

4. Concept Plan Description

4.1 Overview of Concept Plan

The Concept Plan provides the vision and framework for the assessment of future development proposals within Precinct 1 of the HEZ Estate.

The proposed Precinct 1 layout (see figure 4) establishes the physical framework and incorporates design initiatives to facilitate the development of individual sites within the Precinct in a manner that responds to the environmental and physical considerations of the site. This approach has been pursued to implement the environmental and employment objectives of the original LEP that rezoned the land and the objectives of the 4(h) Hunter Employment Zone.

These strategies and requirements are detailed in the supporting documentation and the Draft Statement of Commitments (Appendix A) prepared for the application.

The overall Concept Plan vision for the Precinct has been prepared by EDAW and is supported by a range of technical studies and proposed environmental management strategies which are appended to this report.

The Concept Plan should be read in conjunction with the Project Plan Application assessment prepared for the proposed wall and floor panel manufacturing facility to be accommodated on proposed lot 290.

The project is described in the Director-General's Requirements issued on 25 October 2007 as comprising:

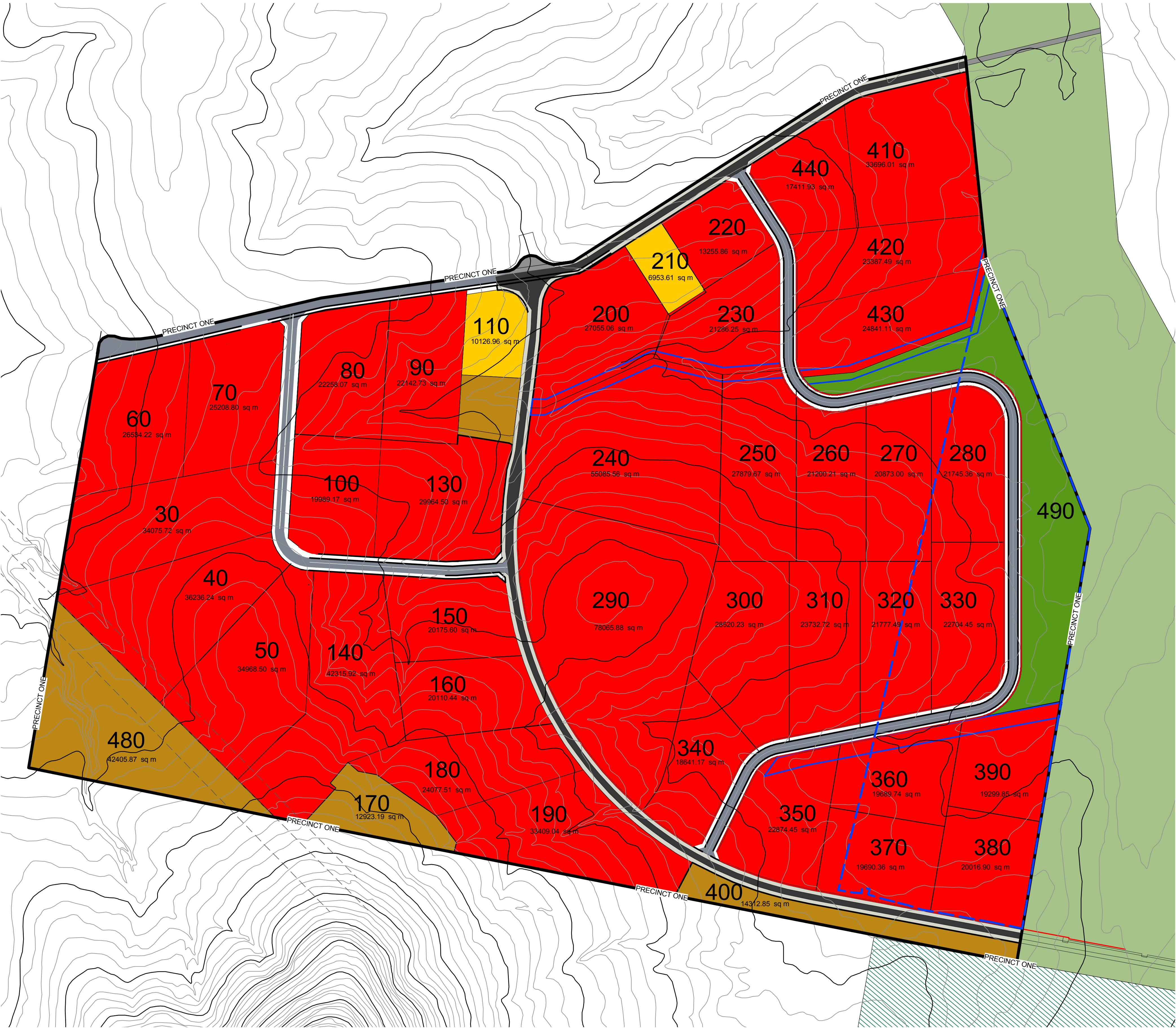
- Subdivision of Precinct 1, a 129 hectare portion of the Hunter Economic Zone (HEZ) for future industrial and employment generating developments;
- The construction and operation of a manufacturing facility within Precinct 1 for the production of wall and floor panels with acoustic –rated and fire-rated properties; and
- The construction of associated Precinct 1 infrastructure, including the Pelaw main by-pass and an extension to Station Street to provide access to the

