- Left turn exit only movements from a northern access driveway.

The internal road network will essentially frame the four precincts. The road layout encourages a low vehicle speed environment and signage will further assist in this regard. The road network is designed to accommodate the turn paths of the largest service vehicles likely to be associated with the campus. Dedicated loading areas will be provided for the private hospital, medi-motel and neighbourhood shops complex.

A total of 617 parking spaces will be provided across the site. This number of parking spaces is less than would be required with direct reference to the Orange City Council *Development Control Plan 2004 – 15 Car Parking*. However, the parking assessment undertaken by Terraffic Pty Ltd (refer Annexure B) demonstrates that the parking numbers required by the DCP can be reduced based on arguments pertaining to dual and complimentary uses. The Terraffic report considers this issue in detail and concludes that the proposed parking provision is adequate and will not generate unacceptable parking implications.

The CPA removes internal bus zones. The development site is already served by Route 537 whereby buses currently operate on a daily basis between Orange City Centre and the main entrance to the Orange Health Service (public hospital). The development site is within easy walking distance of that bus stop and pedestrian access will be facilitated by the safe crossings provided in the proposed four-way signalised intersection.

Pedestrian amenity is provided for via the interconnecting path network throughout the campus. The proposed four way signalised intersection will provide for pedestrian connections between the campus and the public hospital (final path locations will be determined once the intersection has been constructed).

Bike access will be assisted by the existing bike and pedestrian path that extends along the eastern side of Forest Road.

2.8 SERVICES AND INFRASTRUCTURE

The CPA does not significantly increase the level of servicing or infrastructure that was contemplated under the Approved Concept.

Since the original Concept was approved, services and infrastructure have been augmented in the area due to the now developed public hospital and the developing aged care and retirement facility.

2.9 INDICATIVE STAGING

The approved concept indicates that the Precinct 1 (private hospital) and Precinct 2 (medi-motel, ancillary services and retail development) would be developed first.

This CPA clarifies that the medi-motel (Precinct 2) is intended to be developed first with the private hospital (Precinct 1) to follow.

Precincts 1 and 2 have the benefit of separate project approvals which are proposed to be modified in line with the CPA. These modification applications have been lodged under separate cover. Separate Project Applications will be made for the detailed design of individual buildings and spaces within Precincts 3 and 4.

Staging is indicative only and may alter subject to a variety of factors and circumstances beyond the control of the proponents.

2.1 PREFERRED PROJECT AND STATEMENT OF COMMITMENTS

The CPA varies the Preferred Project as follows:

- The hospital building height increases from 3 storeys to 4 storeys. The intention was to maintain views to the west to Mount Canobolas and the western hinterland of Orange. However, the revised layout of buildings in Precinct 2 opens up view corridors that are considered more effective than what would be achieved by limiting building height of the hospital alone.
- The Precinct 2 buildings do not maintain a 24 metre setback from Forest Road along the entire boundary. The setback ranges from 12 metres to 35 metres. The variation is justified on the following grounds:
 - Compared to the current approval for Precinct 2, the proposed modification has the potential to improve the eventual streetscape. The current approval provides for an elongated and largely unbroken building form more or less parallel to Forest Road.

- The proposed modification avoids this by breaking the buildings into separate complexes and providing space and landscaping between them. Further, the proposed buildings are set at non-parallel angles to improve articulation.
- The proposed modification removes the expansive car park that dominates the frontage of the site under the current approval and thus increases the opportunity for landscaping in this area.
- The 24 metres setback was motivated by heritage considerations for the Bloomfield Hospital Campus. It should also be noted that the new public hospital which has been constructed in the Bloomfield Hospital campus is a modern and bulky building that now characterises the precinct. This building and its external support areas diminish the visual relationship between the heritage item and the subject land.
- The CPA does not propose an outdoor theatre as homage to the former use of the site as a drive-in movie theatre. The revised site layout does not lend itself to the inclusion of this element. A photographic record of the site has been completed pursuant to approval conditions in MP 08_0232 and MP 08_0233 and is considered sufficient recognition of the former use.
- The CPA alters the land use mix as explained and justified throughout this report.

There are no aspects of the CPA that conflict with the Statement of Commitments.

2.2 AMENDMENTS TO APPROVAL NOTICE AND CONDITIONS

Should the CPA be approved, the approval notice and conditions will require amendments as suggested below (additions are shown in red and deletions are struck through).

Amend Condition D1 Development Description to read to the following effect:

Concept approval is granted only to carrying out the development solely within the concept plan area as described in the Environmental Assessment Report titled “Orange Private Hospital” prepared by JBA Urban Planning Consultants and dated February 2008 and the Preferred Project Report prepared by JBA Planning Consultants dated August 2008 ~~(as amended)~~ **as amended by the Section 75W Modification of MP 07_0072**. The concept plan includes 4 separate precincts:

1. Private hospital precinct
2. Medi-motel **health facilities and retail** precinct
3. ~~Residential~~ **Community/childcare and residential** precinct
4. ~~Hostel~~ **Residential** precinct

The development includes:

- A part ~~two~~ **three**/part ~~three~~ **four**-storey private hospital
- An ~~80~~ **82** room Medi-Motel and ~~9 consultation suites~~
- **A separate health facilities building for rehabilitation, pathology, medical imaging, chemist, medical suites and the like**
- ~~Short and medium term accommodation for hospital staff and students~~
- Future precincts for residential dwellings and or apartments **incorporating short and medium-term accommodation for hospital staff and students**
- ~~A future precinct for a lodge/hostel for patients and families of patients~~
- Future **community**/child-care facilities and ~~cancer care facilities~~
- ~~A gymnasium~~
- **A separate building for ancillary retail activities (including but not necessarily limited to florist, newsagent and cafe/restaurant hairdresser, shops, kiosks, take away food and drink premises)**
- Internal roads and car parking, and
- Landscaping
- A four way signalised intersection on Forest Road

Amend Condition D2 Development in Accordance with Documentation to read to the following effect:

- 1) The following documentation (including any appendices therein) are approved as part of the concept plan:
 - a) Orange Private Hospital Environmental Assessment Report, prepared by JBA Planning Consultants on behalf of Forest Road Syndicate dated February 2008 **and as amended by the Section 75W Modification of MP 07_0072.**
- 2) The following Preferred Project Report including a revised Statement of Commitments is approved
 - a) Preferred Project and Statement of Commitments, prepared by JBA Planning Consultants on behalf of Forest Road Syndicate dated July 2008 **and as amended by the Section 75W Modification of MP 07_0072.**

3) Additional correspondence listed below:

- a) Letter regarding Orange Private Hospital from JBA Planning Consultants dated 25 July 2008.
- b) Letter regarding Orange Private Hospital from JBA Planning Consultants dated 4 August 2008.
- c) Email enclosing an amended Preferred Project and Statement of Commitments from JBA Planning Consultants dated 19 August 2008.
- d) Email enclosing an amended Preferred Project and Statement of Commitments from JBA Planning Consultants dated 26 September 2008.
- e) Email from JBA Planning Consultants dated 29 September 2008 confirming amendment to Statement of Commitments.

And as amended by the Section 75W Modification of MP 07_0072.

Amend Condition D3 Development in Accordance with Plans to read to the following effect:

- 1) The development will be undertaken in accordance with the following plans:

Drawing Number	Name of Plan	Revision	Date
DA-01	Site Analysis and Local Context Plan		03/12/2007
DA-02	Site Concept Plan		25/06/2008
DA-04	Hotel Precinct Ground Floor Plan		25/06/2008
DA-05	Hotel Precinct First Floor Plan		25/06/2008
DA-06	Hotel Precinct Roof Plan		25/06/2008
DA-07	Western and Eastern Elevations		25/06/2008
DA-08	Northern and Southern Elevations		25/06/2008
DA-09	Inner Eastern and Western Elevations/Section		25/06/2008
SK-01	Site Concept Plan		30/06/2008
DA-02	Site Concept Plan		30/06/2008
DA-03	Private hospital/Site Plan		30/06/2008
Drawing Number	Name of Plan	Revision	Date
DA-04	Private Hospital Precinct Level 1 Plan		30/06/2008
DA-05	Private Hospital Precinct Level 2 Plan		30/06/2008
DA-06	Private Hospital Precinct Level 3 Plan		30/06/2008
DA-07	Private Hospital Precinct Northern and Eastern Elevations		23/11/2007

DA-08	Private Hospital Precinct-Southern and Western Elevations		23/11/2007
DA-09	Private Hospital Precinct Section AA and BB		23/11/2007
DA-10	Private Hospital Precinct Section CC and DD		23/11/2007
	Landscape Plan Hospital Precinct 01	F	June 2008
	Landscape Plan Medi Motel 01	G	June 2008
	Section/Elevation Images 02-Hospital Precinct	E	June 2008
	Section/Elevation Medi Motel Precinct	E	June 2008
5420.3S.01.101	Site Master Concept Plan	13	16.04.14
5420.3S.01.102	Precinct 2-Site Plan	8	16.04.14
5420.3S.01.103	Precinct 2-Elevations and Sections	3	18.03.14
5420.3S.02.101	Health Facilities-Ground Floor	4	18.03.14
5420.3S.02.102	Health Facilities-First Floor	3	18.03.14
5420.3S.02.103	Health Facilities-Elevations and Sections	5	18.03.14
5420.3S.02.104	Health Facilities-Persepctives	1	18.03.14
5420.3S.02.201	Neighbourhood Shops-Ground Floor	5	18.03.14
5420.3S.02.202	Neighbourhood Shops-Sections & Elevations	4	18.03.14
5420.3S.02.203	Neighbourhood Shops-Perspectives	1	18.03.14
5420.3S.02.301	Motel-Ground Floor	5	18.03.14
5420.3S.02.302	Motel-First Floor	4	27.02.14
5420.3S.02.303	Motel-Elevations	3	18.03.14
5420.3S.02.304	Motel-Elevations and Section	3	18.03.14
5420.3S.02.305	Motel-Perspectives	1	18.03.14
5420.3S.06.001	GFA Ground Level	5	18.03.14
5420.3S.06.002	GFA Level 1	4	18.03.14
5420.3S.01.104	Private Hospital Precinct – Site Plan	2	16.04.14
5420.3S.02.401	Private Hospital Precinct – Level 1	1	18.03.14
5420.3S.02.402	Private Hospital Precinct – Level 2	1	18.03.14
5420.3S.02.403	Private Hospital Precinct – Level 3	1	18.03.14
5420.3S.02.404	Private Hospital Precinct – Roof Plan	1	18.03.14
5420.02.405	Private Hospital Precinct – Elevations	1	18.03.14
5420.02.406	Private Hospital Precinct – Elevations	1	18.03.14
5420.02.407	Private Hospital Precinct – Sections	1	18.03.14
5420.02.408	Private Hospital Precinct – Sections	1	18.03.14
	Landscape General Concept Plan 00	C	
	Landscape Masterplan 01	E	
	Landscape Section and Planting List 02	D	

Amend Condition E1 Car Parking in Medi-Motel Precinct read to the following effect:

A minimum of ~~134~~ **188** at grade car parking spaces for use by the public shall be provided for the Medi-Motel Precinct. ~~The precinct boundary and internal circulation driveway shall be relocated 3 metres to the west and the parallel parking spaces along the eastern side of the central road shall be replaced with 90 degree car parking spaces. Additional spaces may be provided along the southern boundary of the site in the redesign and relocation of the bus/loading bay.~~

Amend Condition E3 Traffic and Access to read to the following effect:

E3.1 Vehicle access to the site from Forest Road shall not be provided via Finneran and Kemp Road.

- E3.2 A four-way signalised intersection is to be provided in Forest Road to serve the development. The final design of the intersection, including any road widening, shall be approved by Council and the RTA.
- E3.3 Street lighting is to be provided at the four-way signalised intersection in accordance with RTA and Country Energy requirements.
- ~~E3.4 All bus movements within the development are to be in an anti-clockwise direction and appropriate signage is to be installed in accordance with the Australian Standard.~~
- ~~E3.5 A bus zone and loading bay is to be provided on the eastern side of the Central Road Reserve between the Private Hospital Precinct and the Medi-Motel Precinct.~~
- ~~E3.6 The bus bay to the south of the Medi-Motel is to be relocated to the north of the circulating roadway and the car spaces relocated to the area shown as the bus bay.~~
- E3.6 All costs associated with the traffic and access works outlined above in E3 to E6 shall be met by the proponent.

Amend Condition F2 Maximum Gross Floor Area to read to the following effect:

F2 Maximum Gross Floor Area Site Coverage Control

Future applications for Precincts 3 and 4 shall not exceed the following maximum gross floor areas

Precinct 3	Hostel/short stay accommodation	4,500m ²
	Community/childcare and residential	7,500m ²
Precinct 4	Residential	10,800m ²
		17,000m ²

Gross floor area must be measured in accordance with the definition within the standard instrument (local environmental plans) order 2006. Site coverage of all buildings shall not exceed 50%

ASSESSMENT OF THE PROPOSED MODIFICATION

This section provides an assessment of the proposed modification against the relevant environmental planning instruments and policies and the likely environmental impacts.

3.1 SECTION 75W MODIFICATION

Section 75W of the Environmental Planning & Assessment Act (the Act) provides as follows:

75W Modification of Minister's approval

1) In this section:

"Minister's approval" means an approval to carry out a project under this Part, and includes an approval of a concept plan.

"modification of approval" means changing the terms of a Minister's approval including:

- a) revoking or varying a condition of the approval or imposing an additional condition of the approval, and*
- b) changing the terms of any determination made by the Minister under Division 3 in connection with the approval.*

2) The proponent may request the Minister to modify the Minister's approval for a project. The Minister's approval for a modification is not required if the project as modified will be consistent with the existing approval under this Part.

3) The request for the Minister's approval is to be lodged with the Director-General. The Director-General may notify the proponent of environmental assessment requirements with respect to the proposed modifications that the proponent must comply with before the matter will be considered by the Minister.

4) The Minister may modify the approval (with or without conditions) or disapprove of the modification.

5) The proponent of a project to which section 75K applies who is dissatisfied with the determination of a request under this section with respect to the project (or with the failure of the Minister to determine the request within 40 days after it is made) may, within the time prescribed by the regulations, appeal to the Court. The Court may determine any such appeal".

The applicant requests that the Minister for Planning and Infrastructure, as the consent authority, approve the CPA.

There appear to be no provisions of Section 75W that prohibit or limit the proposed modification.

3.2 ENVIRONMENTAL PLANNING INSTRUMENTS

3.2.1 State Environmental Planning Policy (Major Development) 2005

Major Project MP07_0072 was approved under Part 3A of the Act. Part 3A of the Act was repealed on 1 October 2011 and Schedule 6A of the Act sets out the transitional arrangements applying to projects approved under Part 3A.

Clause 2(5) of Schedule 6A of the Act states that:

A transitional Part 3A project extends to the project as varied by changes to the Part 3A project or concept plan application, to the concept plan approval or to the project approval, whether made before or after the repeal of Part 3A.

Accordingly, any modifications to the approved project will continue to be dealt with under Part 3A as Section 75W modifications.

The Minister for Planning and Infrastructure will continue to determine applications made by State agencies and public proponents, with less significant or non-controversial applications being determined by senior officers of the Department under delegation (refer Department of Planning and Infrastructure Fact Sheet, May 2011).

3.2.2 State Environmental Planning Policy (Infrastructure) 2007

The project (as modified) represents traffic generating development pursuant to Section 104 and Schedule 3 of *State Environmental Planning Policy Infrastructure 2007*. As such the consent authority is required to consult with NSW Roads and Maritime Services.

Traffic matters are addressed in *Section 3.4.1* of this report.

3.2.3 State Environmental Planning Policy No. 55 Remediation of Land

The provisions of SEPP 55 were addressed in the original environmental assessment for the project.

A Stage 1 site assessment was undertaken by Environmental Investigation Services and concluded that the subject land did not indicate any obvious on site activity that could be expected to generate significant soil contamination.

Further, the results of laboratory testing on selected soil samples indicated levels below the adopted health-based assessment criteria.

3.2.4 State Environmental Planning Policy (Building Sustainability Index: BASIX) 2004

SEPP (Building Sustainability Index: BASIX) 2004 (the BASIX SEPP) requires certain types of residential development be accompanied by a list of commitments by the applicant as to the manner in which the development will be carried out. Those commitments show how that development will meet certain water and energy saving targets applying to that development.

BASIX also overrides provisions of other environmental planning instruments and development control plans that would otherwise add to, subtract from or modify any obligations arising under the BASIX scheme.

The residential component of the Orange Private Hospital campus will be designed to satisfy the energy and water targets applying to the site. BASIX certificates showing compliance with this SEPP will accompany future applications for residential development in Precincts 3 and 4.

3.2.5 Orange Local Environmental Plan 2011

The relevant provisions of Orange Local Environmental Plan 2011 (the LEP) are considered below.

Zoning

The subject land is zoned R1 General Residential. The objectives of the R1 Zone are:

- To provide for the housing needs of the community.
- To provide for a variety of housing types and densities.
- To enable other land uses that provide facilities or services to meet the day to day needs of residents.
- To ensure development is ordered in such a way as to maximise public transport patronage and encourage walking and cycling in close proximity to settlement.
- To ensure that development along the Southern Link Road has an alternative access.

There are no aspects of the proposed modification that are adverse to the zone objectives. In particular:

- The proposed modification increases residential opportunities and is entirely consistent with the first and second stated objectives.

- The proposed modification is entirely consistent with the third stated objective in that it will provide a range of other compatible land uses and services that meet the day to day needs of residents.
- In response to the fourth stated objective, the project (as modified) forms part of an evolving medical/hospital precinct and would thus benefit by the various transport modes that service the area.
- The fifth stated objective is not relevant to the proposed modification.

Permissibility

The Concept as modified remains permissible as follows:

- The projects identified for Precincts 1 and 2 represent development for the purposes of a *health services facility* and are permissible in the R1 Zone under *State Environmental Planning Policy (Infrastructure) 2007*. Clause 57(1) of the SEPP states that development for the purpose of *health services facilities* may be carried out by any person with consent on land in a prescribed zone. The R1 General Residential Zone is a *prescribed zone*.
- In regard to the proposed community/childcare facilities within Precinct 3, the Land Use Table in Orange LEP 2011 indicates that *community facilities* and *child care centres* are permissible in the R1 Zone.
- In regard to the proposed residential components in Precincts 3 and 4, the Land Use Table in Orange LEP 2011 indicates that *dwelling houses*; *multi dwelling housing*; *residential accommodation*; *residential flat buildings*; and *semi-detached dwellings* are all permissible in the R1 Zone.

Clause 1.2 Aims of Plan

The particular aims of the LEP are as follows:

- a) to encourage development that complements and enhances the unique character of Orange as a major regional centre boasting a diverse economy and offering an attractive regional lifestyle,
- b) to provide for a range of development opportunities that contribute to the social, economic and environmental resources of Orange in a way that allows the needs of present and future generations to be met by implementing the principles of ecologically sustainable development,
- c) to conserve and enhance the water resources on which Orange depends, particularly water supply catchments,

- d) to manage rural land as an environmental resource that provides economic and social benefits for Orange,
- e) to provide a range of housing choices in planned urban and rural locations to meet population growth,
- f) to recognise and manage valued environmental heritage, landscape and scenic features of Orange.

In consideration of the general aims of LEP 2011, the following comments are provided in support of the proposed modification:

- There are no aspects of the proposed modification that would detract from the character of Orange as a major regional centre [*General Aim (a)*].
- The proposed modification maintains the potential to have a positive effect in terms of the social, economic and environmental resources of the City. There are no aspects of the proposed modification that would compromise the principles of ecologically sustainable development [*General Aim (b)*].
- There are no aspects of the proposed modification that would represent a direct threat to the City's water resources [*General Aim (c)*].
- The management of rural land as an environmental resource is not relevant to this proposal [*General Aim (d)*].
- The proposed modification would enhance the City's range and supply of housing choices [*General Aim (e)*].
- Based on the information provided in this report, the proposed modification will not adversely affect the value of heritage, landscape and scenic features of the City [*General Aim (f)*].

Clause 5.10 Heritage Conservation

The subject land is not within a Heritage Conservation Area. However, it is in the vicinity of "Bloomfield Hospital" which is identified in the LEP as a heritage item of State significance.

Clause 5.10 of the LEP applies. The objectives of this clause are as follows:

- a) to conserve the environmental heritage of Orange,
- b) to conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views,
- c) to conserve archaeological sites,

- d) to conserve Aboriginal objects and Aboriginal places of heritage significance.

Pursuant to Clause 5.10(4), it is necessary to consider the potential impact that the proposed modification may have on any heritage item within the vicinity of the subject land.

Heritage matters are addressed at *Section 3.4.3* of this report.

Clause 7.3 Stormwater management

Clause 7.3 of the LEP applies. The objective of this clause is to minimise the impacts of urban stormwater on the land to which the development applies, and on adjoining downstream properties, native bushland and receiving waters.

The proposed modification does not involve any alteration to the approved stormwater drainage arrangements that will serve the approved project.

Clause 7.6 Groundwater vulnerability

The subject land is defined on the Orange Local Environmental Plan 2011 – Water Map as having groundwater vulnerability. Clause 7.6 of the LEP applies.

The objectives of this clause are to maintain the hydrological functions of key groundwater systems and to protect vulnerable groundwater resources from depletion and contamination as a result of inappropriate development.

There are no aspects of the proposed modification that would cause adverse impact on groundwater resources.

3.3 DEVELOPMENT CONTROL PLANS

3.3.1 Orange Development Control Plan 2004 – 07 Development in Residential Areas

Orange Development Control Plan 2004 – 7 Development in Residential Areas applies to the subject land. The DCP sets Planning Outcomes for residential development pertaining to open space, solar access, parking, urban design and the like.

Future development within Precincts 3 and 4, where relevant, would be subject to these Planning Outcomes at the time that project applications are made.

3.3.2 Orange Development Control Plan 2004 – 13 Heritage

Orange DCP 2004 – 13 Heritage applies to the extent that the subject land is in the vicinity of an identified heritage item. The DCP sets certain Planning Outcomes pertaining to heritage and conservation.

The proposed modification is not adverse to these Planning Outcomes. The heritage impacts of the proposed modification are considered later in this report at *Section 3.4.3*.

3.3.1 Orange Development Control Plan 2004 – 15 Car Parking

The parking requirements of DCP 2004-15 are considered below in *Section 3.4.1*.

3.4 POTENTIAL IMPACTS

3.4.1 Traffic and Parking

Consultants Terraffic Pty Ltd have undertaken a traffic and parking assessment which is included as Annexure B to this report. Terraffic assess the traffic and parking impacts for the entire project as modified. The findings and conclusions of the Terraffic report are summarised below.

Parking Assessment

The proposed development will be served by a total of 617 off-street car parking spaces as follows:

- Precinct 1
 - 101 spaces (including 6 disabled spaces) serving the hospital
- Precinct 2
 - 116 spaces (including 7 disabled spaces) serving the health facilities and retail shops
 - 72 spaces serving the motel comprising 70 guest and 2 staff parking spaces. This parking provision includes 5 disabled guest spaces
- Precinct 3
 - 83 spaces serving the 59 residential units
 - 25 spaces serving the 100 place Child Care Centre
- Precinct 4
 - 220 spaces serving the 157 residential units

In addition to these spaces, there are 41 parallel parking spaces on the internal road network serving the site. Once these roads are constructed to provide access to Precincts 3 and 4, these on-street parking spaces will be available as surplus parking for the overall development (thus the total parking provision will be 658 spaces).

According to DCP 2004–15, the proposed development would generate a parking requirement of 749 spaces as calculated in the following table.

Proposed Use	DCP requirement	Calculation Unit	Spaces Required
Hospital	1 space for every 3 beds plus 1 space each resident doctor plus 1 space for every 2 visiting doctor plus 1 space for every 2 employees.	104 beds = 35 spaces Allow 130 staff including doctors = 65 spaces	100
Health Consulting Rooms	2 spaces for every 1 practitioner with spaces being available for customer and staff use.	Allow 24 practitioners = 48 spaces	48
Motel	1 space per unit plus 1 space for manager plus 1 space for every 2 employees plus 1 space per 3 seats in restaurant plus 1 space per 10m ² of entertainment or function room areas	82 units = 82 spaces 1 manager = 1 space Say 12 staff = 6 spaces 90 seat restaurant=30 spaces 140m ² function= 14 spaces	133
Restaurants	1 space per 10m ² GFA or 1 space for every 3 seats, whichever is greater	150 seats = 50 spaces	50
Childcare Centre	1 space for every 4 children in attendance	100 places = 25 spaces	25
Residential Units	1.2 spaces per 2 bedroom unit 0.2 visitors spaces per unit	216 x 2 bed units=259 spaces 216 x 0.2 = 43 visitor spaces	302
Shops	6.1 spaces per 100m ² GLFA	GLFA 1,498m ² = 91 spaces	91
TOTAL PARKING REQUIREMENT FOR PROPOSED DEVELOPMENT			749

Based on the proposed parking provision and the DCP requirements, the development would generate a parking shortfall of 91 spaces. However, Terrafic seeks to modify the DCP requirements based on the following arguments pertaining to dual and complimentary uses (refer extract below):

Dual and Complimentary Use of Parking

The Council's parking requirements do not consider the Dual and Complementary use of parking that will occur on the site.

Dual use of parking spaces occurs when patrons of one component of a development also patronise another. For example, a proportion of staff and visitors to the proposed hospital and medical facility can also be expected to patronise the retail shops in Precinct 2. Not only will this reduce the parking requirements for the proposed medical facility, it also has the potential to reduce the traffic generating potential of visitors to the site.