proposed manufacturing facility and the larger HEZ area beyond precinct 1.

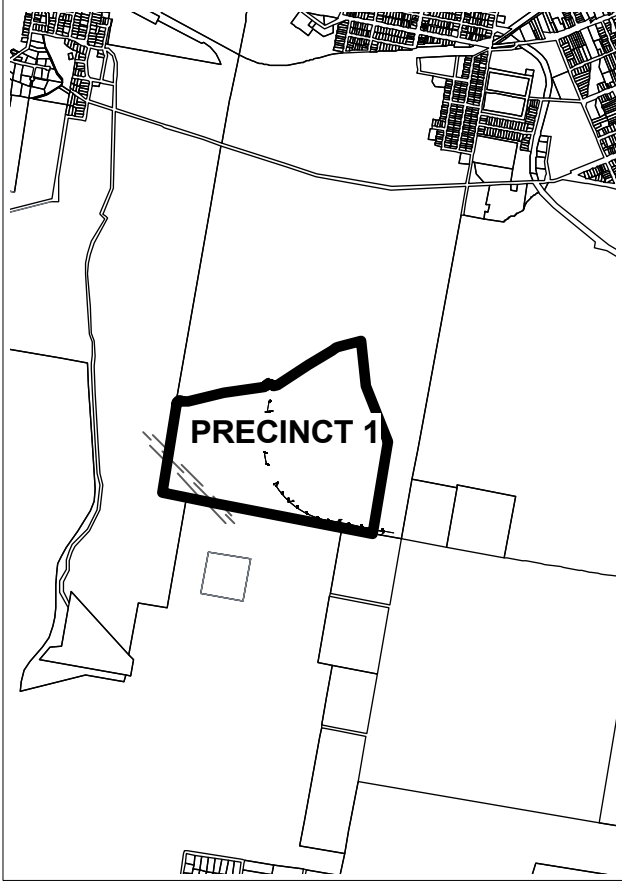
The Concept Plan submission includes:

- Road and lot layouts for precinct 1 incorporating lot modules of generally 2 hectares that can be readily consolidated into larger parcels. This lot size and flexibility responds to the market demand for industrial users to be attracted to the estate generally requiring 2 to 5 hectare lot sizes;
- Incorporation of conservation buffers, environmental features and habitat into the Precinct layout that reinforces the central Environmental protection corridor;
- Incorporation of Precinct wide bushfire protection measures into the Precinct design;
- Provision of a road layout that seeks to respond to the natural drainage patterns of the site to facilitate the provision of water sensitive urban design principles;
- Design principles for the treatment of retained vegetation setback and batter treatments;
- Provision of a comprehensive Water Cycle Management Strategy for the development of the estate (Appendix J);
- Provision of a comprehensive Air Quality Management Strategy (Appendix J);
- Provision of a comprehensive Noise & Vibration, Electrical Interference and Lighting Management Strategy (Appendix K);

The associated Project Plan application seeks the Minister's approval for the use and development of proposed lot 290 within Precinct 1 of the HEZ for the manufacture of wall and floor panels.



KEYPLAN



LEGEND

- Precinct boundary
- Drainage Easement
- Noncommitted lots
- Committed lots
- Infrastructure
- Roads
- Additional conservation zones
- 7(b) Environmental Protection Zone

REFERENCES

CONSULTANT	DRAWING ID	DATE OF ISSUE
HSO	23693_site_boundary_20.12.06	
HSO	23403_G_parviflora pr19.12.06	
HSO	Green-thighed Frog Ha19.12.07	
HSO	21489_Pipeline alignm 24.11.07	
HSO	21489_Pipeline alignm 24.11.07	
HSO	23909_Basedata_7.11.08.11.06	
HSO	pelaw-main-bypass-ba:21.11.07	
HSO	Hollow Bearing Tree_C23.02.07	
HSO	Mature Tree Combined22.02.07	
HSO	Threatened Fauna_Coi22.02.07	
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HEZ	DESIGN.dwg	22.11.07
HEZ	SURVEY.dwg	22.11.07
HEZ	WSP sheet 2 Layout1	27.11.07

Remaining vacant lots	Nominal Area
Lot - 100	19989.168
Lot - 130	29964.504
Lot - 140	42315.919
Lot - 150	20113.033
Lot - 160	20110.442
Lot - 180	24077.512
Lot - 190	33409.04
Lot - 200	27055.064
Lot - 220	13255.863
Lot - 230	21286.248
Lot - 240	55085.56
Lot - 250	27879.674
Lot - 260	21200.215
Lot - 270	20873.001
Lot - 280	21745.358
Lot - 30	34075.717
Lot - 300	28520.232
Lot - 310	23732.719
Lot - 320	21777.491
Lot - 330	22704.449
Lot - 340	18641.165
Lot - 350	22874.446
Lot - 360	19689.738
Lot - 370	19690.362
Lot - 380	20016.904
Lot - 390	19299.851
Lot - 40	36236.243
Lot - 410	33696.007
Lot - 420	23441.953
Lot - 430	24841.111
Lot - 440	17411.933
Lot - 50	34968.496
Lot - 60	26534.216
Lot - 70	25208.803
Lot - 80	22258.069
Lot - 90	22142.728
Lot-290	78065.88
Committed Lots	
Lot - 110	10126.959
Lot - 210	6953.605
Infrastructure	
Lot - 170	12923.194
Lot - 400	14312.855
Lot - 480	42405.866
Lot - 120	7124.859
Roads	
Road 1E	16634.04
Road 1C	118356.69
Road 1D	20523.729
Additional conservation zone	
Lot - 490	47672.44
Precinct 1 exclusions	
Lot - 450&460	266082.489

NOTES

- Information relating to architectural, electrical, hydraulic, civil and other works as represented on EDAW documentation is for reference and EDAW co-ordination purpose only. All documentation to these and other works outside EDAW scope should be referred to the relevant consultants drawings and specification for details.
- This drawing should be read in conjunction with EDAW prepared specification and details. Should a conflict exist advice and direction should be sought from EDAW prior undertaking any construction works.
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Project	HEZ PART 3A APPLICATION
Client	HEZ NOMINEES PTY LTD ATF THE HEZ UNIT TRUST
Drawing	Precinct One Development areas