In addition, another form of Dual use parking is that a proportion of residents in the Precinct 3 and 4 residential flat buildings will be employees of the commercial uses on the site and the Orange Base Hospital. These residents will be allocated an on-site parking space within the residential precinct and will ultimately reduce the overall parking demand and traffic generation of the proposed development site.

Complementary use of parking spaces occurs when the peak parking demand of one component of a development does not coincide with the peak parking demand of another. In this case, the peak parking demand of the restaurant or the function centre in the motel is at night after 6pm, whereas the peak parking demand of the retail shops and health facilities in Precinct 2 is throughout the day.

Modifications to DCP Parking Requirements

When taking into account the dual and complimentary use of car parking, this assessment has slightly modified the following DCP parking requirements:

- 1. The parking requirement for the retail floorspace (6.1 spaces per 100m²) is excessive because it is unlikely that the general public will travel to the site to visit the café/Florist/gift/newsagent stores. Furthermore, a high percentage of shoppers will already be on the site or will be generated by the Base Hospital on the eastern side of Forest Road (dual use). In these circumstances, the parking requirement for that retail floorspace will adopt the Council DCP rate of 4.1 spaces per 100m² which applies to shops in the CBD.*
- 2. The parking requirement specified for the motel rooms (1 space per room) is excessive because 12 of the rooms will be medical suites occupied by persons who are bedridden. Accordingly, the requirement of 1 space per room has been applied to 70 standard rooms.*
- 3. The parking requirement for the restaurant incorporated in Precinct 2 will only apply during the night when the restaurant will be operational. As noted in the foregoing, this approach is applying the principle of complementary use of parking on the site. In addition, it is likely that some restaurant patrons will be staff/visitors to the hospital or guests of the motel who are already in the area. This includes staff and visitors to the Base Hospital on the eastern side of Forest Road. To that end, the restaurant will also be adopting the dual use principle.*
- 4. The parking requirement for the restaurant incorporated in the motel is excessive because a substantial proportion of restaurant patronage, if not all of it, will be drawn from motel guests and patients. In addition there will be a proportion of staff and visitors of the proposed hospital in Precinct 1 and the existing Base Hospital opposite the site. In these circumstances, the parking requirement for the motel restaurant (1 space per 3 seats) has been reduced to 20% of the requirement and will apply*

during the night only. These additional restaurant patrons who are not already in the area will utilise the remaining Precinct 2 parking spaces allocated to the health facility and retail shops that will be closed at that time (complementary use)

5. As per the restaurants on the site, the motel function room is expected to only operate at nights when the health facilities and retail shops are closed on Precinct 2. To that end, the parking requirement of 1 space per 10m² for the motel function room will apply at night time only. Furthermore, it is expected that some of the function guests will also be motel guests who have already parked on the site (dual use).
6. As the hospital in Precinct 1 will be operational 24 hours a day, this assessment has assumed that the night time workforce parking demand will be 20% of the daytime peak. As the visitor peak is typically at nights, the parking demand for the visitor component of the parking requirement will apply during the evening (i.e. 35 spaces).

In addition, this assessment has assumed that the proposed health facilities in Precinct 2 will have a total of 24 practitioners on-site at any given time. As can be appreciated, this is a conservative approach as specialists (such as surgeons and physiotherapists) tend to operate between hospitals and their office and may only be on-site on a given day.

Adopting the above modifications (and referring to Tables 3.1 and 3.2 of the Terraffix report, the parking needs are assessed as follows:

DAYTIME PARKING REQUIREMENT	
Precinct 1	Hospital 100 spaces
Precinct 2	Health Facility 55 spaces Retail Shops 61 spaces Motel 72 spaces (Precinct 2 Sub-total 188 spaces)
Precinct 3	Residential 83 spaces Child Care Centre 25 spaces (Sub-total 108 spaces)
Precinct 4	Residential 220 spaces
Total requirement	616 spaces
NIGHT TIME PARKING REQUIREMENT	
Precinct 1	Hospital 48 spaces
Precinct 2	Restaurant 50 spaces Motel 72 spaces Motel restaurant 8 spaces Motel function room 14 spaces (Precinct 2 Sub-total 144 spaces)
Precinct 3	Residential 83 spaces
Precinct 4	Residential 220 spaces
Total requirement	495 spaces

Based on the above information, the Terraffic report concludes that the parking provision incorporated in the development proposal is adequate and that the proposed development has no unacceptable parking implications.

Servicing Assessment

The Terraffic report concludes that the project will be satisfactory in terms of servicing as summarised below:

- **Precinct 1: Private Hospital Deliveries:**
 - The proposed development will retain the 2 loading docks that were approved to serve the hospital development. The loading docks are required to accommodate the commercial vehicles transporting food, linen, medical supplies, office equipment and provisions to the hospital, and transporting waste from the hospital.
 - The vehicles used in those activities are expected to comprise predominantly vans and the SRV, with some deliveries via the MRV and the HRV. Articulated vehicles (AV) are not expected to be involved in deliveries to/from the site.
 - While the number of commercial vehicle trips generated by the Private Hospital will be variable, it is expected that the maximum daily commercial vehicle generation will be in the order of 20 vehicles per day (40 vehicle trips per day).
 - All delivery vehicles accessing the hospital precinct and loading docks will enter via the Forest Road traffic signals. The ability of the heavy rigid vehicle to access the site and manoeuvre to/from the loading dock has been tested using the AutoTrack computer simulation software. This test found that the HRV can comfortably access the site and the loading docks incorporated in the proposed Private Hospital.
- **Precinct 2: Health Facilities, Neighbourhood Shops and Motel Deliveries:**
 - The proposed health facility is not served by a dedicated loading dock like the hospital in Precinct 1. Couriers making deliveries to this facility in vans will utilise the off-street parking area serving this component of the Precinct 2 development. Deliveries by larger commercial vehicles are not expected to occur for the health facility.
 - The neighbourhood shopping area contains 2 separate loading areas capable of accommodating Medium Rigid Vehicles. The loading docks are located at the eastern and western ends of the development and will provide convenient access to each shop.

- The motel will also be served by a loading area capable of accommodating the Medium Rigid Vehicle. The motel is expected to generate commercial vehicles transporting predominantly food, liquor, linen and other provisions to the motel, and transporting waste from the motel.
- The vehicles used in those activities are expected to comprise predominantly vans, with some deliveries using the SRV and the MRV.
- While the number of commercial vehicle trips generated by the uses in Precinct 2 will be variable, it is expected that the maximum daily commercial vehicle generation will be in the order of 30 vehicles per day (60 vehicle trips per day).
- The ability of medium rigid vehicles to access the site and manoeuvre to/from the loading areas serving each use in Precinct 2 has also been tested using the AutoTrack computer simulation software. This test found that the MRV can comfortably access the site and the loading areas.
- Precinct 3: Child Care Centre and Residential Unit Deliveries:
 - Deliveries to the childcare centre are expected to be only minor, comprising 3 to 4 vehicle trips per day in vans or SRV's.
 - Childcare centres typically avoid deliveries during peak child set-down/pick-up periods and utilise the empty car park outside of the am and pm peak periods.
- Precinct 3 and 4: Residential Unit Deliveries:
 - While the residential component of the proposed development will generate commercial vehicle activity associated with residents moving into and out of the precinct, and some service vehicle activity for residents living there, this commercial vehicle activity will not be frequent nor regular, or of a level that is likely to have any significant traffic implications.
 - Based on these assumptions, the overall development will generate in the order of 45 to 50 deliveries per day ranging from courier vans to Heavy Rigid Vehicles. This level of activity equates to no more than 10 deliveries to the site during the morning and evening peak periods. This commercial vehicle activity will not be of a level that is likely to have any significant traffic implications.

Traffic Assessment

The currently approved access arrangements that serve the site off Forest Road will be retained and comprise the following:

- A new 4-way signalised intersection on Forest Road that will connect the site to the main access driveway serving the Orange Base Hospital which is located on the eastern side of Forest Road. The new signals will include pedestrian crossings on each leg of the intersection to enhance pedestrian safety.
- Left turn exit only movements from a northern access driveway

To assess the adequacy of the approved access arrangements in the context of the proposed modification, the traffic assessment involved:

- Prediction of traffic generation by the development;
- Allowance for traffic growth along Forest Road;
- Modelling of each access point under projected future demand.

Terrafic concludes that the project will not generate unacceptable traffic impacts as summarised below:

- The main traffic implications of the proposed development concern the ability of traffic that it generates to access the site via the two access points off Forest Rd (i.e. the new traffic signals serving the site and the Base Hospital and the northern left out only access driveway).
- The ability for these intersections to accommodate the projected post-development traffic demand can be assessed using the SIDRA traffic model.
- The access points off Forest Rd were modelled under projected future (2024) traffic demand and include the current traffic generation characteristics of the Base Hospital on the eastern side of Forest Road.
- The results of that SIDRA analysis reveal that both intersections will operate satisfactorily under projected traffic demand in 2024.

3.4.2 Visual Amenity

The CPA will alter the bulk and scale of the buildings originally envisaged by the Approved Concept. However the potential visual impacts are considered satisfactory due to the following:

- Precinct 1:
 - The building footprint and siting remains unchanged.
 - At the Forest Road frontage, the building height and design generally remains as approved. The increase in building height (due to the addition of more beds) occurs only on the western wing of the hospital, towards the rear of Precinct 1. This design approach is considered satisfactory in terms of visual impact. The taller element is recessed from the street frontage and site boundaries and should therefore assist to render the development less prominent when viewed from adjoining property and the road network.
 - The proposed modification removes the expansive car park that dominates the frontage of the hospital under the current approval and thus increases the opportunity for additional landscaping in this area.
 - The proposed modification maintains a high level of landscaping.
- Precinct 2:
 - The buildings proposed by this modification maintain the characteristics of the currently approved project, particularly the 2 storey scale and the modern and contemporary architecture (that also compliments the public hospital which has been constructed since the original approvals).
 - High quality materials and finishes are proposed.
 - Compared to the current approval for Precinct 2, the proposed modification has the potential to improve the eventual streetscape. The current approval provides for an elongated and largely unbroken building form more or less parallel to Forest Road. The proposed modification avoids this by breaking the buildings into separate complexes and providing space and landscaping between them. Further, the proposed buildings are set at non-parallel angles to improve articulation.
 - The proposed modification removes the expansive car park that dominates the frontage of the site under the current approval and thus increases the opportunity for landscaping in this area.
 - The proposed modification maintains a high level of landscaping.

- It is envisaged that Precincts 3 and 4 will involve taller building forms. The Concept sets these buildings towards the rear of the site and provides for generous landscaped areas and open space in between. These attributes will underpin detailed planning and design of future buildings within these precincts. Future building forms and potential visual impacts will be assessed in greater detail as part of future applications for development within Precincts 3 and 4.

3.4.3 European Heritage

Schedule 5 of Orange LEP 2011 identifies the Bloomfield Hospital campus (to the east of the subject land on the opposite side of Forest Road) as a heritage item of State significance. More specifically, the listing refers to *Bloomfield Hospital "Nymagee Lodge" (including landscape features, entry gateway, Elm avenue and grounds)*.

A review of the Bloomfield Hospital Conservation Management Plan (CMP) indicates that this project is located well beyond the primary heritage curtilage and thus presents even less of a potential impact upon the heritage values of the place.

In any event and consistent with the Approved Concept, the proposed CPA respects the heritage item. Notwithstanding the increase in building height at the rear of the private hospital, the building heights in Precincts 1 and 2 remain essentially as approved along the Forest Road boundary of the project site. Further, the visual relationship between the heritage item and the proposed taller section of the hospital is diluted due to the physical separation that exists between them.

It should also be noted that the new public hospital which has been constructed in the Bloomfield Hospital campus is a modern and bulky building that now characterises the precinct. This building and its external support areas diminish the visual relationship between the heritage item and the subject land.

The proposed modification of the Concept in relation to Precincts 3 and 4 will not generate adverse impacts in relation to the Bloomfield Hospital campus which is identified as a heritage item in the vicinity of the subject land. This is largely due to the fact that the visual and spatial relationship between Precincts 3 and 4 and the heritage item is diminished by distance and the intervening development that is proposed to occur within Precincts 1 and 2.

3.4.4 Aboriginal Heritage

The potential impacts on Aboriginal heritage were considered in the environmental assessment for the Approved Concept. There are no aspects of the proposed modification that would generate additional impacts in this regard.

However, should any Aboriginal or European Relics be unexpectedly discovered during works associated with the concept plan, all excavations or disturbances in the area will stop immediately and the NSW Heritage Office and NSW National Parks and Wildlife Service shall be informed immediately.

3.4.5 Flora and Fauna

The potential impacts on native flora and fauna were considered in the environmental assessment for the currently approved Concept. There are no aspects of the proposed modification that would generate additional impacts in this regard.

3.4.6 Social and Economic Impacts

The social and economic impacts of the project were considered as positive in the environmental assessment for the Approved Concept, particularly as the co-location of the private hospital campus with the existing public hospital campus would present numerous benefits for patients, staff, administrators and carers to utilise the complementary medical, social and ancillary services of the two facilities.

The CPA is expected to broaden the social and economic benefits due to the following:

- The increased floor area in the private hospital is expected to generate the following benefits:
 - It increases the number of private hospital beds plus offers additional space for other hospital services and facilities including an accident and emergency department.
 - Improved hospital offerings contribute to the notion of a “centre of excellence” as various medical treatment facilities and technologies accumulate in both the private and public sectors of the broader medical precinct.
- The increase in floor area for health services will encourage a broader range of medical and allied health professionals to locate at the site in close proximity to public and private hospitals. The benefits include:
 - Greater convenience and less discomfort for patients in being able to obtain treatment in a relatively contained medical precinct.
 - Greater convenience for health professionals to consult in their rooms and also undertake their hospital-based work.

- Improves the current situation where there are limited sites and available land for private health consulting rooms and medical facilities to become established in close proximity to the existing public and proposed private hospitals.
 - A broader range of health services facilities and consulting rooms contributes to the notion of a “centre of excellence” as various medical treatment facilities and technologies accumulate in both the private and public sectors of the broader medical precinct.
- The proposed increase in residential accommodation opportunities is expected to generate the following benefits:
 - The increase is in line with the proposed increase in health facilities to not only cater for the accommodation needs associated with the private hospital campus but also to cater for the accommodation needs associated with the public hospital campus (which was not in existence when the original Concept was approved).
 - It provides for a broad range of dwelling styles that may include 1, 2, or 3 bedroom apartments that offer residential accommodation for health precinct staff; patients and their families; and downsizing or elderly people who seek a non-institutionalised style of living with the convenience of nearby health and medical services.
- The increase in retail floor area will enable the creation of a neighbourhood scale shopping complex that will be of benefit not only to the private hospital but also serve the nearby public hospital and the aged care and retirement facility. The neighbourhood shops complex will contribute to the notion of a central focus and gathering space, much like the “village common” that was envisaged in the Approved Concept.

CONCLUSION

The CPA remains consistent with the intent of the Approved Concept which sought to form a cohesive, harmonious and interrelated whole about a nucleus of health services.

The CPA responds to circumstances that have occurred since the original Concept Approval was granted. In this time, the public hospital has been commissioned; and an aged care and retirement facility has commenced construction on the site to the south of the subject land.

The new owners of the subject land are of the view that the synergy of this developing health services precinct would benefit not only by increased private hospital and health facilities but also by an increase in retail and accommodation opportunities. Accordingly, the CPA retains the structure of the four development precincts identified in the Approved Concept Plan, but seeks to alter/update the Concept as follows:

- Provide additional floor space to accommodate more private hospital beds and other medical services/facilities.
- Increase floor space for health services and consulting in close proximity to the recently established Orange Health Service (public hospital) as well as the proposed private hospital.
- Increase retail floor space to enable the creation of a neighbourhood scale shopping complex that will be of benefit not only to the private hospital but also serve the nearby public hospital and the aged care and retirement facility.
- Increase residential accommodation opportunities in line with the proposed increase in health facilities to not only cater for the accommodation needs associated with the private hospital campus but also to cater for the accommodation needs associated with the public hospital campus (which was not in existence when the original Concept was approved).

The environmental assessment demonstrates that the CPA will not result in any significant adverse impacts on the surrounding environment and will be likely to generate positive social and economic impacts.

It is recommended that this Section 75W application be approved

Yours faithfully

Peter Basha Planning & Development



Per:

PETER BASHA

Annexure A

Architectural Drawings by TVS Architects

Site Boundary

Site Boundary

Site Boundary

Site Boundary

FOREST ROAD

Vehicle Entry

Site Boundary

RESIDENTIAL 4
(Apartments)

RESIDENTIAL 1
(Apartments)

PRECINCT
4

RESIDENTIAL 3
(Apartments)

RESIDENTIAL 2
(Apartments)

LANDSCAPE

PRECINCT
3

APARTMENTS

HOSPITAL

PRECINCT
1

OUT-PATIENTS

ACCIDENT +
EMERGENCY

LANDSCAPE

HEALTH.
FACILITIES

CARPARKING

PRECINCT
2

NEIGHBOURHOOD
SHOPS

MOTEL

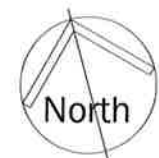
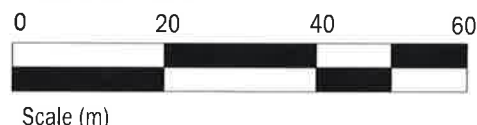
Site Plan

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02.103

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12	Coordination Issue	11.04.14	DH

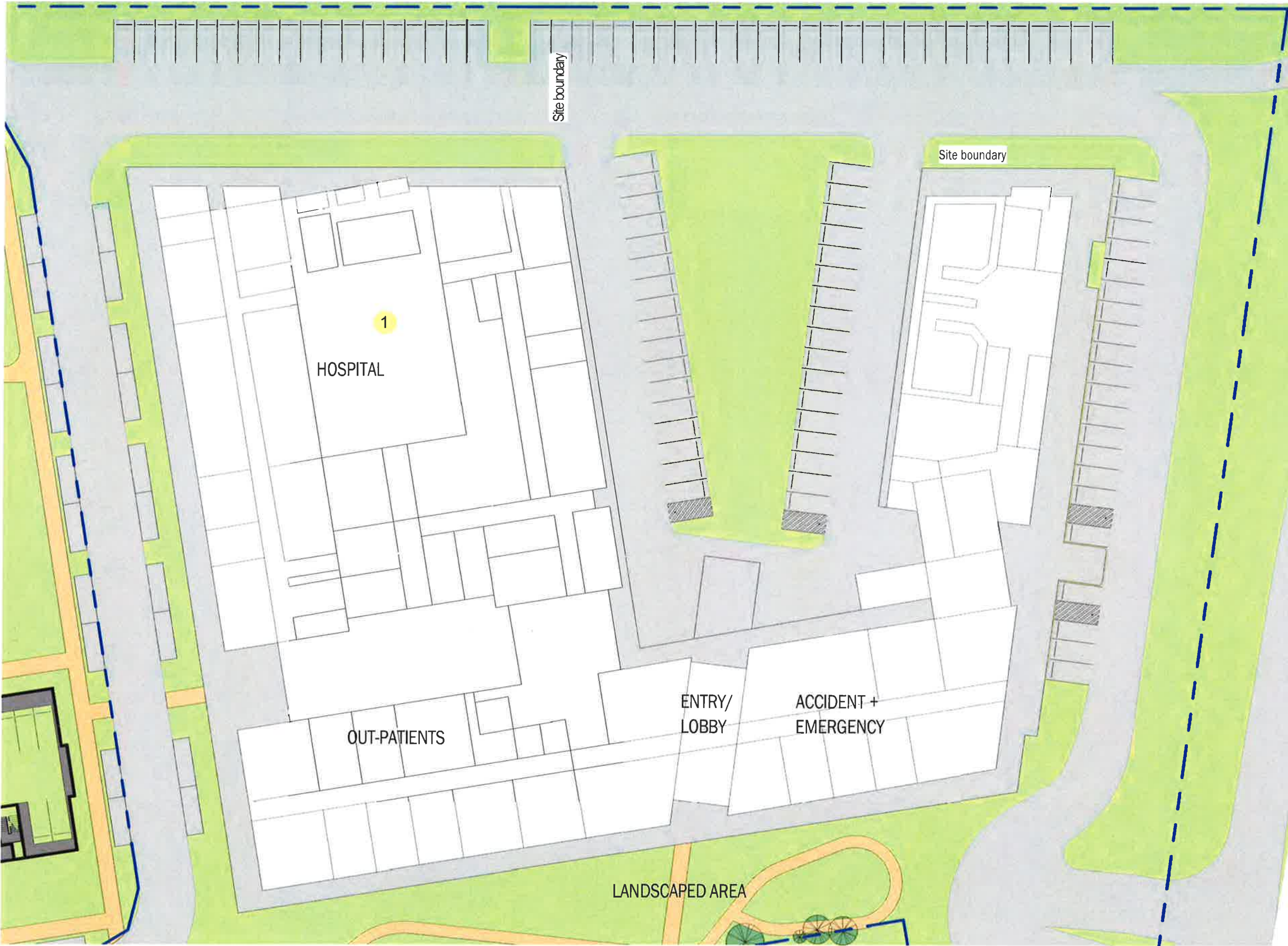
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Project
Orange Private Hospital Site

Client
James Richmark Pty Ltd

Drawing Title
Site Master Concept Plan

Project Architect/Designer M Urquhart
Scale @ A3 1:1000
Drawing No. 5420.3S.01.101
Checked PJ
Date Drawn Aug 13
Date Checked
Revision 13
Drawn By DH



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Scale (m)

North

2	DA Issue	16.04.14	DH
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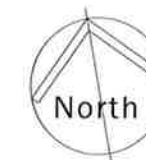
Client
James Richmark Pty Ltd

Drawing Title
Private Hospital Precinct -
Site Plan

Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	BH	Mar 2014
Scale @ A3	Checked	Date Checked
1 : 500		

Drawing No.	Revision
5420.3S.01.104	2

Scale (m)



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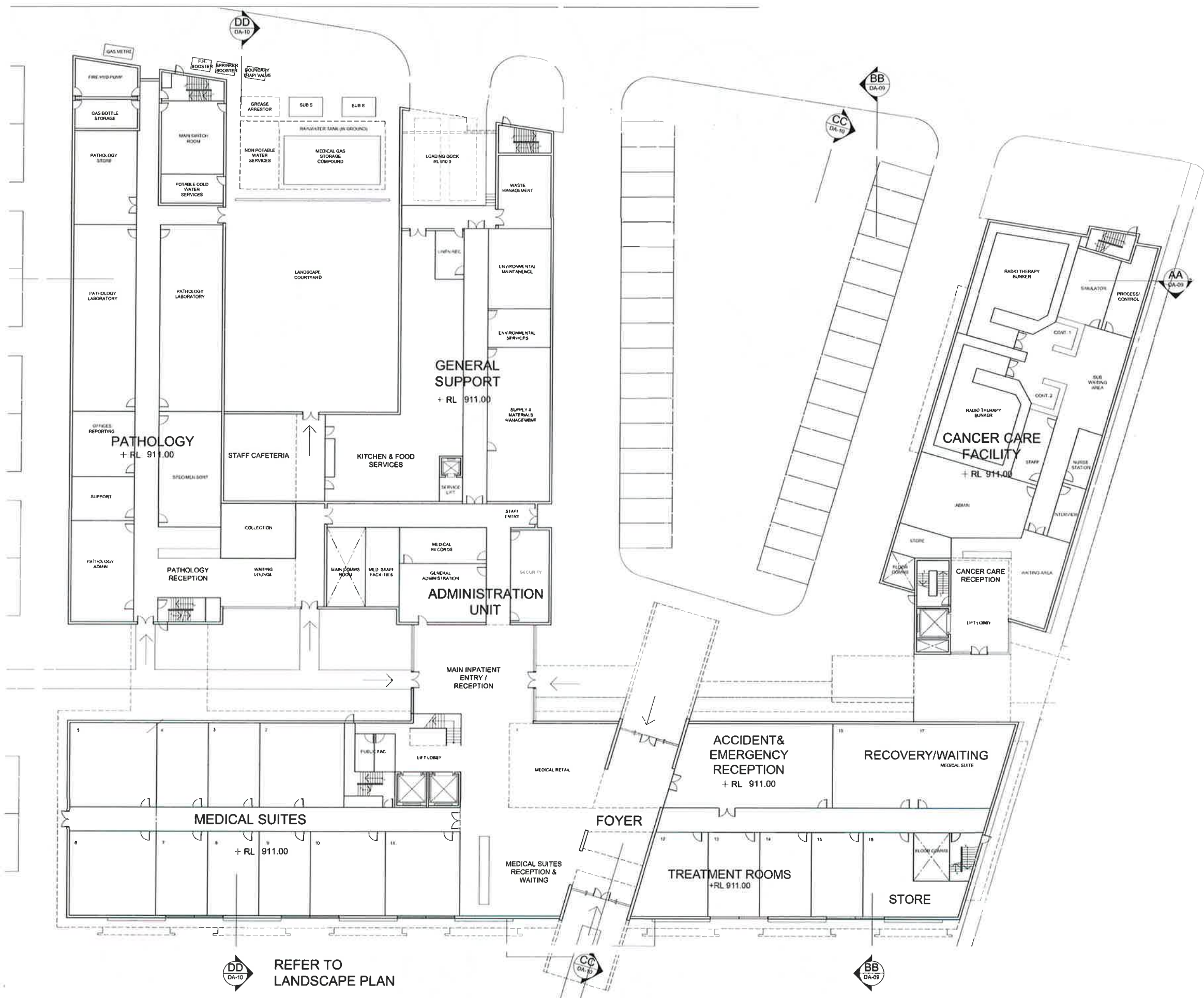
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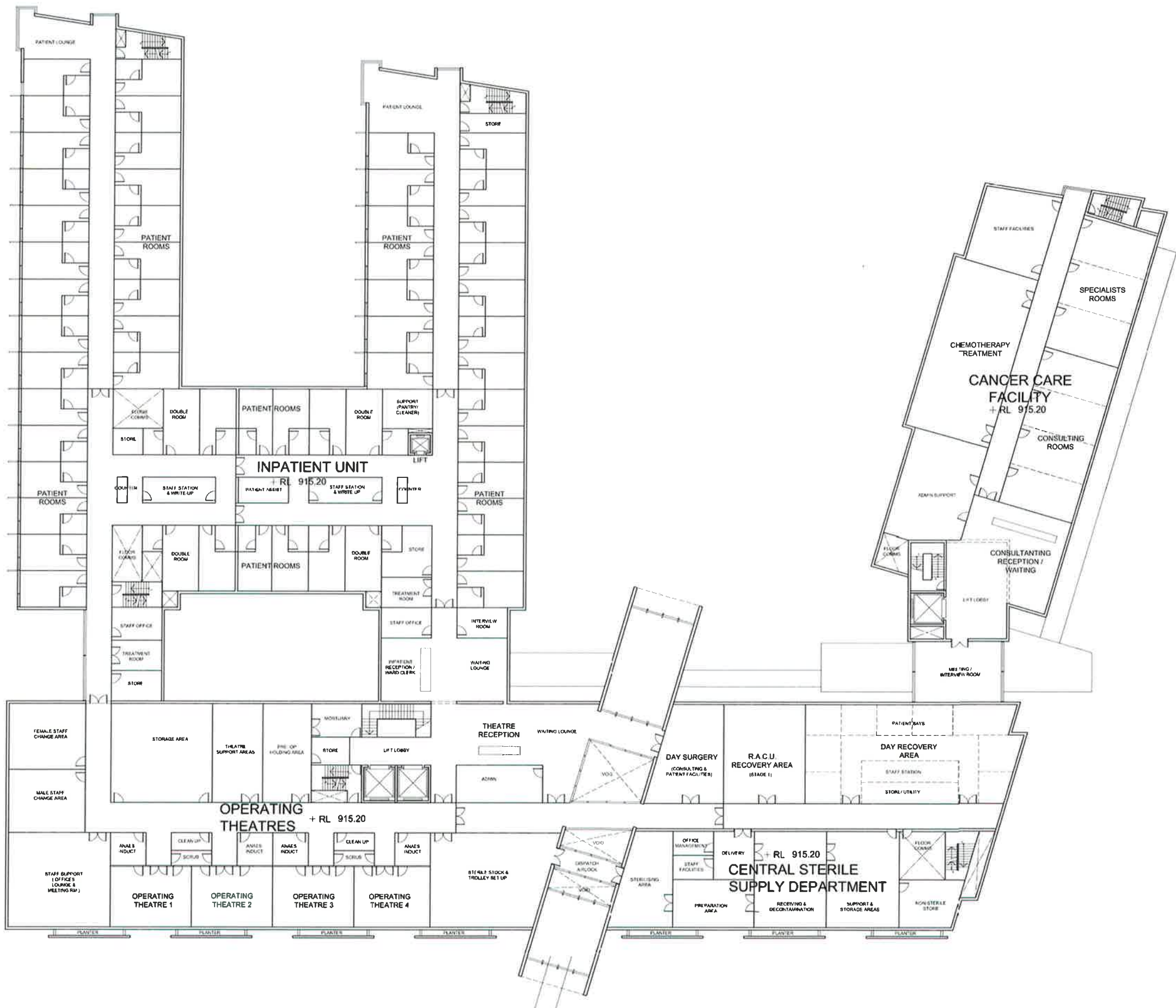
Drawing Title

Private Hospital Precinct -
Level 1

<i>Project Architect/Designer</i>	<i>Drawn By</i>	<i>Date Drawn</i>
M Urquhart	BH	Mar 2014
<i>Scale @ A3</i>	<i>Checked</i>	<i>Date Checked</i>
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5420.3S.02.401 1





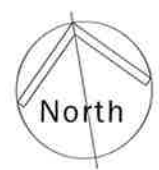
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Scale (m)



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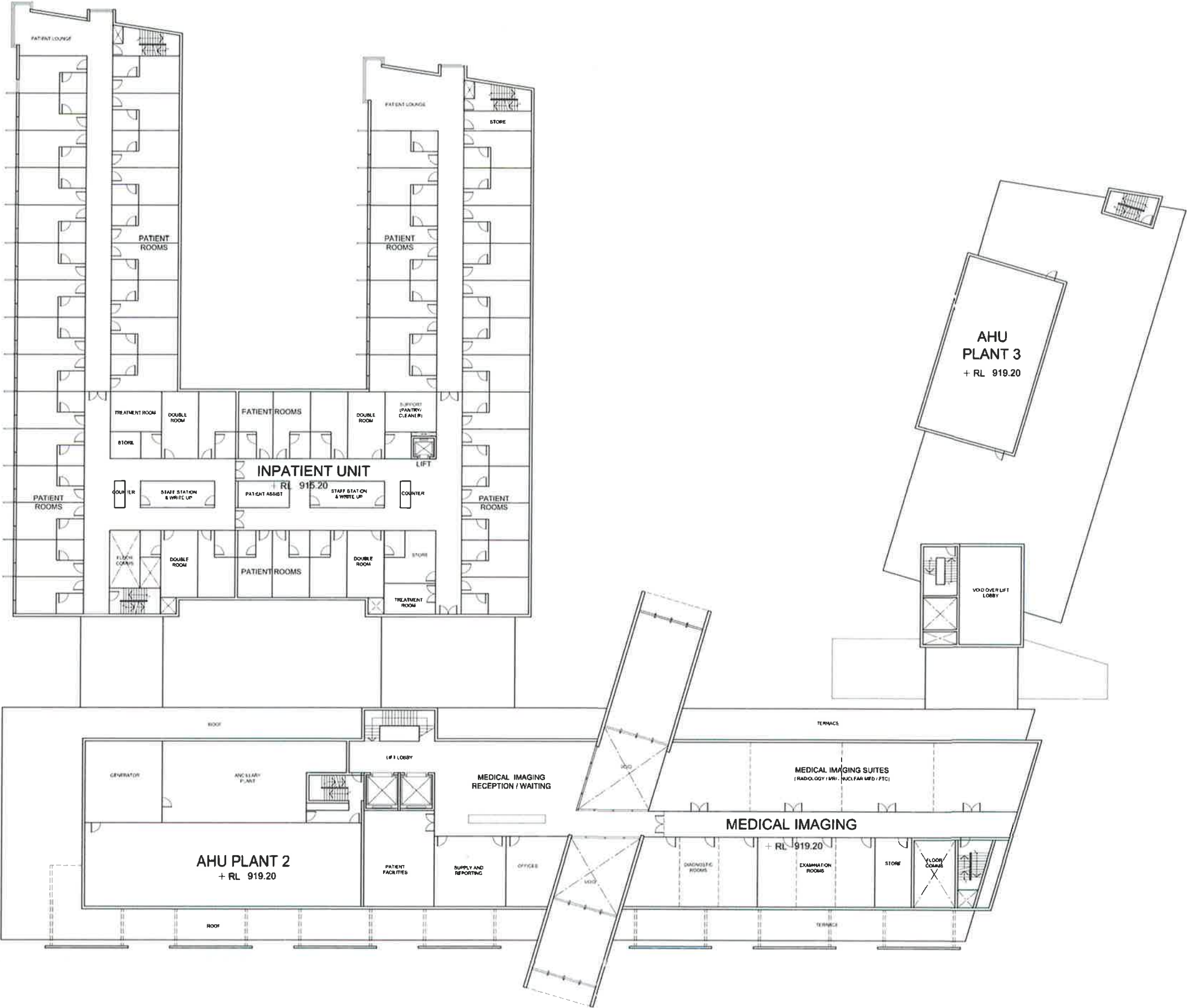
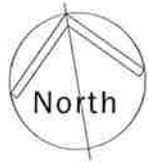
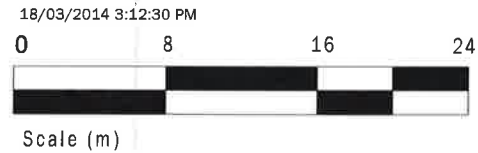
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Drawing Title
Private Hospital Precinct -
Level 2

Project Architect/Designer M Urquhart
Scale @ A3 1 : 400
Drawing No. 5420.3S.02.402

Drawn By BH
Date Drawn Mar 2014
Date Checked
Revision 1

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Client
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Drawing Title
Private Hospital Precinct -
Level 3

Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	BH	Mar 2014
Scale @ A3	Checked	Date Checked
1:400		
Drawing No.	Revision	
5420.3S.02.403	1	

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Project

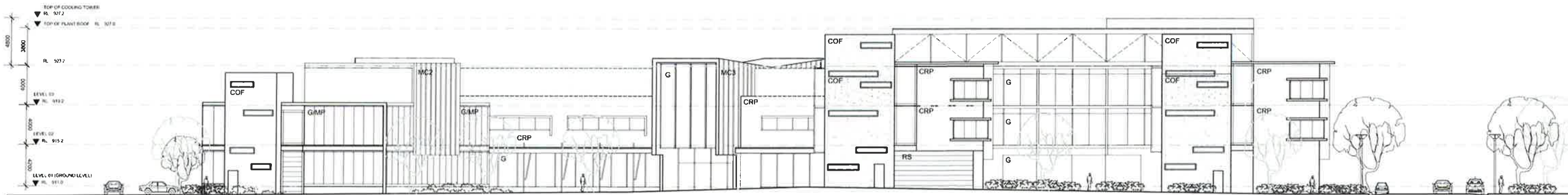
Orange Private Hospital Site

Client
James Richmark Pty Ltd

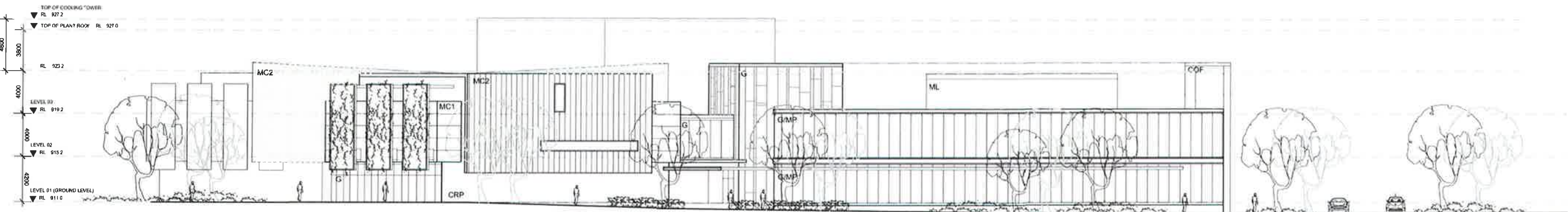
Drawing Title

Private Hospital Precinct -
Roof Plan

Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	BH	Mar 2014
Scale @ A3	Checked	Date Checked
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Drawing No.	Revision	
5420.3S.02.404	1	



NORTHERN ELEVATION



EASTERN ELEVATION

LEGEND:

- CRP - CEMENT RENDER PAINTED TO MASONRY
- G - GLAZING
- GMP - COLOURED GLAZING AND COLOURED METAL PANELS
- TP - TIMBER PLY PANELS
- CT - CERAMIC TILE
- COF - CONCRETE OFF-FORM
- CSS - CONCRETE BLADE SUN SHADE
- RS - ROLLER SHUTTER
- TIM - TIMBER BATTENS SCREEN
- MC1 - PRE-FINISHED METAL CLADDING (HORIZONTAL)
- MC2 - PRE-FINISHED METAL CLADDING (VERTICAL)
- MC3 - PRE-FINISHED METAL CLADDING (PANELS)
- MM - PRE-FINISHED METAL MESH
- MSS - PRE-FINISHED METAL SUN SHADE (HORIZONTAL)
- ML - METAL LOUVRES (HORIZONTAL)
- SG - STEEL FRAMED GRILL PANEL
- PB - STEEL FRAMED PLANTER BOX (HIDDEN) AND CLIMBING PLANTS

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Project
Orange Private Hospital Site
Forest Road, Orange
Drawing Title
Private Hospital Precinct - Elevations

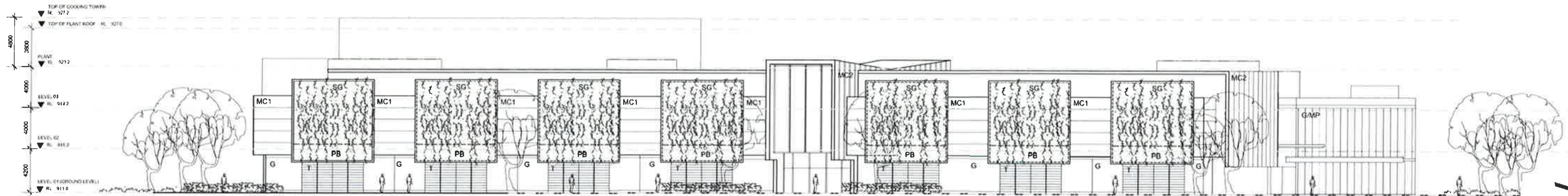
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M Urquhart
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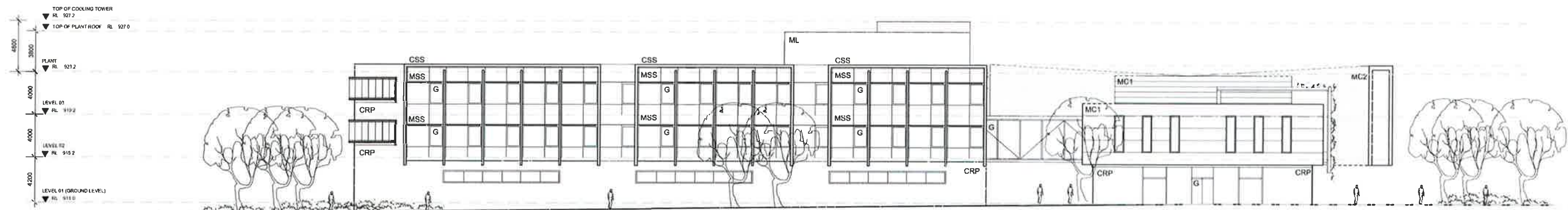
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BH
Checked for Construction
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Date Drawn
Mar 2014
Date Checked
1

Revision
1



SOUTHERN ELEVATION



WESTERN ELEVATION

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COF - CONCRETE OFF-FORM
CSS - CONCRETE BLADE SUN SHADE
RS - ROLLER SHUTTER
TIM - TIMBER BATTENS SCREEN

MC1 - PRE-FINISHED METAL CLADDING (HORIZONTAL)
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ML - METAL LOUVRES (HORIZONTAL)
SG - STEEL FRAMED GRILL PANEL
PB - STEEL FRAMED PLANTER BOX (HIDDEN) AND CLIMBING PLANTS

Project

Orange Private Hospital Site
Forest Road, Orange

Drawing Title

Private Hospital Precinct - Elevations

Revision	Description	Date	Initials
1	DA Issue	18.03.14	DH

Project Architect/Designer

M Urquhart

Drawn By

BH

Date Drawn

Mar 2014

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Date Checked

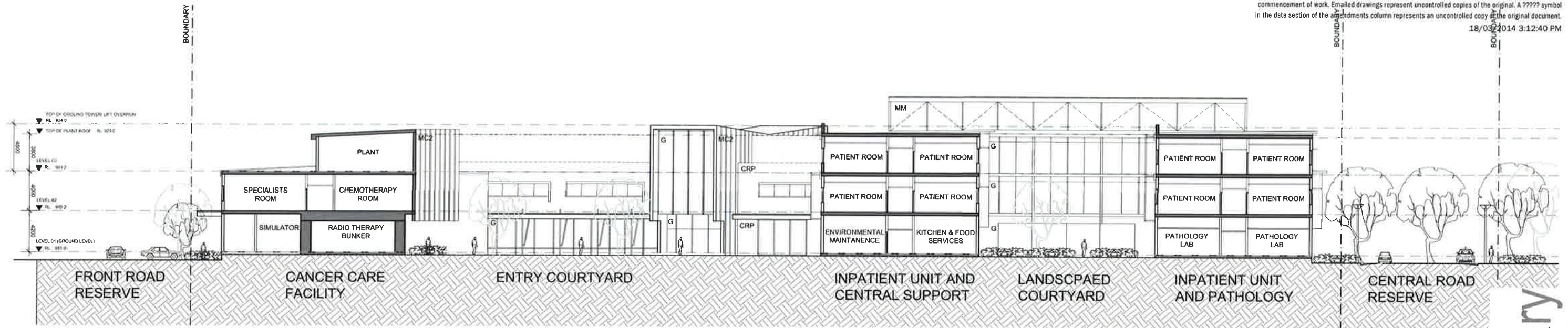
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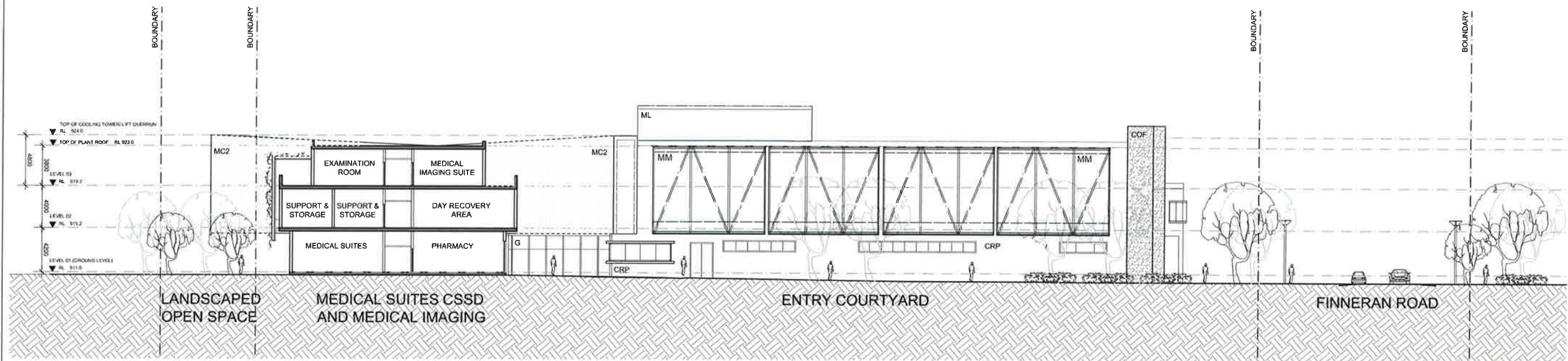
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Revision

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SECTION A-A



SECTION B-B

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CSS	- CONCRETE BLADE SUN SHADE	SG	- STEEL FRAMED GRILL PANEL
RS	- ROLLER SHUTTER	PB	- STEEL FRAMED PLANTER BOX (HIDDEN) AND CLIMBING PLANTS

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Drawing Title
Private Hospital Precinct - Sections

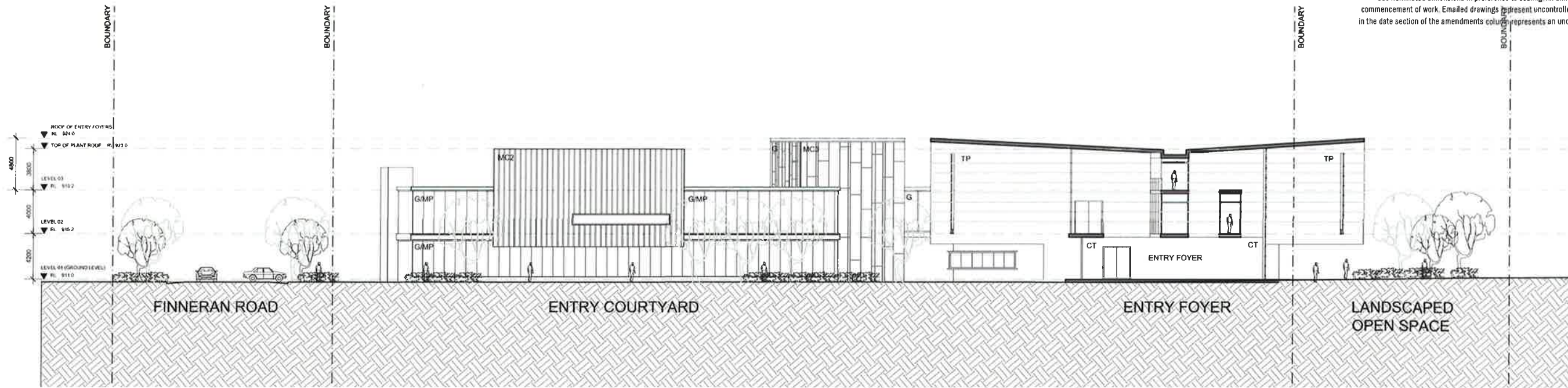
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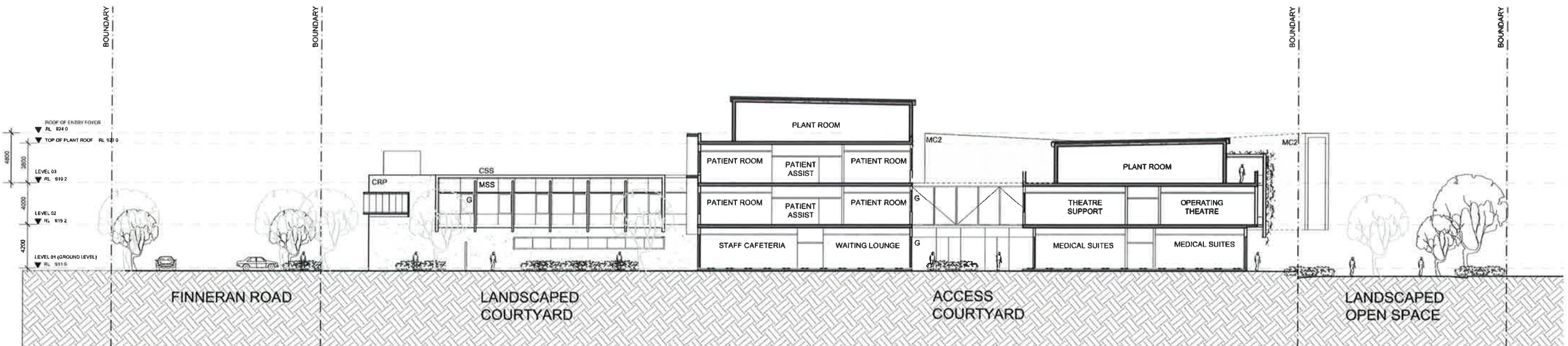
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Mar 2014

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1

Preliminary



SECTION C-C



SECTION D-D

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SG - STEEL FRAMED GRILL PANEL
PB - STEEL FRAMED PLANTER BOX (HIDDEN) AND CLIMBING PLANTS

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Forest Road, Orange
Drawing Title
Private Hospital Precinct - Sections

Revision	Description	Date	Initials
1	DA Issue	18.03.14	DH

Project Architect/Designer
M Urquhart
Scale @ A3
1 : 400
Drawing No.
5420.02.408

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BH
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Date Checked
Revision
1

Preliminary



3	DA Issue	18.03.14	DH
2	Preliminary Issue	27.02.14	MB
Revision	Description	Date	Initials

Drawing List

Sheet No.	Sheet Name	Rev	Date
01.000	Cover sheet	3	18.03.14
01.001	Location Plan	2	18.03.14
01.101	Site Master Concept Plan	10	18.03.14
01.102	Precinct 2 - Site Plan	7	18.03.14
01.103	Precinct 2 - Elevations & Sections	3	18.03.14
02.101	Health Facilities - Ground Floor	4	18.03.14
02.102	Health Facilities - Level 1	3	18.03.14
02.103	Health Facilities - Elevations & Sections	5	18.03.14
02.104	Health Facilities - Perspectives	1	18.03.14
02.201	Neighbourhood Shops - Ground Floor	5	18.03.14
02.202	Neighbourhood Shops - Elevations	4	18.03.14
02.203	Neighbourhood Shops - Perspectives	1	18.03.14
02.301	Motel - Ground Floor	4	18.03.14
02.302	Motel - First Floor	4	18.03.14
02.303	Motel - Elevations	3	18.03.14
02.304	Motel - Elevations & Section	3	18.03.14
02.305	Motel - Perspectives	1	18.03.14
06.001	GFA - Ground Level	5	18.03.14
06.002	GFA - Level 1	4	18.03.14