Scale	1:3000@A3	2297.10 L02F
Project No.	07502428.10	
Drawn	CY	

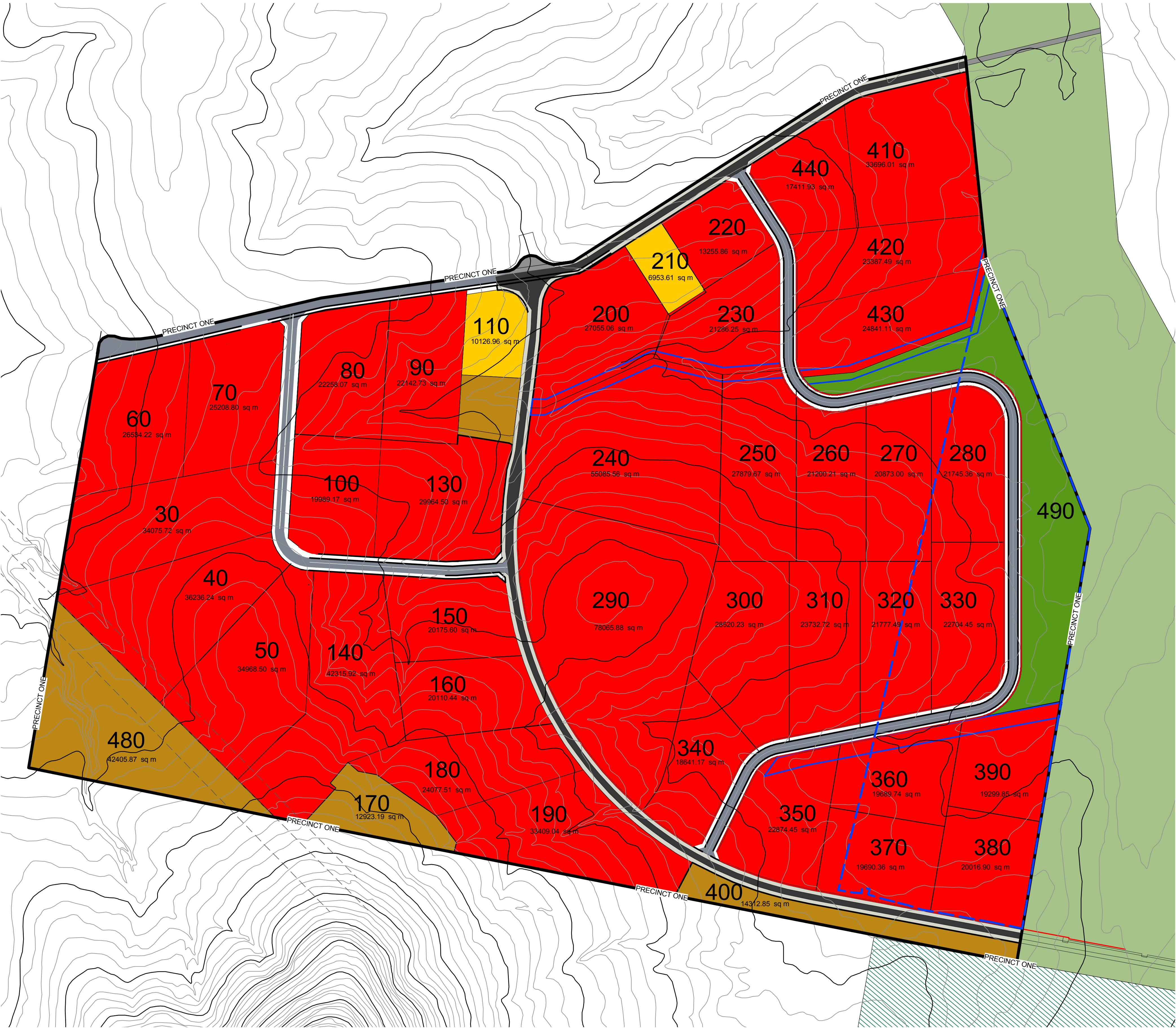
Figure 4 Precinct 1 Subdivision Layout

F	Client Issue	04/04/08
E	Client Issue	26/03/08
D	Client Issue	18/01/08
C	Client Issue	12/12/07
B	Client Issue	12/12/07
A	Client/Consultant Workshop	31/01/07
REV	DESCRIPTION	DATE APPROVED

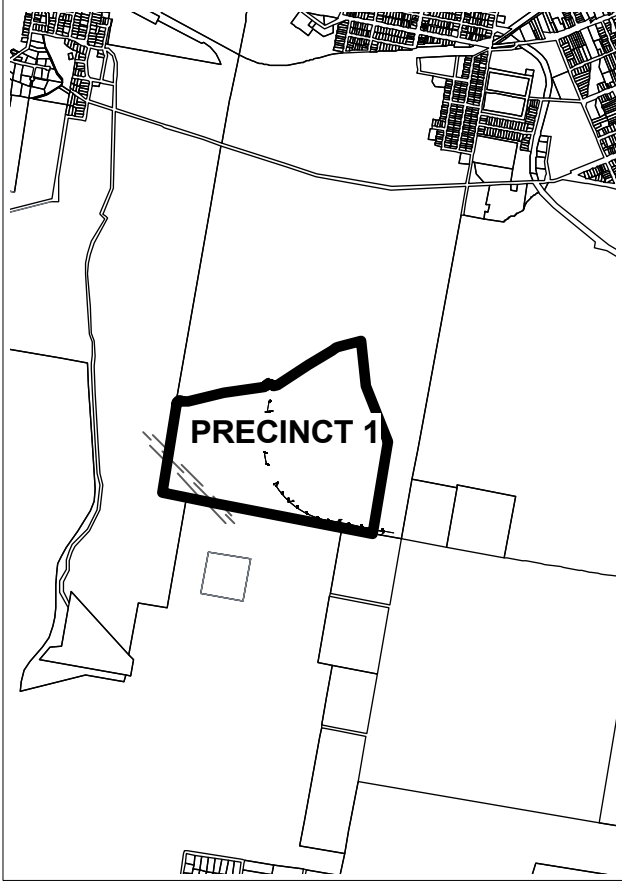
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KEYPLAN



LEGEND

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Figure 4 Precinct 1 Subdivision Layout

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REV	DESCRIPTION	DATE APPROVED

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4.2 Development Objectives for HEZ

The development objectives for the site are:

- To implement the Policies and Objectives of the Lower Hunter Regional Strategy;
- To retain and manage the environmentally sensitive lands on the site for habitat and protection of biodiversity;
- To design and prepare the site in a manner that manages the environmental considerations that apply to the site;
- To provide opportunities for employment generating development providing a range of employment opportunities in the Cessnock LGA and broader Lower Hunter Region;
- To incorporate natural hazard protection measures into the subdivision design;
- To provide guidelines for the ongoing development of the site;
- To provide a framework to manage the potential impacts of the future use and development of the site;
- To provide a framework that will accord with a pending State Significant Site Study for the subject and surrounding lands.

4.3 Detailed Description of Concept Plan elements

The Concept Plan has been described as having three main components comprising:

1. The proposed road layout ,subdivision pattern, associated infrastructure and linkage to Weston via the Station Street extension;
2. The construction of a wall and floor panel manufacturing facility on proposed lot 290;
3. The proposed Pelaw Main by-pass.

4.3.1 Precinct 1 Subdivision

A plan of the proposed subdivision for precinct 1 (figure 4) has been prepared by EDAW for Precinct 1 of the Hunter Economic Zone (HEZ).

The overall concept for the HEZ is to provide a high quality Industrial estate that integrates the environmental attributes into the estate layout providing a visual and recreational amenity for future occupants of the estate as well as providing protection for a variety of flora and fauna species. This approach to the management of the site strives to integrate these outcomes with the provision of long-term employment generating opportunities.

The subdivision layout has been designed to respond to a range of physical and infrastructure considerations these have included:

- Current zoning under Cessnock LEP 1998;
- Existing vegetation communities;
- Threatened flora and fauna;
- Site topography;
- Past mining activities;
- Existing and under construction infrastructure;
- Bushfire mitigation requirements;
- Desired traffic accessibility;

The proposal will create a Concept Layout for 39 development lots and associated infrastructure and environmental management lots.

The road pattern prepared for the Estate has integrated the existing spine access drive, to be known as HEZ Drive, and has sought to integrate a number of fundamental principles to ensure the maximum efficiency of the estate and the integration of the ecological management considerations for the estate.