Legend

- A Health Facilities & Shops
- B Neighbourhood Shops
- C Motel

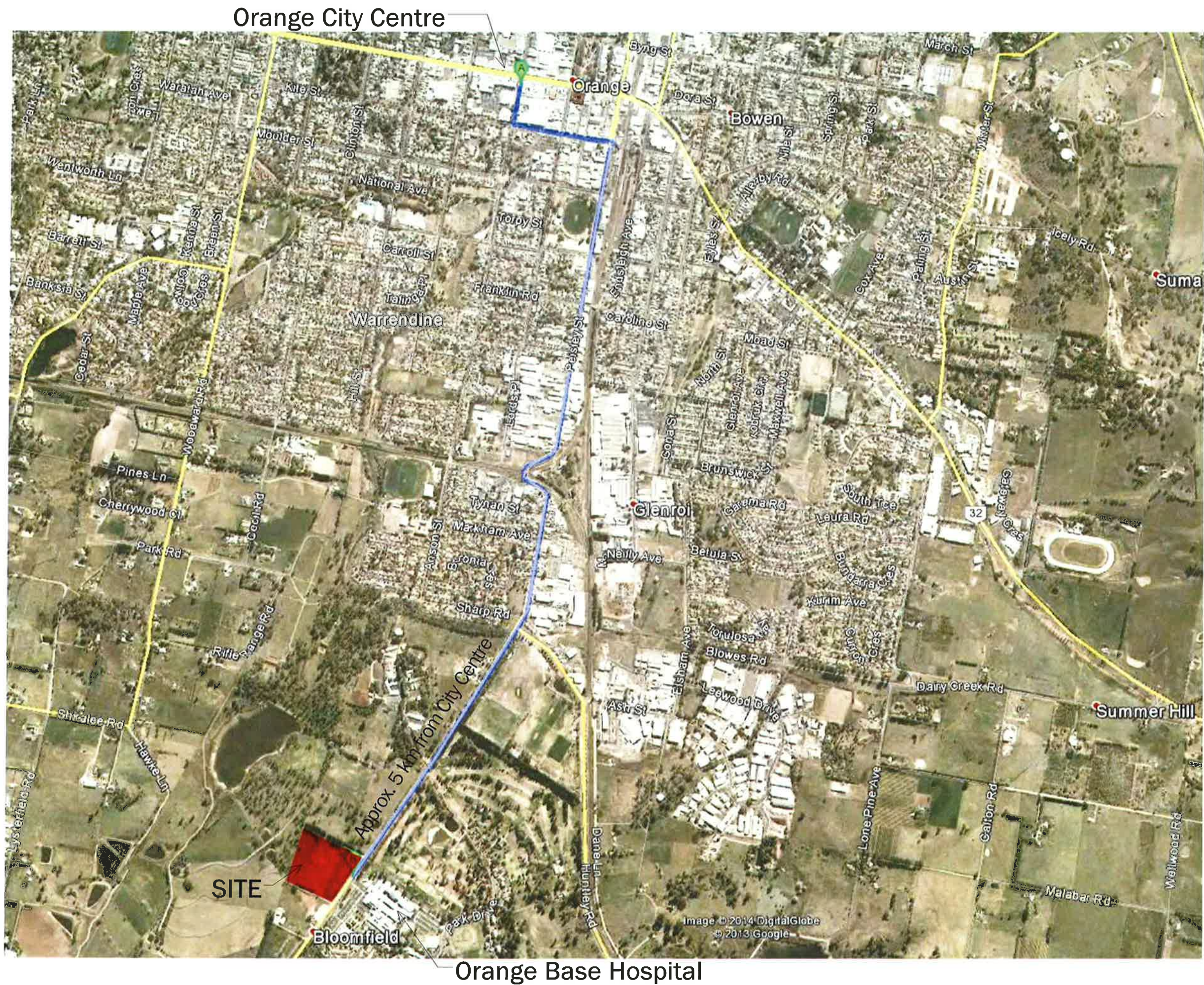
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Project
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Client
James Richmark Pty Ltd

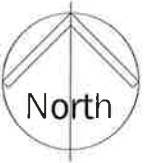
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Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	MB	Feb 14
Scale @ A3	Checked	Date Checked
As indicatedPJ		-
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Client
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Drawing Title
Location Plan

Project Architect/Designer
M Urquhart

Drawn By
DH

Date Drawn
Feb 14

Scale @ A3
1 : 1500

Checked
PJ

Date Checked

Drawing No.
5420.3S.01.001

Revision
2

Site Boundary

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12	Coordination Issue	11.04.14	DH
Revision	Description	Date	Initials

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Project
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Client
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Drawing Title
Site Master Concept Plan

Project Architect/Designer M Urquhart
Scale @ A3 1 : 1000
Drawing No. 5420.3S.01.101

Drawn By DH
Checked PJ
Revision 13

Date Drawn Aug 13
Date Checked .
Revision 13

1
02.103
Site Plan
1:1000



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7	DA Issue	18.03.14	DH

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Project
 Orange Private Hospital Site

Client
 James Richmark Pty Ltd

Drawing Title
 Precinct 2 - Site Plan

Project Architect/Designer M Urquhart
 Scale @ A3 1:500
 Drawing No. 5420.3S.01.102
 Drawn By DH
 Checked PJ
 Date Drawn Oct 13
 Date Checked
 Revision 8

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Scale (m)



1
101.004
Site Section
1:500



3
Site Elevation (East)
1:500

Note: This sheet has been re numbered from 5420.3S.01.003

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2	Preliminary Issue	27.02.14	MB
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Project
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Client
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Drawing Title
Precinct 2 - Elevations &
Sections

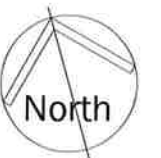
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M Urquhart	MB	Feb 14
Scale @ A3	Checked	Date Checked
1:500	PJ	-
Drawing No.	Revision	
5420.3S.01.103	3	

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Project
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Client
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Drawing Title
Health Facilities - Ground Floor

Project Architect/Designer M Urquhart
Scale @ A3 1 : 300
Drawing No. 5420.3S.02.101

Drawn By MB
Checked PJ
Date Drawn Feb 14
Date Checked

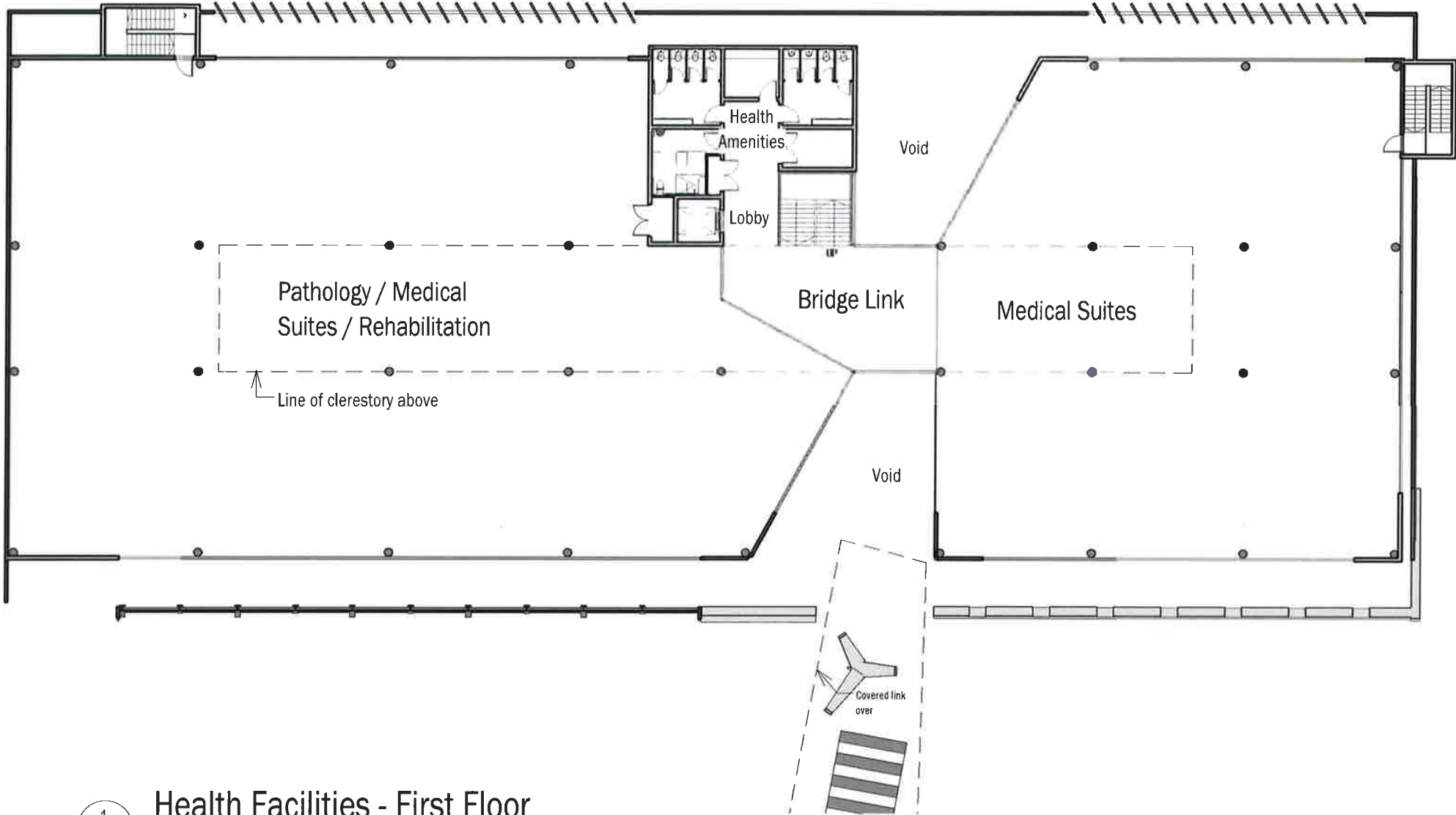
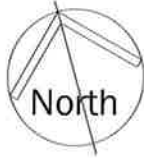
Revision 4

1
02.103
Health Facilities - Ground Floor
1:300

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17/03/2014 12:04:36 PM



Scale (m)



1
02.202
Health Facilities - First Floor
1 : 300

3	DA Issue	18.03.14	DH
2	Preliminary Issue	27.02.14	MB
Revision	Description	Date	Initials

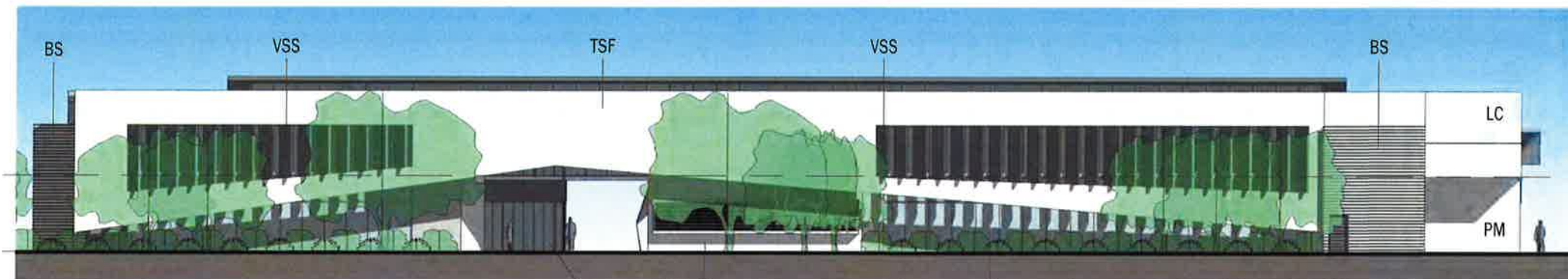
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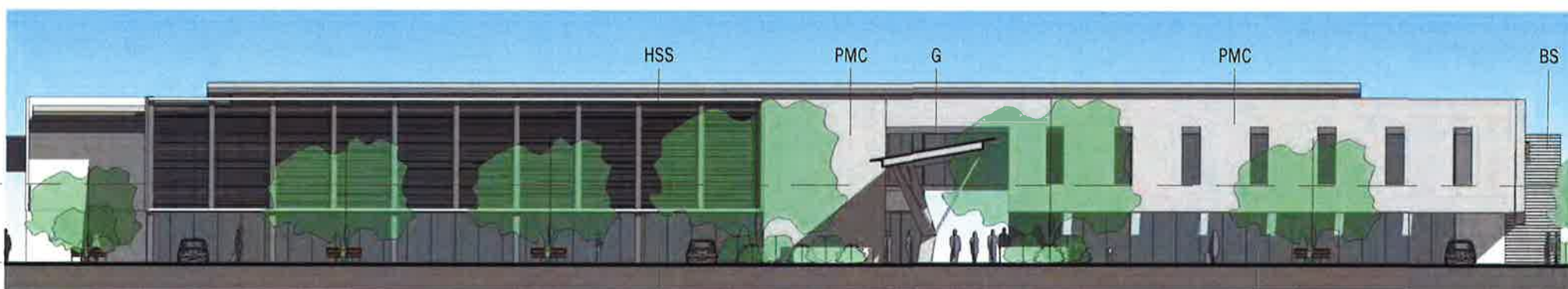
Client
James Richmark Pty Ltd

Drawing Title
Health Facilities - Level 1

Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	MB	Feb 14
Scale @ A3	Checked	Date Checked
1 : 300	PJ	-
Drawing No.	Revision	
5420.3S.02.102	3	



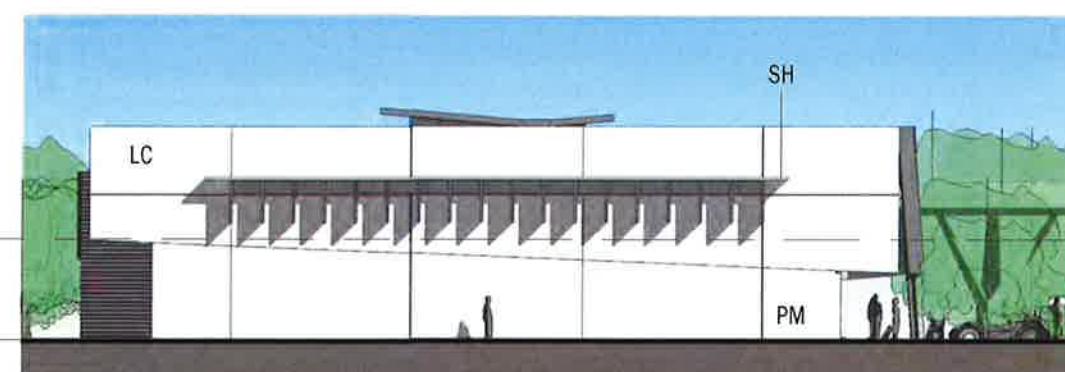
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1:300



2 Health Facilities - South Elevation
1:300



3 Health Facilities - East Elevation
1:300

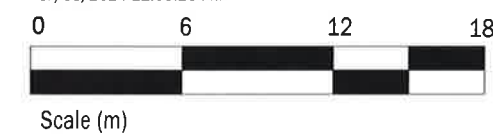


4 Health Facilities - West Elevation
1:300



5 Health Facilities - Section
1:300

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-Legend-

- BS Batten Screen
- G Glazing
- HSS Horizontal Sun Screen
- LC Lightweight Cladding
- PM Painted Masonry
- PMC Pre-coated Metal Cladding
- SH Sun Hood
- TC Textured Cladding
- TSF Translucent Cladding
- VSS Vertical Sun Screen

Note: This sheet has been re numbered from 5420.3S.04.101

5	DA Issue	18.03.14	DH
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Client
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Drawing Title
Health Facilities - Elevations
& Sections

Project Architect/Designer M Urquhart
Scale @ A3 1:300
Drawing No. 5420.3S.02.103

Drawn By MB
Checked PJ
Revision 5

Date Drawn Nov 13
Date Checked -



1 Health - Covered Walkway



2 Health - Shop Front (South Face)



3 Health - Perspective from South East



4 Health - Part Northern Perspective

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Revision	Description	Date	Initials
1	DA Issue	18.03.14	DH

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Project
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Client
James Richmark Pty Ltd

Drawing Title
Health Facilities -
Perspectives

Project Architect/Designer
M Urquhart
Scale @ A3
Checked
PJ

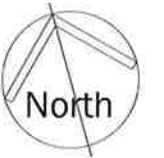
Drawn By
MB
Date Drawn
Feb 14
Date Checked

Drawing No.
5420.3S.02.104
Revision
1

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Scale (m)



5	DA Issue	18.03.14	DH
4	Preliminary Issue	27.02.14	MB
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Project
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Client
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Drawing Title
Neighbourhood Shops -
Ground Floor

Project Architect/Designer
M Urquhart
Scale @ A3
1 : 300
Drawing No.
5420.3S.02.201

Drawn By
DH
Checked
PJ
Revision
5

Date Drawn
Nov 13
Date Checked
-
Revision

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17/03/2014 12:08:01 PM



Scale (m)

-Legend-

BS	Batten Screen
G	Glazing
HSS	Horizontal Sun Screen
LC	Lightweight Cladding
PM	Painted Masonry
PMC	Pre-coated Metal Cladding
SH	Sun Hood
TC	Textured Cladding
TSF	Translucent Cladding
VSS	Vertical Sun Screen

Note: This sheet has been re numbered from 5420.3S.04.201

Revision	Description	Date	Initials
4	DA Issue	18.03.14	DH
3	Preliminary Issue	27.02.14	MB

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Client
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Drawing Title
Neighbourhood Shops - Elevations

Project Architect/Designer M Urquhart
Scale @ A3 1:300
Drawing No. 5420.3S.02.202

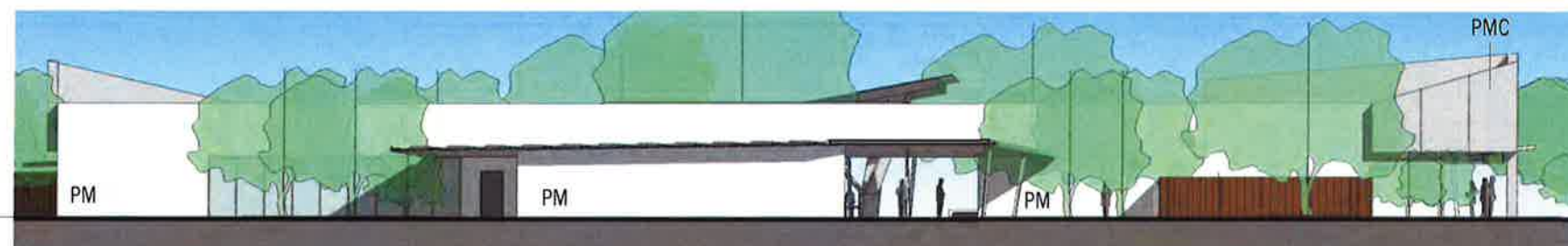
Drawn By DH
Checked PJ
Date Drawn Nov 13
Date Checked
Revision 4



Nbrhood Shops - Ground FFL
FFL912.000

Neighbourhood Shops - North Elevation

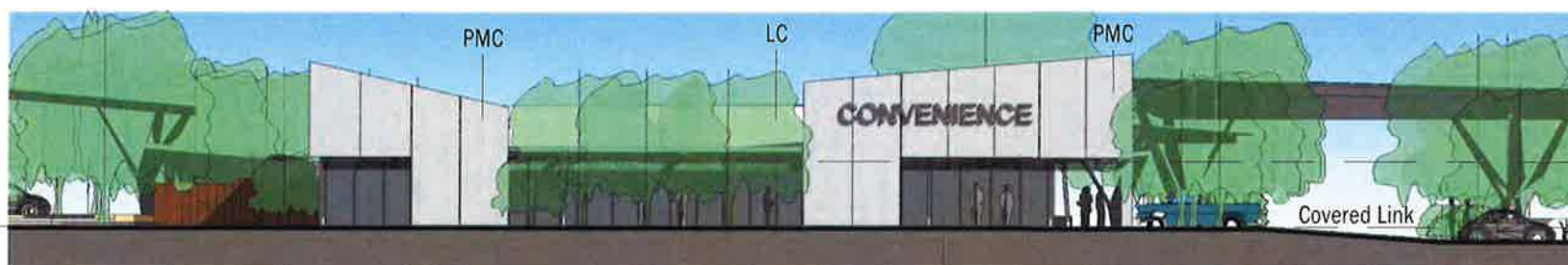
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Nbrhood Shops - Ground FFL
FFL912.000

Neighbourhood Shops - South Elevation

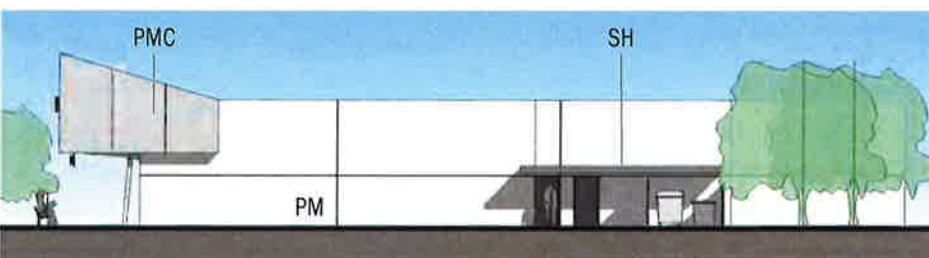
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Nbrhood Shops - Ceiling
FFL914.700
Nbrhood Shops - Ground FFL
FFL912.000

Neighbourhood Shops - East Elevation

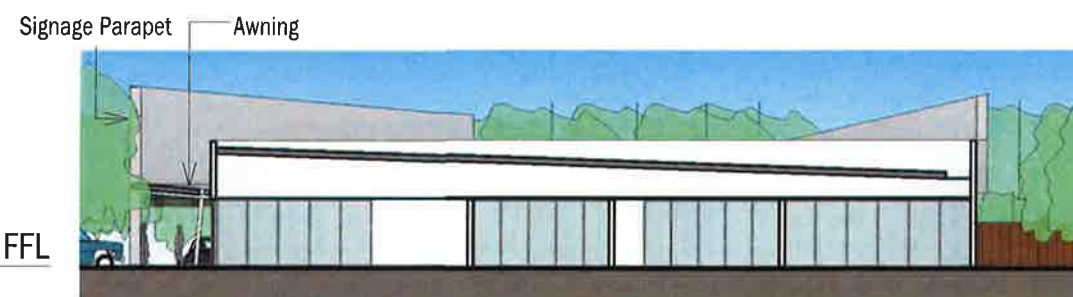
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Nbrhood Shops - Ground FFL
FFL912.000

Neighbourhood Shops - West Elevation

1:300



Neighbourhood Shops - Section

1:300



1 Neighbourhood Shops - North East Corner Perspective

2 Retail - Entrance (North)



3 Neighbourhood Shops - North West Corner Perspective

1	DA Issue	18.03.14	DH
Revision	Description	Date	Initials

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Client
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Drawing Title
Neighbourhood Shops -
Perspectives

Project Architect/Designer
M Urquhart
Scale @ A3
Checked
PJ

Drawn By
DH
Date Drawn
Feb 14
Date Checked
PJ

Drawing No.
5420.3S.02.203
Revision
1

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4	DA Issue	18.03.14	DH
5	DA Issue	16.04.14	DH



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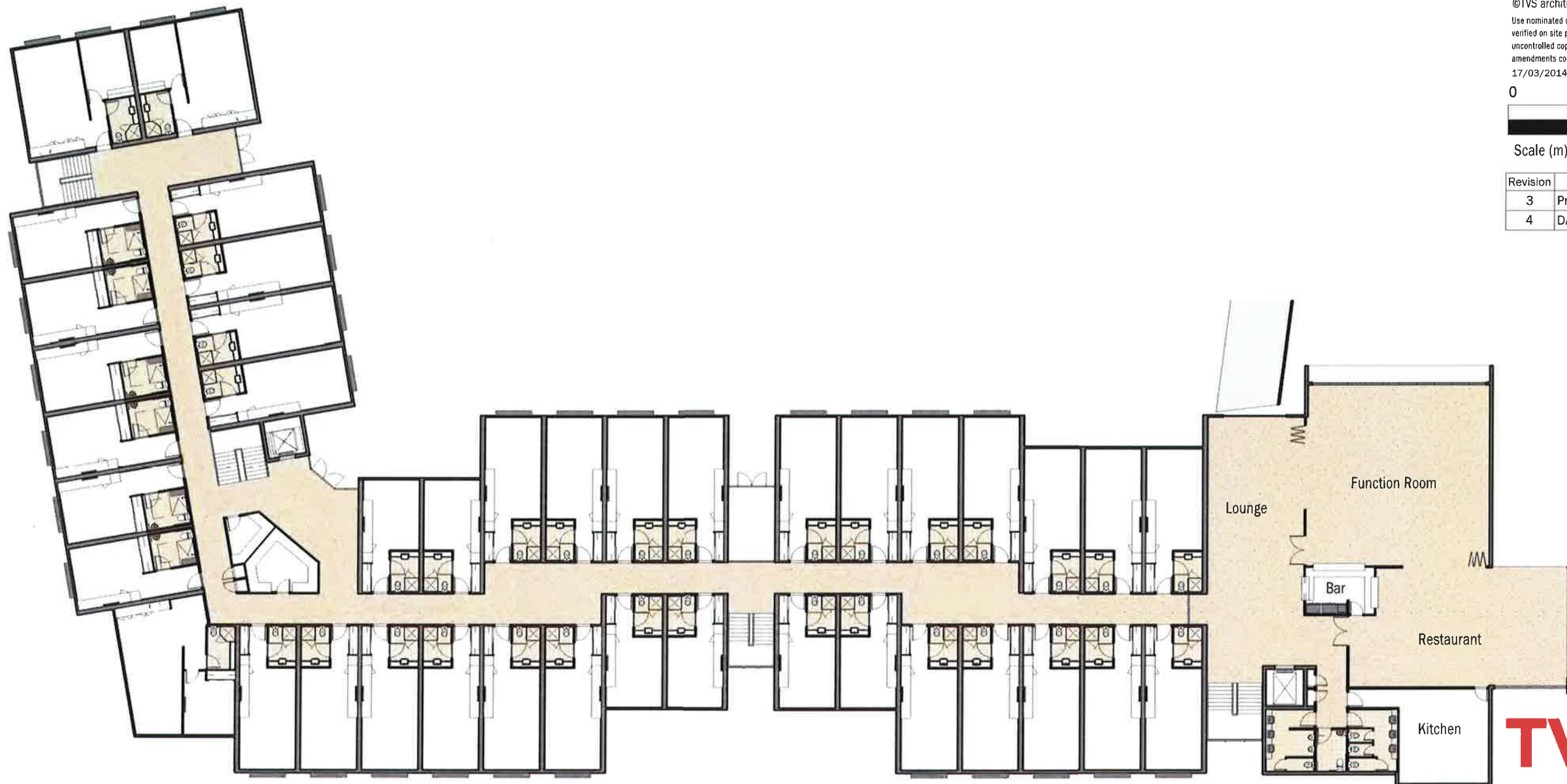
Project
Orange Private Hospital Site

1
02.103
Motel - Ground Floor Plan
1:300

Client
James Richmark Pty Ltd

Drawing Title
Motel - Ground Floor

Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	DH	Oct 13
Scale @ A3	Checked for Construction	Date Checked
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Drawing No.	Revision	
5420.3S.02.301	5	



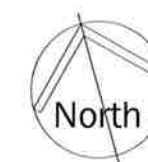
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Scale (m)

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Client
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Drawing Title
Motel - First Floor

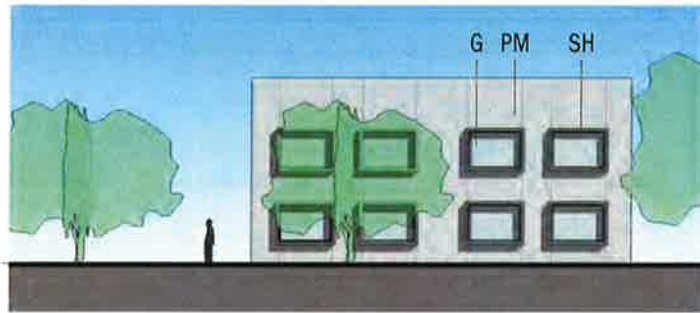
Project Architect/Designer Drawn By Date Drawn
M Urquhart DH Oct 13
Scale @ A3 Checked for Construction Date Checked
1 : 300 PJ -
Drawing No. Revision
5420.3S.02.302 4

1
02.202

Motel - First Floor Plan

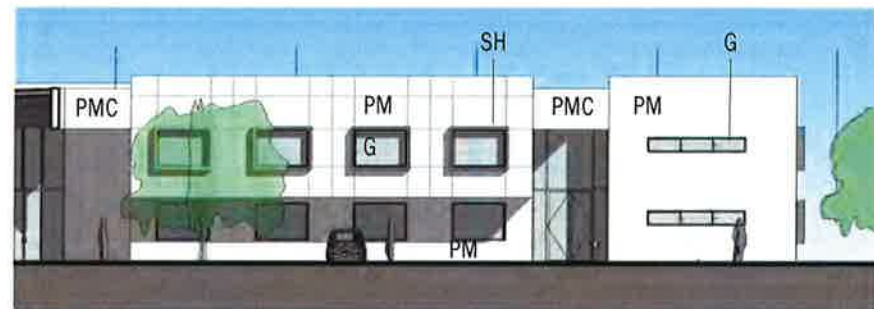
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Site Boundary

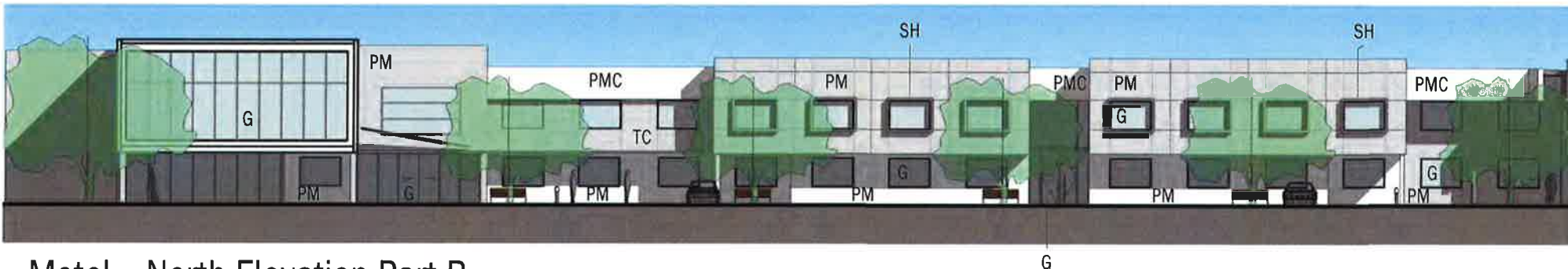


Motel - Ground FFL
FFL912.500

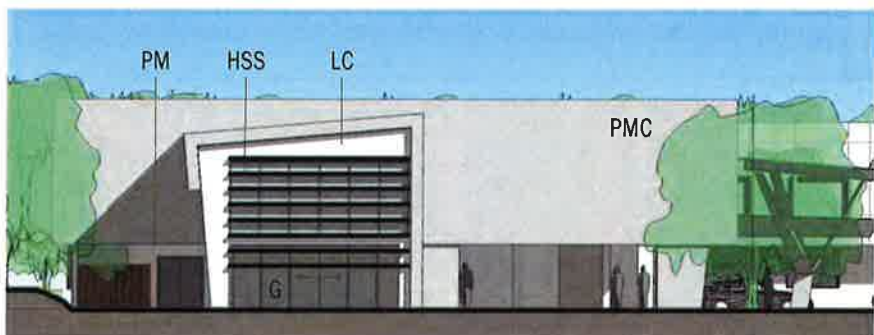
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02.303
Motel - North Elevation - Part A
1:300



2
02.303
Motel - East Elevation Part A
1:300

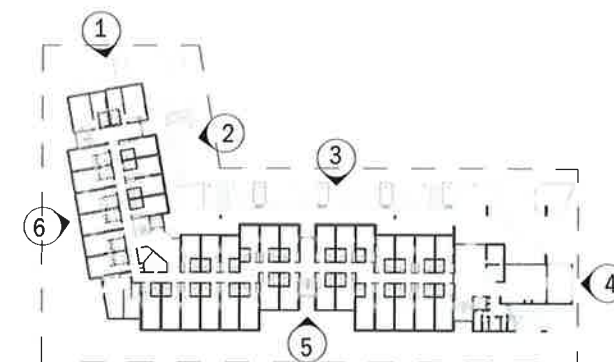


3
02.303
Motel - North Elevation Part B
1:300



Motel - Ground FFL
FFL912.500

4
02.303
Motel - East Elevation - Part B
1:300



Key Plan
1:1500 @ A3

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-Legend-

BS	Batten Screen
G	Glazing
HSS	Horizontal Sun Screen
LC	Lightweight Cladding
PM	Painted Masonry
PMC	Pre-coated Metal Cladding
SH	Sun Hood
TC	Textured Cladding
TSF	Translucent Cladding
VSS	Vertical Sun Screen

Note: This sheet has been re numbered from
5420.3S.04.301

Revision	Description	Date	Initials
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2	Preliminary Issue	27.02.14	MB

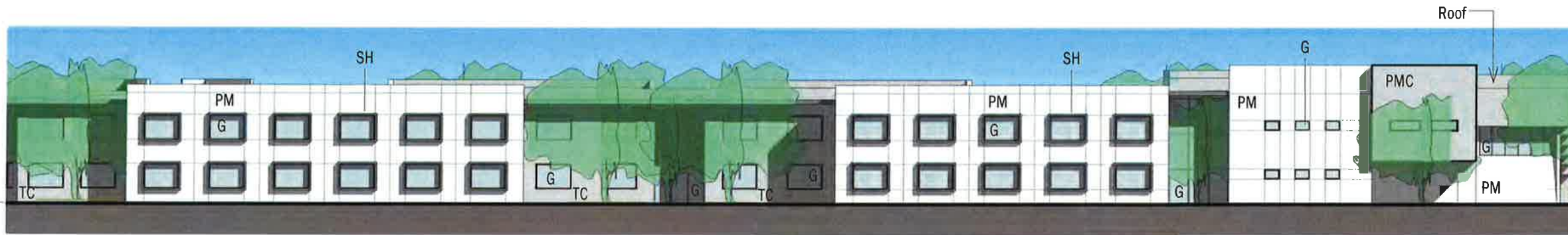
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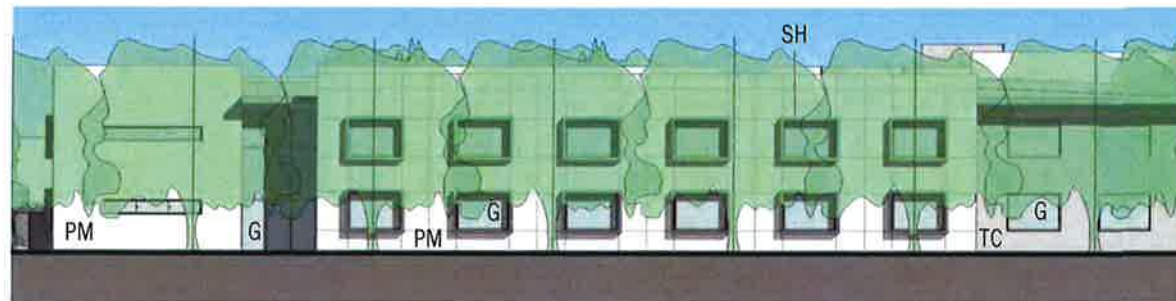
Client
James Richmark Pty Ltd

Drawing Title
Motel - Elevations

Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	DH	Feb 14
Scale @ A3	Checked	Date Checked
As indicated PJ		
Drawing No.	Revision	
5420.3S.02.303	3	



5 Motel - South Elevation
02.303 1:300



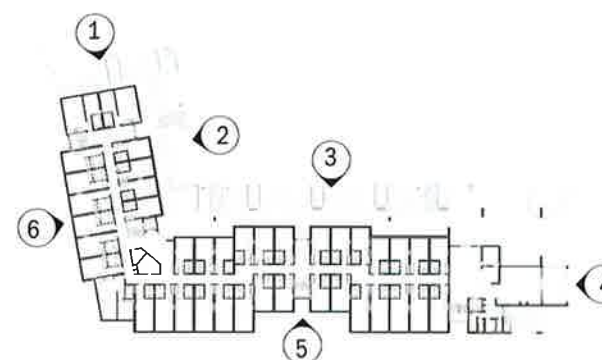
6 Motel - West Elevation
02.303 1:300



7 Motel - Section - Foyer
101.006 1:300



8 Motel - Section - Typical
101.006 1:300



Key Plan
1:1500 @ A3

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Scale (m)

Motel - Ground FFL
FFL912.500

-Legend-

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G	Glazing
HSS	Horizontal Sun Screen
LC	Lightweight Cladding
PM	Painted Masonry
PMC	Pre-coated Metal Cladding
SH	Sun Hood
TC	Textured Cladding
TSF	Translucent Cladding
VSS	Vertical Sun Screen

Note: This sheet has been re numbered from
5420.3S.04.302

Revision	Description	Date	Initials
3	DA Issue	18.03.14	DH
2	Preliminary Issue	27.02.14	MB

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Project
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Client
James Richmark Pty Ltd

Drawing Title
Motel - Elevations & Section

Project Architect/Designer M Urquhart	Drawn By DH	Date Drawn Feb 14
Scale @ A3 As indicated PJ	Checked	Date Checked
Drawing No. 5420.3S.02.304	Revision 3	



1 Motel - North East Perspective



2 Motel - Entrance



3 Motel - Part North Perspective

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Client
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Drawing Title
Motel - Perspectives

Project Architect/Designer M Urquhart
Drawn By DH
Date Drawn Feb 14
Scale @ A3 Checked PJ
Date Checked

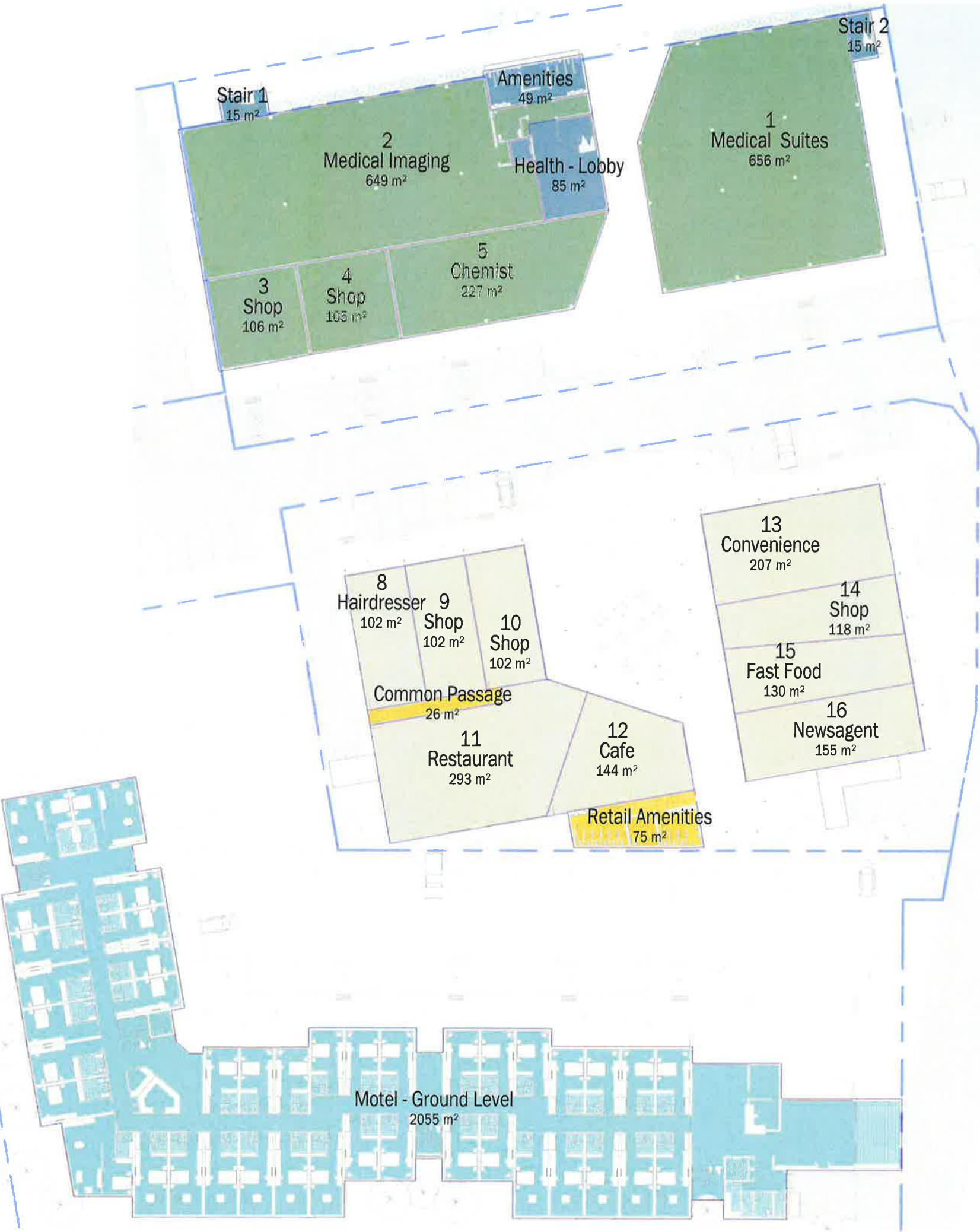
Drawing No. 5420.3S.02.305
Revision 1

GFA Schedule		
Building Type	Tenancy	GFA
Health		
1	Medical Suites	656 m ²
2	Medical Imaging	649 m ²
3	Shop	106 m ²
4	Shop	105 m ²
5	Chemist	227 m ²
6	Medical Suites	655 m ²
7	Pathology / Suites/ Rehab.	1102 m ²
Ground - Common Area		165 m ²
Level 1 - Common Area		180 m ²
		3845 m ²

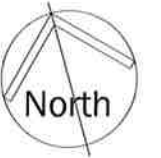
Motel		
	Motel - Ground Level	2055 m ²
	Motel - Level 1	2385 m ²
		4440 m ²

Retail		
8	Hairdresser	102 m ²
9	Shop	102 m ²
10	Shop	102 m ²
11	Restaurant	293 m ²
12	Cafe	144 m ²
13	Convenience	207 m ²
14	Shop	118 m ²
15	Fast Food	130 m ²
16	Newsagent	155 m ²
Ground - Common Area		100 m ²
		1453 m ²

Total Area Schedule	
Building Type	Area
Health	3845 m ²
Motel	4440 m ²
Retail	1453 m ²
Grand total	9739 m ²



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Health	Health
Health - Common Area	Health - Common Area
Motel	Motel
Retail	Retail
Retail - Common Area	Retail - Common Area

5	DA Issue	18.03.14	DH
4	Preliminary Issue	14.01.13	MB
Revision	Description	Date	Initials

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Project
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Client
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Drawing Title
GFA - Ground Level

Project Architect/Designer	Drawn By	Date Drawn
M Urquhart	DH	Dec 13
Scale @ A3	Checked	Date Checked
1 : 500	PJ	
Drawing No.	Revision	
5420.3S.06.001	5	

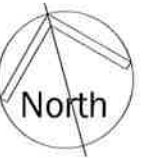


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Scale (m)



- Health
- Health - Common Area
- Motel
- Retail
- Retail - Common Area

4	DA Issue	18.03.14	DH
3	Preliminary Issue	14.01.13	MB
Revision	Description	Date	Initials

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Project
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Client
James Richmark Pty Ltd

Drawing Title
GFA - Level 1

Project Architect/Designer
M Urquhart
Scale @ A3
1 : 500
Drawing No.
5420.3S.06.002

Drawn By
DH
Checked
PJ
Revision
4

Date Drawn
Dec 13
Date Checked
Revision

Annexure B

Traffic and Parking Assessment by Terraffic Pty Ltd



ORANGE PRIVATE HOSPITAL DEVELOPMENT

FOREST ROAD, ORANGE

Traffic and Parking Assessment Report

22 April 2014

Ref: 14016

Prepared by

Terraffic Pty Ltd
Traffic and Parking Consultants

TABLE OF CONTENTS

1. INTRODUCTION	1
2. PROPOSED DEVELOPMENT	8
3. PARKING ASSESSMENT	11
4. SERVICING ASSESSMENT	17
5. TRAFFIC ASSESSMENT	21

APPENDICES

APPENDIX A	PLANS OF PROPOSED DEVELOPMENT PREPARED BY TVS ARCHITECTS
APPENDIX B	TRAFFIC COUNT DATA
APPENDIX C	SIDRA MOVEMENT SUMMARY SHEETS FOR THE FOREST ROAD/BASE HOSPITAL/SUBJECT SITE ACCESS DRIVEWAY
APPENDIX D	SIDRA MOVEMENT SUMMARY SHEETS FOR THE FOREST ROAD NORTHERN SITE EXIT DRIVEWAY

LIST OF ILLUSTRATIONS

FIGURE 1	LOCATION
FIGURE 2	SITE
FIGURE 3	FUTURE TRAFFIC SIGNAL CONTROL



1. INTRODUCTION

This report has been prepared to accompany a Section 75W Application to modify MP08_0232; MP08_0233; and MP07_0072 for the Orange Private Hospital development on the site known as No.3403, Lot 1, DP549856, Forest Road, Orange (Figures 1 and 2).

The site is located on the western side of Forest Road opposite the Orange Base Hospital. It has a frontage of approximately 260m to Forest Road and has an area of approximately 6 hectares. The site was formerly used as an outdoor drive-in cinema, but has been unused for approximately 11 years.

On the 23rd November 2008, the Minister for Planning approved Major Project No.07_0072 on the development site as follows:

Concept approval is granted only to carrying out the development solely within the concept plan area as described in the Environmental Assessment Report titled "Orange Private Hospital" prepared by JBA Urban Planning Consultants and dated February 2008 and the Preferred Project Report prepared by JBA Planning Consultants dated August 2008 (as amended). The concept plan includes 4 separate precincts:

- (1) Private hospital precinct*
- (2) Medi -Motel precinct*
- (3) Residential precinct*
- (4) Hostel precinct*

The development includes:

- a part two / part three-storey private hospital*
- an 80-room Medi-Motel and 9 consultation suites*
- short and medium-term accommodation for hospital staff and students*
- a future precinct for residential dwellings and or apartments*
- a future precinct for a lodge / hostel for patients and families of patients*
- future child-care facilities and cancer care facilities*
- a gymnasium*
- ancillary retail activities (florist, newsagent and cafe/restaurant)*



- internal roads and car parking, and
- landscaping
- a four way signalised intersection on Forest Road.

The “Site Master Concept Plan” prepared by TVS Architects for the proposed development is reproduced in the following pages. Table 1.1 shows a comparison between the approved and proposed developments. As can be seen, the development will retain the 4 precincts as per the approval and will comprise:

Precinct 1 – Hospital site

Precinct 2 – Neighbourhood shops, health facility and motel

Precinct 3 – 3 x residential flat buildings and a 100 space child care centre

Precinct 4 – 4 x residential flat buildings

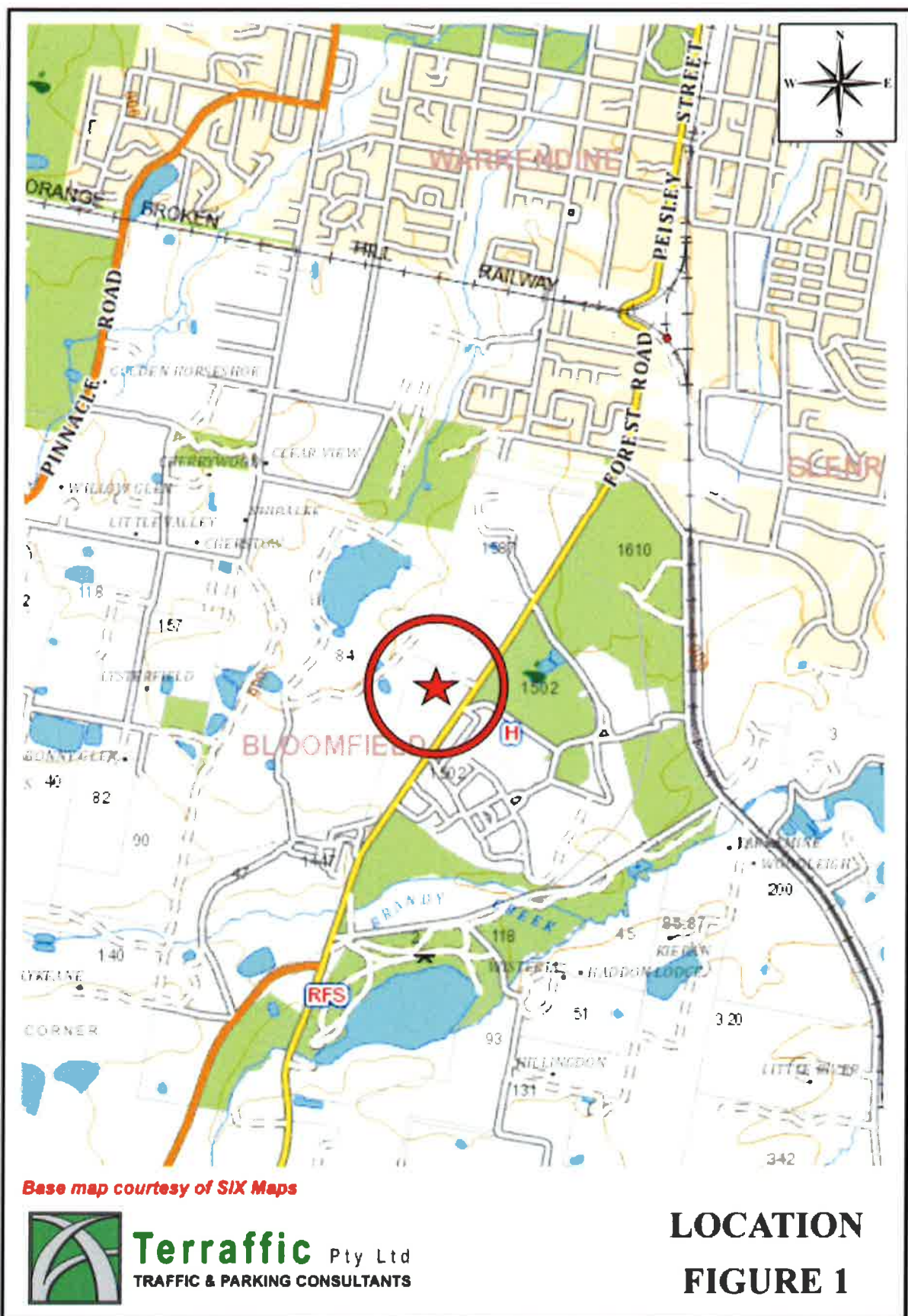
Plans of Precinct 1 and 2 are reproduced in Appendix A. Detailed plans of Precincts 3 and 4 have not been prepared for the DA Submission and are in concept only.