The subdivision and Precinct layout has sought to provide opportunities to:

- Maximise the supplementary bushland retention within lot front setbacks;

- Integration of the road pattern with the topography of the site to maximise WSUD opportunities for future development of the land;
- Demonstrate that vegetation retention targets required by the EPBC Act 1999 approval for the site can be met and exceeded;
- Integrate bushfire management and protection requirements into the estate design without compromising the ecological and vegetation objectives for the site;
- Provide a design and configuration to facilitate the efficient provision of trunk infrastructure to the estate;
- Provide lot sizes and configurations that readily accommodate lots in the target 2 to 5 hectare range and are in modular form that will allow amalgamations and reconfigurations without compromising the underlying objectives of the design;
- Ensure the internal road layout is capable of accommodating B-Double vehicles;
- Ensure that fundamental internal infrastructure issues do not become a constraint to large scale industrial and employment uses developing on the lots to be created.

The design and layout proposed has achieved these objectives, and as demonstrated within the accompanying technical assessments a framework has been provided that will facilitate future development in a manner that will deliver upon the objectives of the original LEP that zoned the land for employment generating purposes.

Proposed Road network

The proposed road network to service Precinct 1 has been designed to:

- Utilise the existing approved and constructed HEZ spine road, to be known as HEZ Drive;
- Avoid dead end and cul de sac roads;
- Provide B Double access throughout the estate to maximise future development options;
- Provide the second linkage into the existing traffic network via the Station Street extension to the north linking into Weston consistent with the original estate masterplan;
- The provision of a second vehicular access into Weston increases the options for site evacuation in the case of an emergency situation such as bushfire threat;
- The road pattern responds to and works with existing drainage patterns to minimise site disturbance requirements and to provide all lots with drainage to the front of the lot;
- The layout provides a 20m conservation corridor within each lot adjacent to the proposed roads;
- The 20m conservation corridor is supplemented by a 10m reinstatement buffer to accommodate site benching, water quality and quantity control measures
- The road alignment have been designed to accommodate significant patches of threatened species within the adjoining 20m conservation corridors, particularly *Rutidosia heterogama* in the south western section of the precinct;
- The road verge has been designed to be reinstated with modified plant communities consistent with existing plant community distribution;
- The 20m conservation setbacks provide significant linear corridors to supplement the offset lands within the estate that have been zoned 7(b) Environment Protection(Conservation);
- The road network provides easy scope for the amalgamation of lots without any impact upon the integrity of the 20m conservation corridors that abut the propose roads;
- The 20m Conservation corridor is intended to be punctuated only by site access driveways.

All proposed roads within the Estate are proposed to be dedicated as public Roads. All roads approved and constructed to date have been designed to comply with the requirements and standards of Cessnock Council. This approach will be retained with all future roads.

Lot size and Mix

The Concept Plan development and service lots are detailed within the following table:

Proposed development lots	Area (sqm)
Lot - 100	19989
Lot - 140	42316
Lot - 150	20113
Lot - 160	20110
Lot - 180	24078
Lot - 190	33409
Lot - 200	27055
Lot - 220	13256
Lot - 230	21286
Lot - 240	55086
Lot - 250	27880
Lot - 260	21200
Lot - 270	20873
Lot - 280	21745
Lot - 30	34076
Lot - 300	28520
Lot - 310	23733
Lot - 320	21777
Lot - 330	22704
Lot - 340	18641
Lot - 350	22874
Lot - 360	19690
Lot - 370	19690
Lot - 380	20017
Lot - 390	19300
Lot - 40	36236

Lot - 420	23442
Lot - 430	24841
Lot - 440	17412
Lot - 50	34968
Lot - 60	26534
Lot - 70	25209
Lot - 80	22258
Lot - 90	22143
Lot-290	78066
Total	994189
Committed development Lots	Area (sqm)
Lot - 110	10127
Lot - 210	6954
Lot - 410	33696
Lot - 130	29965
Total	17081
Infrastructure Lots	Area (sqm)
Lot - 170	12923
Lot - 400	14313
Lot - 120	7125
Total	76767
Roads	
Total	155514
Conservation Lots	Area (sqm)
Lot - 490	47672
Lot - 480	42406
Total	90078

The proposed Precinct design accommodates lot modules of generally 2 hectares or greater. This configuration has been pursued to permit easy amalgamation of lots to provide industrial lots in the range of 2 to 5 hectares.