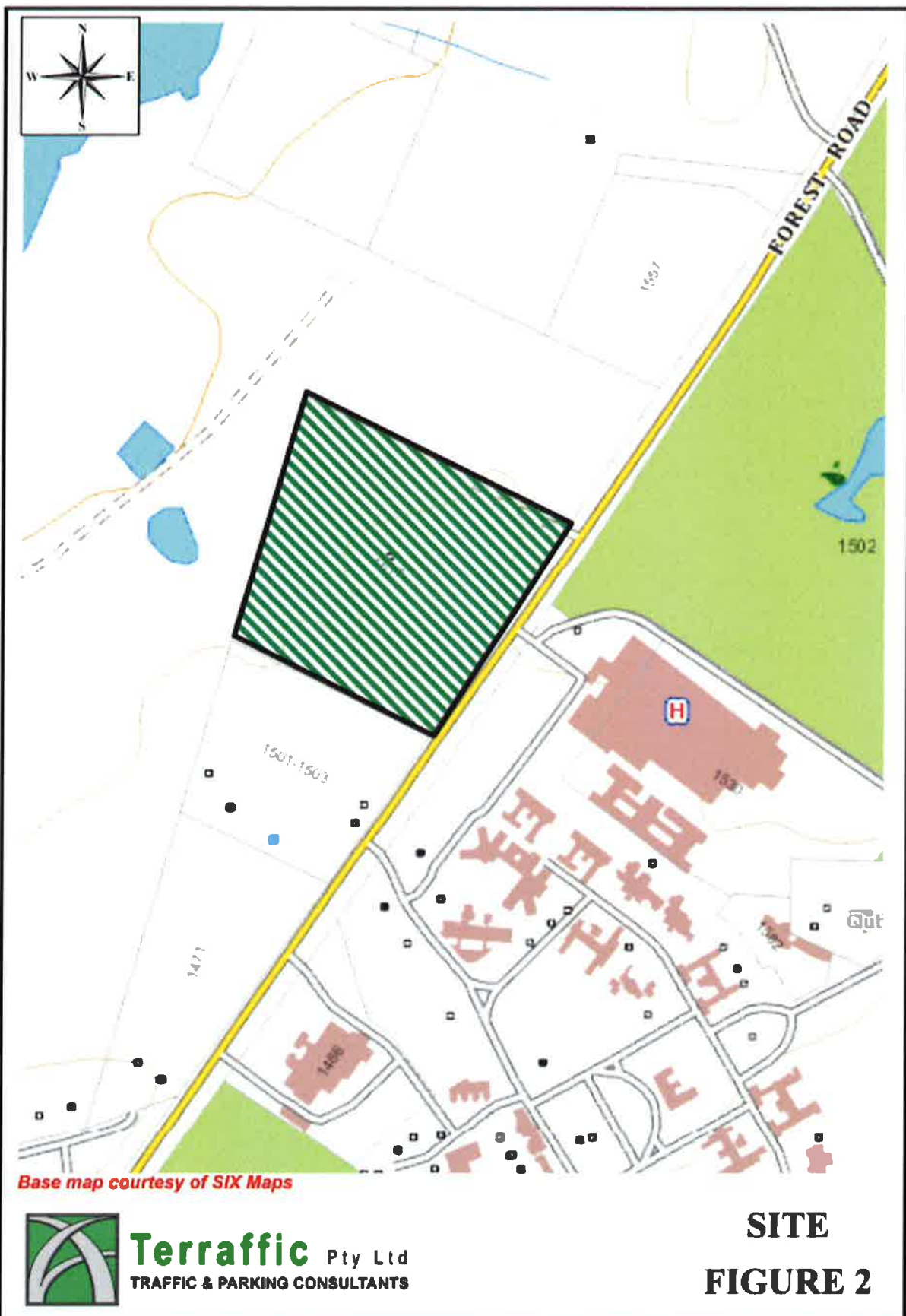
The development site is served by Route 537 which is operated by Orange Buslines. A copy of the Route 537 timetable is reproduced below. As can be seen, the buses currently operate on a daily basis between Orange City Centre and the main entrance to the Orange Hospital.

ROUTE 537		ORANGE HOSPITAL (to HOSPITAL)															
		Monday - Friday												Saturday			
Route		537 am	537 am	537 am	537 am	537 am	537 am	537 pm	537 pm	537 pm	537 pm	537 pm	537 pm	537 pm	537 pm	534* am	537 am
1	Orange Central/City Centre	6.40	8.00	8.45	9.40+	10.10	11.20	12.25	1.05	1.40+	2.10	2.40	3.10	4.55	5.15	9.00	11.00
5	Orange Railway Station	6.42	8.02	8.47	9.42	10.12	11.22	12.27	1.07	1.42	2.12	2.42		4.57	5.17		11.02
42	Forest Rd/Tynan St	6.45	8.05	8.50	9.45	10.15	11.25	12.30	1.10	1.45	2.15	2.45		5.00	5.20	9.08	11.05
71	Jack Brabham Park - Forest Road	6.46	8.06	8.51	9.46	10.16	11.26	12.31	1.11	1.46	2.16	2.46	3.35	5.01	5.21	9.09	11.06
70	Orange Hospital - Main Entrance	6.48	8.08	8.53	9.48	10.18	11.28	12.33	1.13	1.48	2.18	2.48	3.40	5.03	5.23	9.10	11.08

ROUTE 537		ORANGE HOSPITAL (from HOSPITAL)															
		Monday - Friday												Saturday			
Route		537 am	537 am	537 am	534 am	537 am	537 am	537 pm	537 pm	537 pm	537 pm	537 pm	537 pm	537 pm	537 pm	534 am	537 am
70	Orange Hospital - Main Entrance	7.03	7.35	8.08	8.53	9.48	10.20	11.30	12.35	1.15	1.50	2.20	2.50	3.40	5.07	5.25	6.15
71	Forest Rd/Sharp Rd	7.05	7.36	8.10	8.55	9.50	10.22	11.32	12.37	1.17	1.52	2.22	2.52	3.41	5.09	5.27	6.17
42	Forest Rd/Tynan St	7.06	7.37		8.56	9.51	10.23	11.33	12.38	1.18	1.53	2.23	2.53	3.42	5.10	5.28	6.18
5	Railway Station - opp Pelsley St	7.09	7.40			9.54	10.26	11.36	12.41	1.21	1.56	2.26	2.56	3.44	5.13	5.31	6.21
2	Canobolas Hotel - Summer St	7.11	7.42	8.16	9.08	9.56	10.28	11.38	12.43	1.23*	1.58	2.28	2.58	3.45	5.15	5.33	6.23

Orange Buslines timetable effective from 4th July 2013







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0 20 40 60
Scale 1:1000
Sea level



Note: This sheet has been re-titled and from
5420.35.01.001

13	04	Issue	16.04.14	Dr
12	04	Construction	11.04.14	Dr
11	04	Revision	11.04.14	Dr
10	04	Revision	11.04.14	Dr

TVS
architects

Project:
Orange Private Hospital Site

Client:

James Richmark Pty Ltd

Drawn by:

Site Master Concept Plan

Project Architect/Designer: Drawn By: Date Drawn:
M. Ullrich: Dr: Aug 13
Scale of 1:1000: Checked: Date Checked:
1:1000: PI: 5420.35.01.101
Drawing No: Revision No: 13



Site Plan
1:1000



Precinct 1	APPROVED DEVELOPMENT	PROPOSED DEVELOPMENT
Inpatient Unit	2,230m ² (60 beds)	4,500m ² (104 beds)
Administration Unit	325m ²	325m ²
General Support	680m ²	680m ²
Operating Unit (4 theatres)	1,715m ²	1,715m ²
Accident and Emergency	n/a	2,000m ²
Cancer Care Facility	1,400m ²	1,400m ²
Medical Imaging, pathology	2,000m ²	2,000m ²
Health Consulting Rooms	1,750m ²	n/a
Pharmacy	250m ²	n/a

Precinct 2	APPROVED DEVELOPMENT	PROPOSED DEVELOPMENT
Health Consulting Rooms	750m ²	1,311m ²
Rehabilitation Centre (Physio/chiro)	750m ²	700m ²
Medical Imaging, pathology	n/a	1,050m ²
Gym/Health and Fitness	800m ²	n/a
Retail shops	500m ² (4 shops)	1,498m ² (11 shops)
Restaurant	n/a	293m ² (150 seats)
Motel (Public Rooms)	60 rooms	70 rooms
Motel (Patient Suites)	20 rooms	12 rooms
Motel Restaurant	150m ² (100 seats)	130m ² (90 seats)
Motel Function Room	n/a	140m ²

Precinct 3	APPROVED DEVELOPMENT	PROPOSED DEVELOPMENT
Residential Flat Buildings	n/a	59 x 2 bedroom units
Child Care Centre	20 children	100 children
Hostel	100 x 1 bedroom units	n/a

Precinct 4	APPROVED DEVELOPMENT	PROPOSED DEVELOPMENT
Residential Flat Buildings	114 x 2 bedroom units	157 x 2 bedroom units

Table 1.1 – Comparison between approved and proposed developments



The purpose of this report is to assess the traffic and parking implications of the development proposal. To that end, this report:

- describes the site and provides details of the development proposal
- assesses the adequacy and suitability of the quantum of off-street carparking provided on the site
- assesses the adequacy and suitability of the loading facilities provided on the site
- reviews the geometric design features of the proposed carparking facilities for compliance with the relevant codes and standards
- reviews the road network in the vicinity of the site, and the traffic conditions on that road network
- estimates the traffic generation potential of the existing approved and proposed developments and assigns the additional traffic generation to the road network serving the site
- assesses the traffic implications of the development proposal in terms of road network capacity



2. PROPOSED DEVELOPMENT

As noted in the foregoing, the proposed development will retain the four approved precincts on the site and will comprise the following:

Precinct 1 (Hospital Site)

	GFA(m ²)	Staffing Levels	Hours operation
Inpatient Unit (104 rooms)	4500	30	24 hours
Administration Unit	325	10	7am-6pm
General Support	680	20	8am-6pm
Operating Unit (4 theatres)	1715	20	7am-6pm
Accident and Emergency	2000	10	8am-6pm
Cancer Care Facility	1400	20	8am-6pm
Medical Imaging, Pathology	2000	20	8am-6pm
Total Precinct 1 Hospital	12620	130	

Precinct 2 (Health Facilities, Neighbourhood Shops and Motel)

<u>Health Facilities</u>	GFA(m ²)	Staffing Levels	Hours operation
Health Consulting Rooms (8 Practitioners + 2 staff)	1311	10	8am-6pm
Rehabilitation Centre (8 Practitioners + 2 staff)	700	10	8am-6pm
Imaging and pathology (8 Practitioners + 10 staff)	1050	18	8am-6pm
Total Precinct 2 Health Facility	3061	38	

Shopping Centre

	GFA(m ²)	Staffing Levels	Hours operation
11 Retail Shops	1498	20	8am-5pm

Restaurant

	GFA(m ²)	Staffing Levels	Hours operation
Restaurant (150 seats - <u>dinner only</u>)	293	8	6pm - 11pm

Motel

	GFA(m ²)	Staffing Levels	Hours operation
70 Motel Rooms for Public (1 resident manager and 2 staff)	n/a	3	24 hours
12 Motel Rooms for Patients	n/a	n/a	24 hours
Motel Restaurant (90 seats generally guests only)	130	4	6am-10am, 6pm-11pm
Function Room (night time use only)	140	5	6pm - 11pm
Total Precinct 2 Motel	270	12	

Precinct 3 (Residential and Child Care Centre Site)

	GFA(m ²)	Staffing Levels	Hours operation
Approximately 59 x 2 bedroom apartments	n/a	n/a	n/a
100 space Child Care Centre	n/a	12	7am - 6pm
Total Precinct 3		12	

Precinct 4 (Residential Apartments)

	GFA(m ²)	Staffing Levels	Hours operation
Approximately 157 x 2 bedroom apartments	n/a	n/a	n/a
Total Precinct 4			

Table 2.1 – The Development Proposal



The proposed development will be served by a total of 617 off-street car parking spaces as follows:

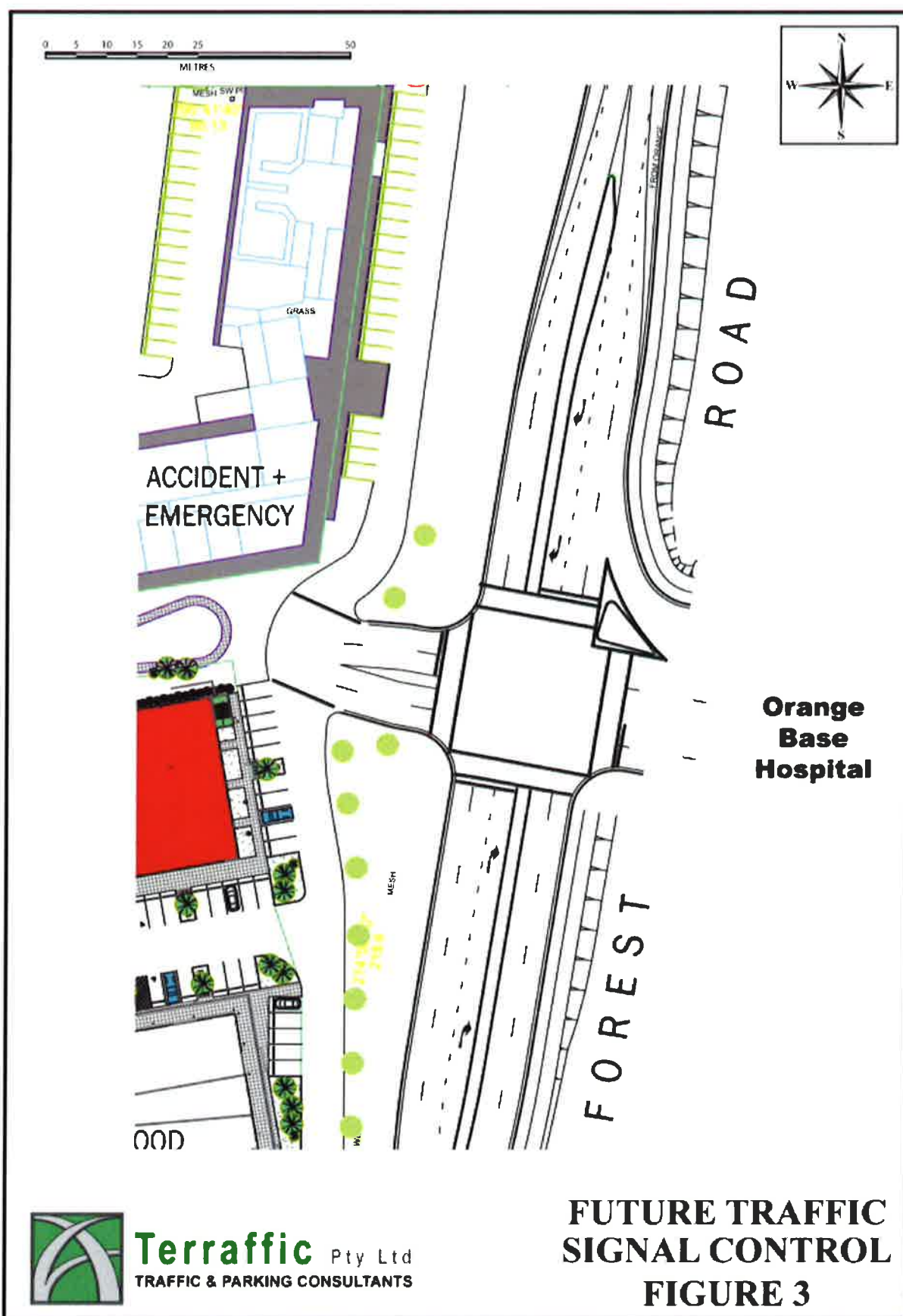
Precinct 1	101 spaces (including 6 disabled spaces) serving the hospital
Precinct 2	116 spaces (including 7 disabled spaces) serving the health facilities and retail shops 72 spaces serving the motel comprising 70 guest and 2 staff parking spaces. This parking provision includes 5 disabled guest spaces
Precinct 3	83 spaces serving the 59 residential units 25 spaces serving the 100 place Child Care Centre
Precinct 4	220 spaces serving the 157 residential units
Total	617 spaces

In addition to these spaces, there are 41 parallel parking spaces on the internal road network serving the site. Once these roads are constructed to provide access to Precincts 3 and 4, these on-street parking spaces will be available as surplus parking for the overall development.

The approved access arrangements serving the site off Forest Road will be retained and comprise the following:

- A new 4-way signalised intersection that will connect the site to the main access driveway serving the Orange Base Hospital which is located on the eastern side of Forest Road. The new signals will include pedestrian crossings on each leg of the intersection to enhance pedestrian safety.
- Left turn **exit only** movements from a northern access driveway

A plan of the main site access and future traffic signal controls is reproduced on Figure 3.





3. PARKING ASSESSMENT

Car Parking Requirements

Orange Development Control Plan 2004 (30 June 2007) specifies the following parking requirements which are relevant to the proposed development:

Hospital	1 space for every 3 beds + 1 space each resident doctor and 1 space for every 2 visiting doctor + 1 space for every 2 employees.
Health Consulting Rooms	2 spaces for every 1 practitioner with spaces being available for customer and staff use.
Motel	1 space per unit + 1 space for each resident manager + 1 space for every 2 employees + 1 space for every 3 seats in a restaurant + 1 space per 10m ² of entertainment or function room areas
Restaurants	1 space per 10m ² GFA or 1 space for every 3 seats, whichever is greater
Childcare Centre	1 space for every 4 children in attendance
Residential Units	1-bedroom unit - 1.0 space per unit 2-bedroom unit - 1.2 spaces per unit 3 (or more) bedroom unit - 1.5 spaces per unit Visitors - 0.2 spaces per unit
Shops and Shopping Centres	6.1 spaces per 100m ² GLFA

Dual and Complimentary Use of Parking

The Council's parking requirements do not consider the *Dual and Complimentary* use of parking that will occur on the site.

Dual use of parking spaces occurs when patrons of one component of a development also patronise another. For example, a proportion of staff and visitors to the proposed hospital and medical facility can also be expected to patronise the retail shops in Precinct 2. Not only will this reduce the parking requirements for the proposed medical facility, it also has the potential to reduce the traffic generating potential of visitors to the site.



In addition, another form of *Dual use* parking is that a proportion of residents in the Precinct 3 and 4 residential flat buildings will be employees of the health facilities and retail shops on the site and possibly the Orange Base Hospital. These residents will be allocated an on-site parking space within the residential precinct and will not require an additional car space in either Precinct 1 or 2. These residents will ultimately reduce the overall parking demand and traffic generation of the proposed development site.

Complementary use of parking spaces occurs when the peak parking demand of one component of a development does not coincide with the peak parking demand of another. In this case, the peak parking demand of the restaurant or the function centre in the motel is at night after 6pm, whereas the peak parking demand of the retail shops and health facilities in Precinct 2 is throughout the day.

Modifications to DCP Parking Requirements

When taking into account the dual and complimentary use of carparking, this assessment has slightly modified the following DCP parking requirements:

1. The parking requirement for the retail floorspace (6.1 spaces per 100m²) is excessive because it is unlikely that the general public will travel to the site to visit the café/flowerist/gift/newsagent stores. Furthermore, a high percentage of shoppers will already be on the site or will be generated by the Base Hospital on the eastern side of Forest Road (*dual use*). In these circumstances, the parking requirement for that retail floorspace will adopt the Council DCP rate of 4.1 spaces per 100m² which applies to shops in the CBD.
2. The parking requirement specified for the motel rooms (1 space per room) is excessive because 12 of the rooms will be medical suites occupied by persons who are bedridden. Accordingly, the requirement of 1 space per room has been applied to 70 standard rooms.
3. The parking requirement for the restaurant incorporated in Precinct 2 will only apply during the night when the restaurant will be operational. As noted in the foregoing,



this approach is applying the principle of *complementary use* of parking on the site. In addition, it is likely that some restaurant patrons will be staff/visitors to the hospital or guests of the motel who are already in the area. This includes staff and visitors to the Base Hospital on the eastern side of Forest Road. To that end, the restaurant will also be also adopting the *dual use* principle.

4. The parking requirement for the restaurant incorporated in the motel is excessive because a substantial proportion of restaurant patronage, if not all of it, will be drawn from motel guests and patients. In addition there will be a proportion of staff and visitors of the proposed hospital in Precinct 1 and the existing Base Hospital opposite the site. In these circumstances, the parking requirement for the motel restaurant (1 space per 3 seats) has been reduced to 20% of the requirement and will apply during the night only. These additional restaurant patrons who are not already in the area will utilise the remaining Precinct 2 parking spaces allocated to the health facility and retail shops that will be closed at that time (*complementary use*)
5. As per the restaurants on the site, the motel function room is expected to only operate at nights when the health facilities and retail shops are closed on Precinct 2. To that end, the parking requirement of 1 space per 10m² for the motel function room will apply at night time only. Furthermore, it is expected that some of the function guests will also be motel guests who have already parked on the site (*dual use*).
6. As the hospital in Precinct 1 will be operational 24 hours a day, this assessment has assumed that the nighttime workforce parking demand will be 20% of the daytime peak. As the visitor peak is typically at nights, the parking demand for the visitor component of the parking requirement will apply during the evening (ie 35 spaces).

In addition, this assessment has assumed that the proposed health facilities in Precinct 2 will have a total of 24 practitioners on-site at any given time. As can be appreciated, this is a conservative approach as specialists (such as surgeons and physiotherapists) tend to operate between hospitals and their office and may only be on-site on a given day.



Parking Calculations

Those parking requirements, with appropriate modifications, are applied to each component of the proposed development in Table 3.1 indicating a total parking requirement of up to 616 spaces during the daytime comprising:

DAYTIME PARKING REQUIREMENT

Precinct 1	Hospital	100 spaces
Precinct 2	Health Facility	55 spaces
	Retail Shops	61 spaces
	Motel	72 spaces
	<i>(Precinct 2 Sub-total</i>	<i>188 spaces)</i>
Precinct 3	Residential	83 spaces
	Child Care Centre	25 spaces
	<i>(Sub-total</i>	<i>108 spaces)</i>
Precinct 4	Residential	220 spaces
Total Requirement		616 spaces

Application of the modified parking requirements to take into account *dual and complementary use* of parking indicates a total parking requirement in Table 3.2 of up to 495 spaces during the nighttime comprising:

NIGHTTIME PARKING REQUIREMENT

Precinct 1	Hospital	48 spaces
Precinct 2	Restaurant	50 spaces
	Motel	72 spaces
	Motel restaurant	8 spaces
	Motel function room	14 spaces
	<i>(Precinct 2 Sub-total</i>	<i>144 spaces)</i>
Precinct 3	Residential	83 spaces
Precinct 4	Residential	220 spaces
Total Requirement		495 spaces

As noted in the foregoing, the proposed development makes provision for a total of 617 parking spaces, satisfying the likely daytime and nighttime demand of the proposed uses.



In addition, the development contains 41 parallel parking spaces on the internal road network serving Precincts 3 and 4. Once these precincts have been developed, these 41 spaces will be available as surplus parking for the overall development.

In the circumstances, it can be concluded that the parking provision incorporated in the development proposal is adequate such that the proposed development has no unacceptable parking implications.