No battle axe lots are required or proposed ensuring all development lots have substantial frontage to a proposed public road.

The lot configuration proposed provides development sites that are capable of development for a wide range of industrial and employment generating uses. The modules provide lots consistent with the market demand and consistent with the lot sizes for the approved use on lot 410 and the proposed use on lot 130.

The market considerations undertaken by the proponent have identified that the market with the greatest demand is in the 2 to 5 hectare range. Should demand arise for larger lots the sites can be readily amalgamated with no loss of integrity for the road frontage 20m bushland conservation and restoration setback and the 10m transition setback.

Utilities and Infrastructure Provision

Construction has been completed for the provision of an Energy Australia substation within Lot 220. This substation site has been designed to accommodate future potential growth in power demand for development within the precinct.

Construction is well advanced on a water supply reservoir for Hunter Water within proposed Lot 170. The associated water distribution and pump station is also nearing completion.

Consent has been granted by Hunter Water, and construction has commenced for the provision of the trunk sewer to service the precinct.

An application for approval of the reticulated sewer within the Precinct is pending with Hunter Water.

All roads within the estate will be directly provided. A Traffic Assessment for the development of up to 206 hectares of land has been prepared which is intended to form the basis for a Deed of Agreement with the RTA for road and traffic improvement works associated with the development of the estate.

Wildlife Corridors and Bushland Setbacks

The zone boundary configuration for the HEZ study area provides a 200m wide central corridor that is zoned 7(b) Environmental Protection (Conservation) under Cessnock LEP 1989. This zone covers Chinamans Hollow Creek and has been protected to conserve the habitat and floral values of these lands.

This process has lead to conservation zoning protection to approximately 63% of the land covered by the HEZ LEP.

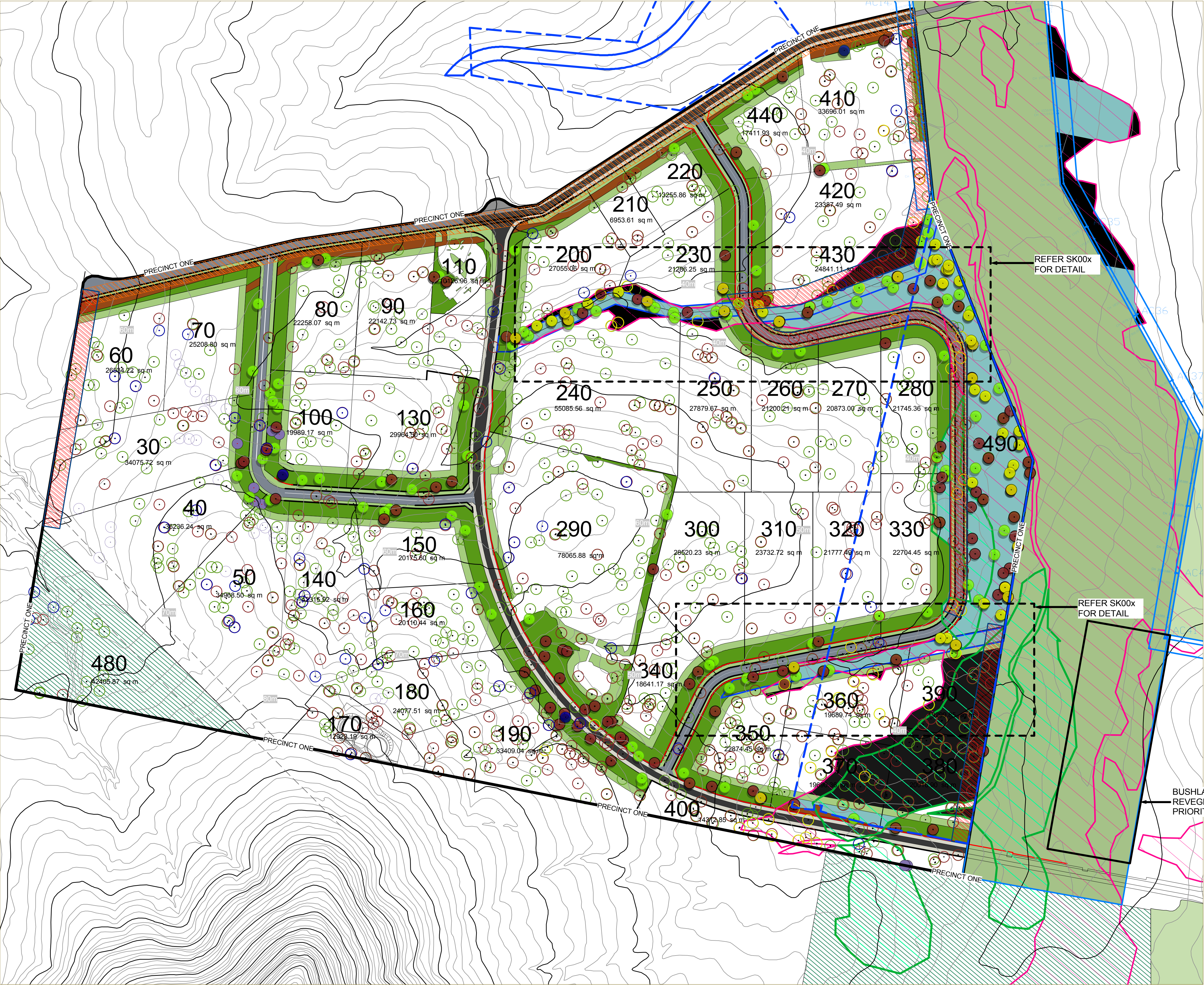
This protection has now been further reinforced through the approval for the estate issued under the Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act) which among other matters has required the preparation and implementation of Conservation Plans of Management for the HEZ Pty Ltd lands.