Development Site Daytime Parking Requirements

Precinct 1 (Hospital Site)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Staff parking	Visitor parking
Inpatient Unit (104 rooms)	4500	30	1 space per 3 beds + 1 space per 2 employees and visiting doctors	50	0 15	35 0
Administration Unit	325	10	1 space per 2 employees	5	5	0
General Support	680	20	1 space per 2 employees	10	10	0
Operating Unit (4 theatres)	1715	20	1 space per 2 employees and visiting doctors	10	10	0
Accident and Emergency	2000	10	1 space per 2 employees and visiting doctors	5	5	0
Cancer Care Facility	1400	20	1 space per 2 employees and visiting doctors	10	10	0
Medical Imaging, Pathology	2000	20	1 space per 2 employees and visiting doctors	10	10	0
Total Precinct 1	12620	130		100	65	35
Precinct 2 (Health Facilities, Neighbourhood Shops and Motel)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Staff parking	Visitor parking
Health Facilities						
Health Consulting Rooms (8 Practitioners)	1311	8	2 spaces per 1 practitioner with space for customers	16	8	8
Consulting Room support staff		2	1 space per 2 employees	1	1	0
Rehabilitation Centre (8 practitioners)	700	8	2 spaces per 1 practitioner with space for customers	16	8	8
Rehabilitation Centre support staff		2	1 space per 2 employees	1	1	0
Imaging and pathology (8 Practitioners)	1050	8	2 spaces per 1 practitioner with space for customers	16	8	8
Imaging and pathology support staff		10	1 space per 2 employees	5	5	0
Total Precinct 2 Health Facility	3061	38		55	31	24
Shopping Centre						
11 Retail Shops	1498	20	4.1 spaces per 100m ²	61	10	51
Restaurant						
Restaurant (150 seats - dinner only)	293	8	not applicable	0	0	0
Motel						
70 Motel Rooms for Public			1 space per unit	70	0	70
12 Motel Rooms for Patients			not applicable	0	0	0
Motel staff (1 resident manager and 2 staff)		3	1 space per resident manager + 1 space per 2 employees	2	2	0
Motel Restaurant Patrons (90 seats)	130		not applicable	0	0	0
Motel Restaurant Staff		4	not applicable	0	0	0
Function Room	140	5	not applicable	0	0	0
Total Precinct 2 Motel	270	12		72	2	70
Total Precinct 2				188	43	145
Precinct 3 (Residential and Child Care Centre Site)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Resident /Staff parking	Visitor parking
Approximately 59 x 2 bedroom apartments			1,2 spaces per dwelling + 1 space per 5 for visitors	83	71	12
100 space Child Care Centre		12	1 space per 4 children (includes staff parking)	25	6	19
Total Precinct 3		12		108	77	31
Precinct 4 (Residential Apartments)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Resident parking	Visitor parking
Approximately 157 x 2 bedroom apartments			1,2 spaces per dwelling + 1 space per 5 for visitors	220	189	31
Total Precinct 4				220	189	31
TOTAL DEVELOPMENT DAYTIME ONLY				616	374	242

Table 3.1 – Daytime Parking Requirements



Development Site Nighttime Parking Requirements

Precinct 1 (Hospital Site)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Staff parking	Visitor parking
Inpatient Unit (104 rooms)	4500	30	1 space per 3 beds	38	0	35
			1 space per 2 employees and visiting doctors x 20%		3	0
Administration Unit	325	10	1 space per 2 employees x 20%	1	1	0
General Support	680	20	1 space per 2 employees x 20%	2	2	0
Operating Unit (4 theatres)	1715	20	1 space per 2 employees and visiting doctors x 20%	2	2	0
Accident and Emergency	2000	10	1 space per 2 employees and visiting doctors x 20%	1	1	0
Cancer Care Facility	1400	20	1 space per 2 employees and visiting doctors x 20%	2	2	0
Medical Imaging, Pathology	2000	20	1 space per 2 employees and visiting doctors x 20%	2	2	0
Total Precinct 1	12620	130		48	13	35
Precinct 2 (Health Facilities, Neighbourhood Shops and Motel)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Staff parking	Visitor parking
Health Facilities						
Health Consulting Rooms (5 Practitioners)	1311	0	not applicable	0	0	0
Consulting Room support staff		0	not applicable	0	0	0
Rehabilitation Centre (8 practitioners)	700	0	not applicable	0	0	0
Rehabilitation Centre support staff		0	not applicable	0	0	0
Imaging and pathology (8 Practitioners)	1050	0	not applicable	0	0	0
Imaging and pathology support staff		0	not applicable	0	0	0
Total Precinct 2 Health Facility	3061	0		0	0	0
Shopping Centre						
11 Retail Shops	1498	0	not applicable	0	0	0
Restaurant						
Restaurant (150 seats - dinner only)	293	8	1 space per 10m ² or 1 space per 3 seats	50	4	46
Motel						
70 Motel Rooms for Public			1 space per unit	70	0	70
12 Motel Rooms for Patients			not applicable	0	0	0
Motel staff (1 resident manager and 2 staff)		3	1 space per resident manager + 1 space per 2 employees	2	2	0
Motel Restaurant Patrons (90 seats)	130		1 space per 3 seats x 20%	6	0	6
Motel Restaurant Staff		4	1 space per 2 employees	2	2	0
Function Room	140	5	1 space per 10m ²	14	2	12
Total Precinct 2 Motel	270	12		94	6	88
Total Precinct 2				144	10	134
Precinct 3 (Residential and Child Care Centre Site)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Resident /Staff parking	Visitor parking
Approximately 59 x 2 bedroom apartments			1,2 spaces per dwelling + 1 space per 5 for visitors	83	71	12
100 space Child Care Centre		0	not applicable	0	0	0
Total Precinct 3		12		83	71	12
Precinct 4 (Residential Apartments)	GFA(m ²)	Staffing Levels	Parking Requirement	Req'd spaces	Resident parking	Visitor parking
Approximately 157 x 2 bedroom apartments			1,2 spaces per dwelling + 1 space per 5 for visitors	220	189	31
Total Precinct 4				220	189	31
TOTAL DEVELOPMENT DAYTIME ONLY				495	279	166

Table 3.2 – Nighttime Parking Requirements



4. SERVICING ASSESSMENT

The Australian Standards AS/NZS2890.1:2004 - "*Off-street car parking facilities*" and AS2890.2:2002 - "*Off-street commercial vehicle facilities*" identify the following categories of trucks and delivery vehicles:

Standard	Vehicle class	Length	Width	Clearance height	Design turning radius
AS2890.1	B99 courier van	5.2m	1.94m	2.2m	6.3m
AS2890.2	Small Rigid Vehicle (SRV)	6.4m	2.3m	3.5m	7.1m
AS2890.2	Medium Rigid Vehicle (MRV)	8.8m	2.5m	4.5m	10.0m
AS2890.2	Heavy Rigid Vehicle (HRV)	12.5m	2.5m	4.5m	12.5m
AS2890.2	Articulated Vehicle (AV)	19.0m	2.5m	4.5m	12.5m

Precinct 1: Private Hospital Deliveries

The proposed development will retain the 2 loading docks that were approved to serve the hospital development. The loading docks are required to accommodate the commercial vehicles transporting food, linen, medical supplies, office equipment and provisions to the hospital, and transporting waste from the hospital.

The vehicles used in those activities are expected to comprise predominantly vans and the SRV, with some deliveries via the MRV and the HRV. Articulated vehicles (AV) are not expected to be involved in deliveries to/from the site.

While the number of commercial vehicle trips generated by the Private Hospital will be variable, it is expected that the maximum daily commercial vehicle generation will be in the order of 20 vehicles per day (40 vehicle trips per day) as follows:



Precinct 1 (Hospital Site)	GFA(m²)	Hours operation	Deliveries per day	Deliveries per week
Inpatient Unit (104 rooms)	4500	24 hours	4 x vans/SRVs + 4 MRV/HRV's	28 x vans/SRVs + 28 MRV/HRV's
Administration Unit	325	7am-6pm	1 courier van	5 courier vans
Operating Unit (4 theatres)	1715	7am-6pm	4 courier vans	28 courier vans
Accident and Emergency	2000	8am-6pm	4 courier vans/SRV's	28 courier vans
Cancer Care Facility	1400	8am-6pm	1 courier van	5 courier vans
Medical Imaging, Pathology	2000	8am-6pm	2 courier van	10 courier vans
Total Precinct 1 Hospital	11940		20 deliveries	132 deliveries

All delivery vehicles accessing the hospital precinct and loading docks will enter via the Forest Road traffic signals. The ability of the heavy rigid vehicle to access the site and manoeuvre to/from the loading dock has been tested using the AutoTrack computer simulation software. This test found that the HRV can comfortably access the site and the loading docks incorporated in the proposed Private Hospital.

Precinct 2: Health Facilities, Neighbourhood Shops and Motel Deliveries

The proposed health facility is not served by a dedicated loading dock like the hospital in Precinct 1. Couriers making deliveries to this facility in vans will utilise the off-street parking area serving this component of the Precinct 2 development. Deliveries by larger commercial vehicles are not expected to occur for the health facility.

The neighbourhood shopping area contains 2 separate loading areas capable of accommodating Medium Rigid Vehicles. The loading docks are located at the eastern and western ends of the development and will provide convenient access to each shop.

The motel will also be served by a loading area capable of accommodating the Medium Rigid Vehicle. The motel is expected to generate commercial vehicles transporting predominantly food, liquor, linen and other provisions to the motel, and transporting waste from the motel. The vehicles used in those activities are expected to comprise predominantly vans, with some deliveries using the SRV and the MRV.



While the number of commercial vehicle trips generated by the uses in Precinct 2 will be variable, it is expected that the maximum daily commercial vehicle generation will be in the order of 30 vehicles per day (60 vehicle trips per day) as follows:

Precinct 2 (Health Facilities, Neighbourhood Shops and Motel)	GFA(m²)	Hours operation	Deliveries per day	Deliveries per week
<u>Health Facilities</u>				
Health Consulting Rooms	1311	8am-6pm	1 courier van	5 courier vans
Rehabilitation Centre	700	8am-6pm	1 courier van	5 courier vans
Imaging and pathology	1050	8am-6pm	1 courier van	5 courier vans
Total Precinct 2 Health Facility	3061		3 deliveries	15 deliveries
<u>Shopping Centre</u>				
11 Retail Shops	1498	8am-5pm	20 x courier vans/SRV's	100 x courier vans/SRV's
<u>Restaurant</u>				
Restaurant	293	6pm - 11pm	2 courier vans/SRV's	10 courier vans/SRV's
<u>Motel</u>				
Motel Rooms	na	24 hours	1 courier van/SRV	5 courier vans/SRV's
Motel Restaurant	100	6am-10am, 6pm-11pm	2 courier van/SRV's	10 courier vans/SRV's
Function Room	140	6pm - 11pm	1 courier van	5 courier vans
Total Precinct 2 Motel	240		4 deliveries	20 deliveries

The ability of medium rigid vehicles to access the site and manoeuvre to/from the loading areas serving each use in Precinct 2 has also been tested using the AutoTrack computer simulation software. This test found that the MRV can comfortably access the site and the loading areas.

Precinct 3: Child Care Centre and Residential Unit Deliveries

Deliveries to the childcare centre are expected to be only minor, comprising 3 to 4 vehicle trips per day in vans or SRV's. Childcare centres typically avoid deliveries during peak child set-down/pick-up periods and utilise the empty carpark outside of the AM and PM peak periods.



Precinct 3 and 4: Residential Unit Deliveries

While the residential component of the proposed development will generate commercial vehicle activity associated with residents moving into and out of the precinct, and some service vehicle activity for residents living there, this commercial vehicle activity will not be frequent nor regular, or of a level that is likely to have any significant traffic implications.

Based on these assumptions, the overall development will generate in the order of 45 to 50 deliveries per day ranging from courier vans to Heavy Rigid Vehicles. This level of activity equates to no more than 10 deliveries to the site during the morning and evening peak periods. This commercial vehicle activity will not be of a level that is likely to have any significant traffic implications.

In the circumstances, the proposed medical facility will have no unacceptable servicing implications.



5. TRAFFIC ASSESSMENT

Existing Road Network

Forest Rd is classified by the RMS as a Regional Road performing a sub-arterial road function. The section of Forest Rd in the vicinity of the site is constructed to a two-lane rural road standard with a sealed carriageway approximately 6.5m wide between edge lines with a 500mm bitumen shoulder on each side.

As can be seen in the aerial photograph below, the main access driveway serving the Orange Base Hospital opposite the subject site is controlled by a sea-gull treatment. Traffic making a right turn into the site are within a protected lane and do not obstruct northbound traffic. Vehicles making a right turn from the Hospital turn into a protected merge lane.



Aerial photograph of existing Base Hospital access arrangements

Orange City Council has engaged GHD to prepare roadwork plans for the upgrade of Forest Road and in particular the provision of traffic signals at the intersection of Forest Road, the Base Hospital and the subject site. The plan of these proposed access arrangements was reproduced earlier in this report on Figure 3.



Existing Traffic Conditions

An indication of the existing traffic conditions on the road network in the vicinity of the site is provided by peak period traffic surveys undertaken at the intersection of Forest Road and Base Hospital main access driveway between 7.00-9.00am and 4.00-6.00pm on Friday 7th march 2014. The results of the traffic surveys are reproduced in full in Appendix B and reveal that:

- the morning peak period occurs between 7.45-8.45am. At that time, the traffic flow on Forest Road to the north of the access driveway was 792 vehicles per hour (vph), while to the south of the driveway there were only 474vph
- during the morning peak, the Base Hospital generates in the order of 352vph with 95% of that traffic approaching from the north and departing to the north
- the evening peak period occurs between 4.15-5.15pm. At that time, the traffic flow on Forest Road to the north of the access driveway was 845vph, while to the south of the driveway there were 542vph
- during the evening peak period, the Base Hospital generates in the order of 317vph with 95% of that traffic approaching from the north and 99% departing to the north

Traffic Generating Potential of Proposed Medical Facility

Section 3.11 of the RMS publication “*Guide to Traffic Generating Developments*” (October 2002) specifies the following traffic generating rates that can apply to the proposed development:

Private Hospitals

$$\text{AM Peak Period} = -10.21 + 0.47 B + 0.06 \text{ ASDS}$$

$$\text{PM Peak Period} = -2.84 + 0.25 B + 0.40 \text{ ASDS}$$

Where:



B - No of beds

ASDS - Average number of staff per weekday shift.

Specialty Shops

Evening Peak Periods	4.6vtph per 100m ²
----------------------	-------------------------------

Restaurant

Evening peak hour vehicle trips	5vtph per 100m ²
---------------------------------	-----------------------------

Motel

Evening peak hour vehicle trips	0.4 per unit
---------------------------------	--------------

Child Care Centre

AM peak period	0.8 vtph per child
----------------	--------------------

PM peak period	0.7 vtph per child
----------------	--------------------

Residential Flat Buildings (up to 2 bedroom units)

Peak periods	0.4-0.5vtph per unit
--------------	----------------------

As the Guidelines do not provide a traffic generation rate for the professional consulting rooms, the rehabilitation centre and the imaging and pathology services, this assessment will adopt a “*first principles*” approach to determine an appropriate traffic generation for these components of the Precinct 2 “Health Facilities”.

The “*first principles*” approach will be based on the following:

1. The number of spaces allocated to long-term (staff) parking
2. The number of spaces allocated to short-term (patient/visitor) parking
3. The number of consulting rooms or doctors/professionals
4. An appropriate patient turnover for each doctor/professional per hour
5. An appropriate car driver rate for visitors/patients

Traffic Generation of Precinct 2 Health Facilities

The parking requirements for the Precinct 2 Health Facilities calculated in Chapter 3 of this report comprise the following:



Consulting Rooms (8 practitioners and 2 staff)	9 staff spaces plus 8 visitor spaces
Rehabilitation Centre (8 practitioners and 2 staff)	9 staff spaces plus 8 visitor spaces
Imaging/Pathology (8 practitioners and 10 staff)	13 staff spaces plus 8 visitor spaces
Total Requirement (21 practitioners and 14 staff)	31 staff spaces plus 24 visitor spaces

While the centre is open between 8.00am and 6.00pm, it is unlikely that all practitioners will start at 8.00am and finish at 6.00pm. Furthermore, it is unlikely that all 24 specialists (such as surgeons and physiotherapists) will be on-site at any given time as they tend to operate between hospitals and their office. However, in order to prepare a robust assessment, it has been assumed that all practitioners and staff will arrive during the morning peak and depart during the evening peak. To that end, there will be 31 staff vehicles entering the site in the morning peak and 31 staff vehicles departing during the evening peak.

As can be appreciated, patients visiting a specialist are required to make an appointment which evenly distributes their traffic and parking generation throughout the day. In addition, a proportion of these visitors will utilise the local bus service that services the site. Notwithstanding this assessment will assume that:

- each practitioner will have a continual patient turnover of 1 visitor every 30 minutes, i.e. 2 patients per hour
- each visitor will drive to the site

As there are 24 practitioners, the visitor traffic generation will be as follows:

AM Peak	48 inbound vehicle trips + 24 outbound vehicle trips
PM Peak	24 inbound vehicle trips + 48 outbound vehicle trips

To that end, the proposed health facilities will have the following peak period traffic generating characteristics:

AM Peak	72 inbound + 24 outbound = 96 vehicle trips
PM Peak	24 inbound + 72 outbound = 96 vehicle trips



Traffic Generation of Remaining Components

Application of the abovementioned RMS traffic generating rates to the remaining components of the proposed development yields the following:

Precinct 1:

Private Hospital (with 104 beds and 130 staff per weekday shift)

AM Peak Period = $-10.21 + (0.47 \times 104 \text{ beds}) + (0.06 \times 130 \text{ staff}) = 46\text{vtph}$

PM Peak Period = $-2.84 + (0.25 \times 104 \text{ beds}) + (0.40 \times 130 \text{ staff}) = 75\text{vtph}$

Precinct 2:

Specialty Shops

AM Peak Period 1,498m² @ 4.6vtph per 100m² = 69vtph

PM Peak Period 1,498m² @ 4.6vtph per 100m² = 69vtph

Restaurant

AM Peak Period closed

PM Peak Period 293m² @ 5vtph per 100m² = 15vtph

Motel (does not include patient rooms)

AM Peak Period = 70 motel rooms @ 0.4vtph per room = 28vtph

PM Peak Period = 70 motel rooms @ 0.4vtph per room = 28vtph

Motel Restaurant

AM Peak Period closed

PM Peak Period 130m² @ 5vtph per 100m² = 7vtph

Precinct 3:

Child Care Centre

AM Peak Period 100 children @ 0.8vtph per child = 80vtph

PM Peak Period 100 children @ 0.7vtph per child = 70vtph

Residential

AM Peak Period 59 units @ 0.4vtph per unit = 24vtph

PM Peak Period 59 units @ 0.4vtph per unit = 24vtph

**Precinct 4:****Residential**

AM Peak Period 157 units @ 0.4vtph per unit = 63vtph

PM Peak Period 157 units @ 0.4vtph per unit = 63vtph

Based on the RMS generic traffic generation rates and first principle assumptions, the proposed development will generate in the order of 406vtph during the morning peak and 457vtph during the evening peak as follows:

AM PEAK PERIOD TRAFFIC GENERATION			
Use	Inbound	Outbound	Total
Precinct 1 Hospital	36	10	46
Precinct 2 Health Facility	72	24	96
Precinct 2 Specialty Shops	49	20	69
Precinct 2 Restaurant	0	0	0
Precinct 2 Motel	8	20	28
Precinct 2 Motel Restaurant	0	0	0
Precinct 3 Child Care Centre	45	35	80
Precinct 3 Residential	6	18	24
Precinct 4 Residential	13	50	63
Total Development	229	177	406



PM PEAK PERIOD TRAFFIC GENERATION			
Use	Inbound	Outbound	Total
Precinct 1 Hospital	15	60	75
Precinct 2 Health Facility	24	72	96
Precinct 2 Specialty Shops	20	49	69
Precinct 2 Restaurant	12	3	15
Precinct 2 Motel	20	8	28
Precinct 2 Motel Restaurant	5	2	7
Precinct 3 Child Care Centre	35	45	80
Precinct 3 Residential	18	6	24
Precinct 4 Residential	50	13	63
Total Development	199	258	457

That traffic projected traffic generation potential has been assigned to the road network serving the site generally reflecting the origin/destination characteristics of the surveyed traffic generation of the existing Base Hospital as follows:

To/from North 95%

To/from South 5%

***Traffic Growth of Forest Road***

The Traffic and Parking Assessment Report prepared for the approved development by John Coady Consulting Pty Ltd (dated 1 July 2008) received the following information from Orange City Council with regard to traffic growth along Forest Road:

- The average daily traffic flows on Forest Rd in the vicinity of the proposed development site are in the order of 6,500 vehicle trips per day, and grew at an average annual rate of 3.9% in the period 1998 to 2005.
- The AM peak hour traffic flows are in the order of 520 vtp/h and grew at an annual rate of 3.0% per annum between 1998 and 2005, while the PM peak traffic flows are in the order of 600 vtp/h and grew at an annual rate of 3.7% between 1998 and 2007.

This report will continue to assume a 4% annual increase in traffic flow over next 10 years. The current traffic flows accessing the Orange Base Hospital will not be increased. To that end, the 2024 base flows on Forest Road travelling past the site will be as follows:

Direction	2014 Traffic Flow	2024 Traffic Flow
AM Northbound	236	349
AM Southbound	238	352
PM Northbound	337	499
PM Southbound	205	303

Traffic Implications of Proposed Development

The main traffic implications of the proposed development concern the ability of traffic that it generates to access the site via the two access points off Forest Rd (ie the new traffic signals serving the site and the Base Hospital and the northern left out only access driveway).



The ability of these intersections to accommodate the projected post-development traffic demand can be assessed using the SIDRA traffic model, and criteria for interpreting the results of SIDRA analysis are set out on the schedule reproduced in the following pages.

The access points off Forest Rd were modelled under projected future (2024) traffic demand and include the current traffic generation characteristics of the Base Hospital on the eastern side of Forest Road.

The results of that SIDRA analysis are set out in Table 5.1 (Traffic Signals) and Table 5.2 (northern access driveway) revealing that both intersections operate satisfactorily under projected traffic demand in 2024.

The Sidra Movement Summary Sheets for the Forest Road/Base Hospital/Subject Site Main Access are reproduced in Appendix C, while the Movement Summary Sheets for the Forest Road Northern Exit Driveway are reproduced in Appendix D.

**TABLE 5.1 – RESULTS OF SIDRA ANALYSIS OF FOREST ROAD AND
BASE HOSPITAL / MAIN SITE ACCESS INTERSECTION**

<u>2024 Conditions</u>	Level of Service	Degree of Saturation	Total Average Vehicle Delay (sec)
AM Peak	B	0.631	15.5
PM Peak	B	0.701	15.3

**TABLE 5.2 – RESULTS OF SIDRA ANALYSIS OF FOREST ROAD AND
NORTHERN EXIT DRIVEWAY**

<u>2024 Conditions</u>	Level of Service	Degree of Saturation	Total Average Vehicle Delay (sec)
AM Peak	A	0.439	0.6
PM Peak	A	0.450	1.3

In the circumstances, it can be concluded that the proposed development has no unacceptable traffic implications.



Criteria for Interpreting Results of SIDRA Analysis

1. Level of Service (LOS)

LOS	Traffic Signals and Roundabouts	Give Way and Stop Signs
'A'	Good operation.	Good operation.
'B'	Good with acceptable delays and spare capacity.	Acceptable delays and spare capacity.
'C'	Satisfactory.	Satisfactory but accident study required.
'D'	Operating near capacity.	Near capacity and accident study required.
'E'	At capacity; at signals incidents will cause excessive delays. Roundabouts require other control mode.	At capacity and requires other control mode.
'F'	Unsatisfactory and requires additional capacity.	Unsatisfactory and requires other control mode.

2. Average Vehicle Delay (AVD)

The AVD provides a measure of the operational performance of an intersection as indicated on the table below which relates AVD to LOS. The AVD's listed in the table should be taken as a guide only as longer delays could be tolerated in some locations (ie inner city conditions) and on some roads (ie minor side street intersecting with a major arterial route).

Level of Service	Average Delay per Vehicle (secs/veh)	Traffic Signals, Roundabout	Give Way and Stop Signs
A	less than 14	Good operation.	Good operation.
B	15 to 28	Good with acceptable delays and spare capacity.	Acceptable delays and spare capacity.
C	29 to 42	Satisfactory.	Satisfactory but accident study required.
D	43 to 56	Operating near capacity.	Near capacity and accident study required.
E	57 to 70	At capacity; at signals incidents will cause excessive delays. Roundabouts require other control mode.	At capacity and requires other control mode.

3. Degree of Saturation (DS)

The DS is another measure of the operational performance of individual intersections.

For intersections controlled by traffic signals¹ both queue length and delay increase rapidly as DS approaches 1, and it is usual to attempt to keep DS to less than 0.9. Values of DS in the order of 0.7 generally represent satisfactory intersection operation. When DS exceeds 0.9 queues can be anticipated.

For intersections controlled by a roundabout or GIVE WAY or STOP signs, satisfactory intersection operation is indicated by a DS of 0.8 or less.

¹

The values of DS for intersections under traffic signal control are only valid for cycle length of 120 secs.



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APPENDIX A

PLANS OF PROPOSED DEVELOPMENT PREPARED BY TVS ARCHITECTS



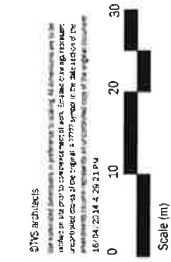
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No.	Description	Area (sqm)	Unit
1	DA House	15.04	DA
2	DA House	15.04	DA

TVS architects
Orange Private Hospital Site

Client: James Richmark Pty Ltd
Owner: Private Hospital Precinct - Site Plan
Project: Private Hospital Precinct - Site Plan
Design: M. Bingham
Scale: 1:100
Drawing No: 5420.3S.01.104 2





APPENDIX B

TRAFFIC COUNT DATA

**Traffic Count*****Forest Road and Base Hospital Access Driveway*****Friday 7th March 2014**

Time Period	A	B	C	D	E	F	Total
	Northbound	Right In	Southbound	Left In	Right Out	Left Out	
0700 - 0715	63	2	39	30	26	2	162
0715 - 0730	81	2	40	45	23	4	195
0730 - 0745	47	1	61	36	24	0	169
0745 - 0800	40	2	67	82	15	0	206
0800 - 0815	47	3	43	66	17	3	179
0815 - 0830	59	4	67	65	18	0	213
0830 - 0845	77	4	57	50	22	1	211
0845 - 0900	46	2	38	55	18	0	159
0900 - 0915	38	1	60	41	19	1	160
0915 - 0930	48	4	49	33	19	2	155
0930 - 0945	27	3	41	27	18	2	118
0945 - 1000	43	4	29	41	32	1	150
Total	616	32	591	571	251	16	2077

Time Period	A	B	C	D	E	F	Total
	Northbound	Right In	Southbound	Left In	Right Out	Left Out	
1500 - 1515	87	4	40	24	48	1	204
1515 - 1530	83	3	43	26	78	2	235
1530 - 1545	65	3	43	23	68	2	204
1545 - 1600	63	0	48	20	50	3	184
1600 - 1615	67	0	46	20	32	3	168
1615 - 1630	90	0	47	22	38	1	198
1630 - 1645	113	1	43	15	67	2	241
1645 - 1700	55	2	53	27	60	0	197
1700 - 1715	75	1	59	15	66	0	216
1715 - 1730	59	0	48	15	35	1	158
1730 - 1745	48	1	67	17	39	0	172
1745 - 1800	19	2	88	16	27	3	155
Total	824	17	625	240	608	18	2332



Traffic Count

Forest Road and Base Hospital Access Driveway

Friday 7th March 2014

Time Period	A	B	C	D	E	F	Total
	Northbound	Right In	Southbound	Left In	Right Out	Left Out	
0700 - 0800	231	7	207	193	88	6	732
0715 - 0815	215	8	211	229	79	7	749
0730 - 0830	193	10	238	249	74	3	767
0745 - 0845	223	13	234	263	72	4	809
0800 - 0900	229	13	205	236	75	4	762
0815 - 0915	220	11	222	211	77	2	743
0830 - 0930	209	11	204	179	78	4	685
0845 - 0945	159	10	188	156	74	5	592
0900 - 1000	156	12	179	142	88	6	583

Peak	223	13	234	263	72	4	809
------	-----	----	-----	-----	----	---	-----

Time Period	A	B	C	D	E	F	Total
	Northbound	Right In	Southbound	Left In	Right Out	Left Out	
1500 - 1600	298	10	174	93	244	8	827
1515 - 1615	278	6	180	89	228	10	791
1530 - 1630	285	3	184	85	188	9	754
1545 - 1645	333	1	184	77	187	9	791
1600 - 1700	325	3	189	84	197	6	804
1615 - 1715	333	4	202	79	231	3	852
1630 - 1730	302	4	203	72	228	3	812
1645 - 1745	237	4	227	74	200	1	743
1700 - 1800	201	4	262	63	167	4	701

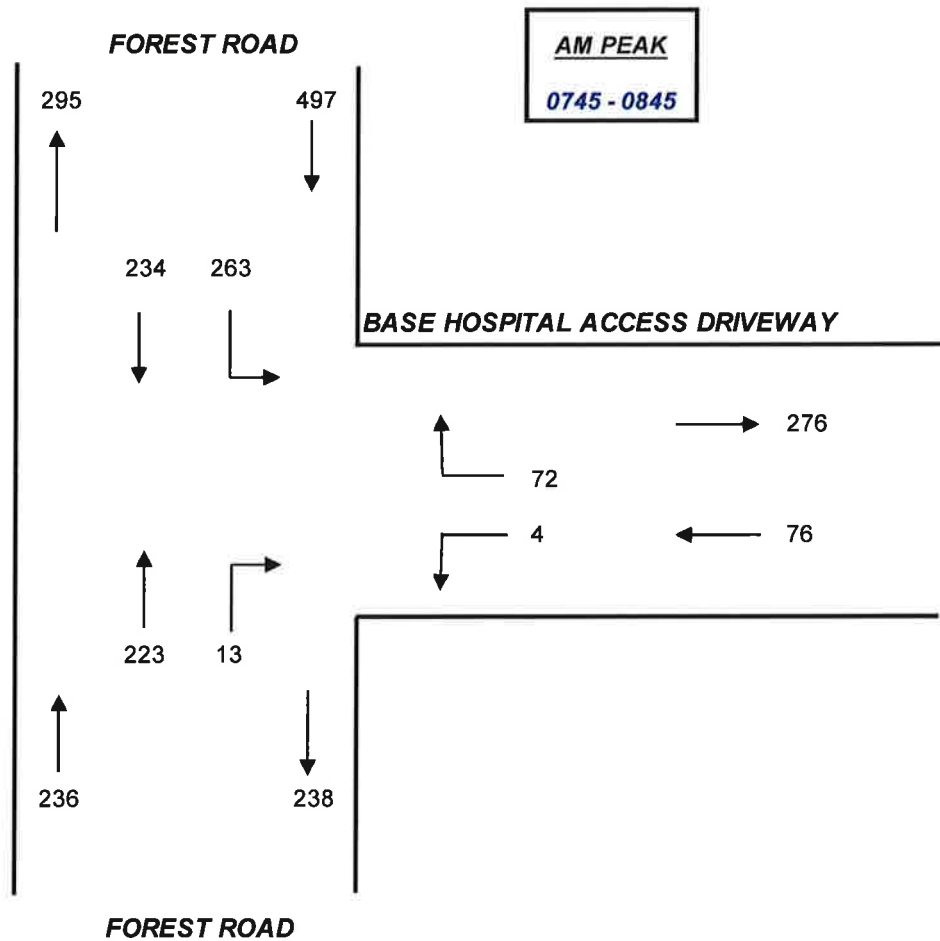
Peak	333	4	202	79	231	3	852
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Traffic Count

Forest Road and Base Hospital Access Driveway

Friday 7th March 2014

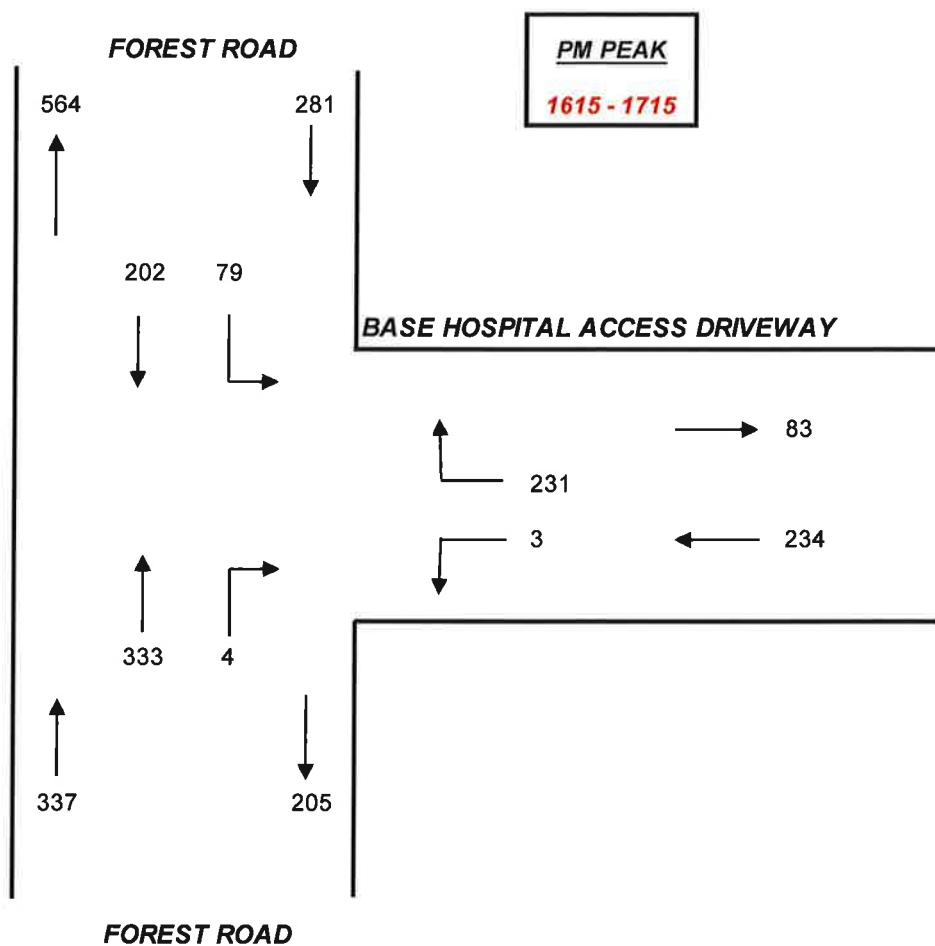




Traffic Count

Forest Road and Base Hospital Access Driveway

Friday 7th March 2014





APPENDIX C

SIDRA MOVEMENT SUMMARY SHEETS FOR THE FOREST ROAD/BASE HOSPITAL/SUBJECT SITE ACCESS DRIVEWAY

**MOVEMENT SUMMARY****Site: Forest Road Traffic Signals - AM Peak**

2024 Traffic Signals

Signals - Fixed Time Cycle Time = 50 seconds (Practical Cycle Time)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Flow Total veh/h	Flow HV %	Dep. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Forest Road											
1	L2	12	0.0	0.130	17.3	LOS B	1.5	10.8	0.70	0.58	38.8
2	T1	349	5.0	0.395	12.9	LOS A	5.1	37.1	0.77	0.64	49.4
3	R2	13	0.0	0.038	21.1	LOS B	0.2	1.7	0.77	0.68	25.8
Approach		374	4.7	0.395	13.3	LOS A	5.1	37.1	0.77	0.64	47.6
East: Base Hospital											
4	L2	4	0.0	0.005	9.6	LOS A	0.1	0.4	0.62	0.40	27.3
6	R2	72	0.0	0.124	10.2	LOS A	1.1	8.0	0.66	0.49	27.2
Approach		76	0.0	0.124	10.1	LOS A	1.1	8.0	0.66	0.49	27.2
North: Forest Road											
7	L2	263	0.0	0.402	17.3	LOS B	5.6	39.5	0.71	0.76	26.9
8	T1	352	5.0	0.402	12.5	LOS A	5.6	39.5	0.77	0.69	49.0
9	R2	217	0.0	0.631	25.3	LOS B	5.2	36.1	0.94	0.85	31.3
Approach		832	2.1	0.631	17.3	LOS B	5.6	39.5	0.79	0.75	35.3
West: Development Site											
10	L2	55	0.0	0.073	10.1	LOS A	0.8	5.9	0.65	0.49	38.0
12	R2	9	0.0	0.014	9.5	LOS A	0.1	0.9	0.62	0.40	38.5
Approach		64	0.0	0.073	10.0	LOS A	0.8	5.9	0.64	0.48	38.1
All Vehicles		1346	2.6	0.631	15.5	LOS B	5.6	39.5	0.77	0.69	37.6

Level of Service (LOS) Method: Delay (RTA NSW).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akcelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians								
Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	10	19.4	LOS B	0.0	0.0	0.88	0.88
P2	East Full Crossing	10	18.5	LOS B	0.0	0.0	0.86	0.86
P3	North Full Crossing	10	19.4	LOS B	0.0	0.0	0.88	0.88
P4	West Full Crossing	10	18.5	LOS B	0.0	0.0	0.86	0.86
All Pedestrians		40	18.9	LOS B			0.87	0.87

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)

Pedestrian movement LOS values are based on average delay per pedestrian movement.

Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.

**MOVEMENT SUMMARY****Site: Forest Road Traffic Signals - PM Peak**

2024 Traffic Signals

Signals - Fixed Time Cycle Time = 50 seconds (Practical Cycle Time)

Movement Performance - Vehicles											
Mov ID	OD Flow	Demand Flows Total veh/h	HV %	Seg Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Forest Road											
1	L2	10	0.0	0.184	17.6	LOS B	2.2	15.7	0.72	0.59	38.9
2	T1	499	5.0	0.557	13.7	LOS A	7.7	56.3	0.82	0.70	48.9
3	R2	4	0.0	0.010	19.1	LOS B	0.1	0.5	0.72	0.64	26.1
Approach		513	4.9	0.557	13.8	LOS A	7.7	56.3	0.82	0.69	48.4
East: Base Hospital											
4	L2	3	0.0	0.004	9.6	LOS A	0.0	0.3	0.62	0.39	27.3
6	R2	231	0.0	0.438	13.4	LOS A	4.5	31.3	0.80	0.66	26.6
Approach		234	0.0	0.438	13.3	LOS A	4.5	31.3	0.80	0.66	26.6
North: Forest Road											
7	L2	79	0.0	0.270	18.6	LOS B	3.3	23.7	0.71	0.67	27.2
8	T1	303	5.0	0.270	12.0	LOS A	3.3	23.7	0.73	0.64	49.4
9	R2	189	0.0	0.701	29.1	LOS C	4.9	34.4	0.98	0.90	29.5
Approach		571	2.7	0.701	18.5	LOS B	4.9	34.4	0.81	0.73	38.6
West: Development Site											
10	L2	121	0.0	0.161	10.5	LOS A	1.9	13.5	0.67	0.54	37.7
12	R2	13	0.0	0.021	9.6	LOS A	0.2	1.4	0.63	0.41	38.4
Approach		134	0.0	0.161	10.4	LOS A	1.9	13.5	0.67	0.53	37.8
All Vehicles		1452	2.8	0.701	15.3	LOS B	7.7	56.3	0.80	0.69	38.5

Level of Service (LOS) Method: Delay (RTA NSW).

Vehicle movement LOS values are based on average delay per movement.

Intersection and Approach LOS values are based on average delay for all vehicle movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akoelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Movement Performance - Pedestrians								
Mov ID	Description	Demand Flow ped/h	Average Delay sec	Level of Service	Average Back of Queue Pedestrian ped	Distance m	Prop. Queued	Effective Stop Rate per ped
P1	South Full Crossing	10	19.4	LOS B	0.0	0.0	0.88	0.88
P2	East Full Crossing	10	18.5	LOS B	0.0	0.0	0.86	0.86
P3	North Full Crossing	10	19.4	LOS B	0.0	0.0	0.88	0.88
P4	West Full Crossing	10	18.5	LOS B	0.0	0.0	0.86	0.86
All Pedestrians		40	18.9	LOS B			0.87	0.87

Level of Service (LOS) Method: SIDRA Pedestrian LOS Method (Based on Average Delay)

Pedestrian movement LOS values are based on average delay per pedestrian movement.

Intersection LOS value for Pedestrians is based on average delay for all pedestrian movements.



APPENDIX D

SIDRA MOVEMENT SUMMARY SHEETS FOR THE FOREST ROAD NORTHERN SITE EXIT DRIVEWAY

**MOVEMENT SUMMARY**

▽ **Site: Forest Road Exit Driveway - AM Peak**

2024 Flows
Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deg Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Forest Road											
2	T1	473	5.0	0.250	0.0	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		473	5.0	0.250	0.0	NA	0.0	0.0	0.00	0.00	59.9
North: Forest Road											
8	T1	830	5.0	0.439	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		830	5.0	0.439	0.1	NA	0.0	0.0	0.00	0.00	59.9
West: Site Access Driveway											
10	L2	113	0.0	0.111	7.5	LOS A	0.4	3.0	0.48	0.70	52.1
Approach		113	0.0	0.111	7.5	LOS A	0.4	3.0	0.48	0.70	52.1
All Vehicles		1416	4.6	0.439	0.6	NA	0.4	3.0	0.04	0.06	59.2

Level of Service (LOS) Method: Delay (RTA NSW).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akgelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.



MOVEMENT SUMMARY

▽ Site: Forest Road Exit Driveway - PM Peak

2024 Flows

Giveaway / Yield (Two-Way)

Movement Performance - Vehicles											
Mov ID	OD Mov	Demand Total veh/h	Flows HV %	Deq. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Queue Distance m	Prop. Queued	Effective Stop Rate per veh	Average Speed km/h
South: Forest Road											
2	T1	849	5.0	0.450	0.1	LOS A	0.0	0.0	0.00	0.00	59.9
Approach		849	5.0	0.450	0.1	NA	0.0	0.0	0.00	0.00	59.9
North: Forest Road											
8	T1	186	5.0	0.098	0.0	LOS A	0.0	0.0	0.00	0.00	60.0
Approach		186	5.0	0.098	0.0	NA	0.0	0.0	0.00	0.00	60.0
West: Site Access Driveway											
10	L2	124	0.0	0.215	11.2	LOS A	0.8	5.5	0.70	0.88	49.5
Approach		124	0.0	0.215	11.2	LOS A	0.8	5.5	0.70	0.88	49.5
All Vehicles		1159	4.5	0.450	1.3	NA	0.8	5.5	0.08	0.09	58.6

Level of Service (LOS) Method: Delay (RTA NSW).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA: Intersection LOS and Major Road Approach LOS values are Not Applicable for two-way sign control since the average delay is not a good LOS measure due to zero delays associated with major road movements.

SIDRA Standard Delay Model is used. Control Delay includes Geometric Delay.

Gap-Acceptance Capacity: SIDRA Standard (Akeelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Processed: Monday, April 21, 2014 10:50:11 AM
SIDRA INTERSECTION 6 0 20 4860
Project: E:\my documents\staa\Traffic\aa\SIDRA Projects\14016T Orange.sip6
8002622, TERRAFFIC PTY LTD, PLUS / IPC

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**SIDRA
INTERSECTION 6**

Annexure C

Landscaping Plans by McGregor Coxall

ORANGE PRIVATE HOSPITAL SITE

Forest Road

Orange

LANDSCAPE GENERAL CONCEPT PLAN

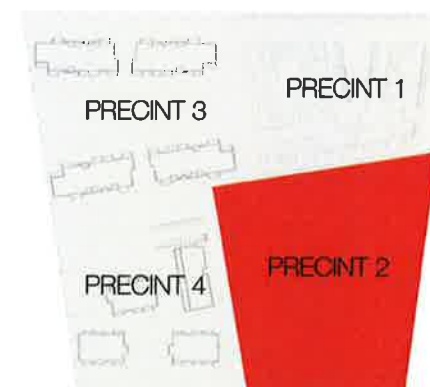
REVISION C | SCALE 1:750 @ A1

0 5 10 15 20 25 M



- Site boundary
--- Precinct 2 boundary
- 1 Main vehicle access
2 Main boulevard
3 Plaza with feature tree
4 Grass mounds - The "Health Garden"
5 Covered pedestrian crossing
6 Covered/raised pedestrian crossing
7 Drop off zone
8 Future pedestrian connection
9 Stormwater filtration with native wetland planting

- SEATING WALL
QUALITY PAVING
CONCRETE PEDESTRIAN PAVING
VEHICLE CARTRIDGE WAY
PARKING BAY - UNIT PAVERS
DECOMPOSED GRANITE
VEHICLE DIRECTIONAL MARKING
OUTDOOR FURNITURE
SEATING BENCH
- IRRIGATED LAWN IN 150MM GARDEN SOIL MIX AT GRADE
PROPOSED NATIVE SHRUBS/GROUNDCOVERS PLANTING IN 300MM DEPTH GARDEN SOIL MIX AT GRADE
PROPOSED NATIVE GRASSES IN 300MM DEPTH GARDEN SOIL MIX AT GRADE
FILTRATION BIORETENTION DRAINAGE WITH NATIVE GRASS/WETLAND PLANTING IN 300MM DEPTH GARDEN SOIL MIX AT GRADE
PROPOSED NATIVE SHADE TOLERANT SHRUBS/GROUNDCOVER PLANTING IN 300MM DEPTH GARDEN SOIL MIX AT GRADE
PROPOSED NATIVE FLOWERING GRASS/SHRUB/GROUNDCOVER PLANTING IN 300MM DEPTH GARDEN SOIL MIX AT GRADE
HEDGE PLANTING IN 300MM DEPTH GARDEN SOIL MIX AT GRADE
SHADE GARDEN/UTURN GARDEN PLANTERS AND SEATING AREAS
PROPOSED NATIVE TREE PLANTING
PROPOSED NATIVE SMALL TREE PLANTING
PROPOSED EXOTIC TREE PLANTING



ORANGE PRIVATE HOSPITAL SITE - PRECINCT 2

Forest Road

Orange

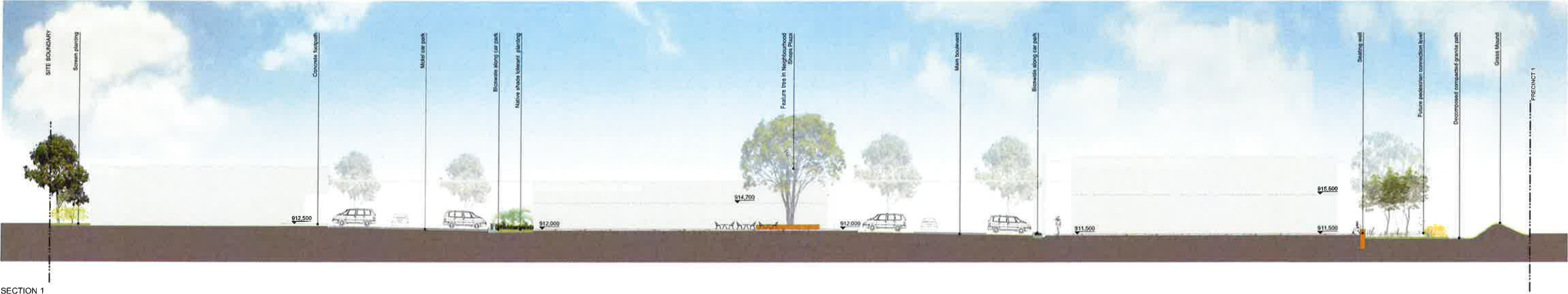
LANDSCAPE MASTERPLAN

REVISION E | SCALE 1:350 @ A1

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01

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SECTION 1

- Street Trees 25L [min.] 400L [max.]**
Oreococcus nivalis - Oak [8-12m]
Japaneana nivalis - Japaneana [10-15m]
- Fruit Trees 25 L [min.] 400L [max.]**
Carus [E] - Lime
Morus domestica [E] - Apple Daisy Pink Lady
Olea europaea [E] - Kalonji Fruit, Cusco Fruit
Pyrus communis [E] - Pear
Prunus [E] - Cherry
- Shrubs [min. 200mm containers]**
Cornelia japonica
Magnolia blanda Nigra
- Perennials/Ground Covers/Climbers [min. 150mm containers]**
Asterum x hybrid
Hemerocallis hybrid
Iris spicata
Lavandula stoechas
Rosmarinus - Tuscan Blue [E]
Lynx vulgaris [E]
- Grasses [min. 150mm containers]**
Carex appressa
Panicum glaucum
Pennisetum purpureum
- Roses [min. 150mm containers]**
Floribunda bush roses
European - deep crimson
Kobold - pure white
Mol. nobile - creamy apricot
Queen of Hades - pure pink
Sea Foam - pale pink to white
Modern shrub roses
Kiss of Life - light pink
Tomb Raider - red
- note:
[E] - edible plant

- Trees 25 L [min.] 400L [max.]**
Acacia dealbata - Silver Wattle [4-6m]
Casuarina cunninghamiana - River She Oak [10-15m]
Eucalyptus melliodora - Yellow Box [10-20m]
Eucalyptus brachyphylla - Apple Box
Eucalyptus viminalis - Rubber Gum
Melaleuca quinquanervia - Broadleaved Paperbark [12m]
Tristania laurina - Water Gum [10m]
- Native Shrubs [min. 200mm containers]**
Acacia longifolia - Sydney Golden Wattle [2-4m]
Acacia meunieri - Black Wattle [3-10m]
Banksia marginata - Silver Banksia [4-6m]
Banksia spinulosa - Hairy Banksia [4-16m]
Callistemon citrinus - Citrus Bottlebrush [2-3m]
Correa alba - White Glory [2m]
Dillwynia retorta - Hardi Palm Tree [0.5-1m]
Doryanthes acicula - Gynera Lily [1.5m]
Grevillea bursaria - Hairy Grevillea [8-10m]
Grevillea longifolia - Long leaved Grevillea [3m]
Kurua parviflora - Kurua [1.5-2.5m]
Leptospermum laevis [E] - Lemon Tea Tree
Oleandra phlogoea - Daisy Bush [1.5m]
Syzygium camellifolium [E] - Mugger Cherry
Westringia floribunda - Coastal Rosemary [2m]
- Native Ground Covers/Climbers [min. 150mm containers]**
Brachycome 'Break O' Day' - Break O' Day Daisy [50cm]
Brachycome multifida - Daisy [50cm]
Cissus antarctica - Kangaroo Vine
Clematis aristata - Clematis
Clematis integrifolia - Small Leaved Clematis
Gentiana solanifolia - Native Gentiana
Hortensia vitacea - False Sandpiper [10cm]
Hortensia vitacea - Hairy Guinea Flower [40cm]
Hortensia vitacea - Golden Guinea Flower [40cm]
Hortensia vitacea - Silky Guinea Flower [40cm]
Kerria reticulata - Dusky Coral Pea [10cm]
Marek australis [E] - River Mint
Oenothera americana [E] - Wild Basil
Panicum panduratum - Wanga Wanga Vine
Passiflora subcordata [E] - Passionfruit
Rhus parvifolia [E] - Native Rhipidophyllum
Scaevola ramosissima - Hairy Fan Flower [10cm]
Vicia hederacea - Native Violet [10cm]

- Native Grasses [min. 150mm containers]**
Anglozanthus 'Bush Gem' - Kangaroo Paw 'Bush Gem' [1m]
Bambusa nana [1m]
Dianella caerulea - Parrot Lily
Dianella teretica - Blue Fox Lily [1m]
Dianella teretica - 'Lipsa Row' - Lipsa Row Fox Lily
Gymnoschoenus sphaerocarpus - Button Grass
Gymnoschoenus sphaerocarpus - Saw Sedge [1.5m]
Lomandra longifolia - Spiny Mat Rush [1m]
Lomandra longifolia - Wattle Mat Rush [50cm]
Lomandra longifolia - Mary Flowered Mat Rush [50cm]
Rhus parvifolia [E] - Native Rhipidophyllum
Thymus hederacea - Kangaroo Grass [0.5m]
- Native Wetland Plants/Sludges [min. 150mm containers]**
Bambusa nana - Bare Ivy Rush [1m]
Bambusa nana - [2.5m]
Bambusa nana - [1.5m]
Carex appressa - Tall Sedge [1m]
Carex appressa - Tall Sedge [1m]
Isotria medeoloides - Swamp Clubrush
Juncus roemerianus - Common Rush [0.5m]
Juncus roemerianus - [1.25m]
Juncus roemerianus - [1.5m]
Juncus roemerianus - [1.5m]
Gymnoschoenus sphaerocarpus - Sedge [1.5m]

note:
[E] - edible plant



PLANTING LIST

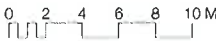
ORANGE PRIVATE HOSPITAL SITE - PRECINCT 2

Forest Road

Orange

SECTION AND PLANTING LIST

REVISION D | SCALE 1:200 @ A1



02

Annexure D

Confirmation of Physical Commencement

D13/25002
PR4072

8 November 2013

Mr Peter Basha
Peter Basha Planning and Development
PO Box 1827
ORANGE NSW 2800

Dear Mr Basha

**PHYSICAL COMMENCEMENT OF WORKS - PRIVATE HOSPITAL DEVELOPMENT
(MAJOR PROJECTS MP07_0072; MP08_0230 & MP08_0233) AT LOT 1 DP 549856 1517
FOREST ROAD.**

Reference is made to your letter dated 7 November 2013 in which you request advice relating to the physical commencement of the Private Hospital development (Major Projects MP07_0072; MP08_0230 & MP08_0233) at Lot 1 DP 549856 1517 Forest Road.

It is our opinion that by carrying out the extensive demolition works on the site, as detailed within your letter, the development has attained physical commencement.

I trust this clarifies the matter for you. I wish you well with the continued development of this site and look forward to discussing the development further with you when it comes to the time of appointing the Principal Certifying Authority for the construction works.

Yours faithfully



Mark Hodges
MANAGER BUILDING AND ENVIRONMENT