The 7(b) zoned creekline corridor includes a significant proportion of the endangered Hunter Lowland Redgum Forest HLRF). The Precinct 1 Design proposes to augment the already protected HLRF through the reconfiguration of Deferred Area 1 (Figure 5). The subdivision design proposes the protection and retention of the LHRF community along two watercourses to the west of Chinamans Hollow Creek. The road alignment provides a managed edge to the central conservation corridor. The design protects or retains 6.6 hectares of HLRF, or approximately half of the occurrence within the 4(h) zoned lands. This additional protected area along with the LHRF protected within the central 7(b) zoned lands results in the conservation of 52ha of HLRF within HEZ Pty Ltd owned lands. This represents the protection and management of 40% of the total HLRF within the Cessnock Local Government Area.

The central corridor is further augmented by the proposed 20m bushland Conservation setbacks within the front setback of all lots fronting the proposed roads. These conservation setbacks are proposed to be supplemented by additional 10m bushland reinstatement setbacks within which water control measures and devices can be provided. These setbacks are demonstrated within Figure 3.

This will effectively provide 30m bushland setbacks to each side of the roads

throughout the Precinct. These will provide significant opportunities for the retention of mature and hollow bearing trees for habitat and foraging potential but also provide a significant visual resource and amenity for the estate.



K	Final Issue	05.05.08	
J	Final Issue	04.04.08	
I	Final Issue	26.03.08	
H	Final Issue	25.03.08	
G	Final Issue	19.03.08	
F	Final Issue	29.02.08	
E	Final Issue	13.02.08	
D	Final Issue	13.12.07	
C	Client Issue	12.12.07	
B	Client Issue	11.12.07	
A	HEZ Part 3A Precinct One Concept Application (Option 2)	29.11.07	
REV	DESCRIPTION	DATE	APPROVED

Figure 5 Precinct 1 layout with setbacks and retention areas

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PRECINCT ONE;

RETAINED MATURE TREES; 177 (FROM 1017 EXISTING) OR 17.40%

RETAINED C. MACULATA AND E. TERETICORNIS; 50 (FROM 159 EXISTING) OR 31.44%

RETAINED HOLLOW BEARING; 110 (FROM 610 EXISTING) OR 18.03%

HUNTER LOWLAND REDGUM

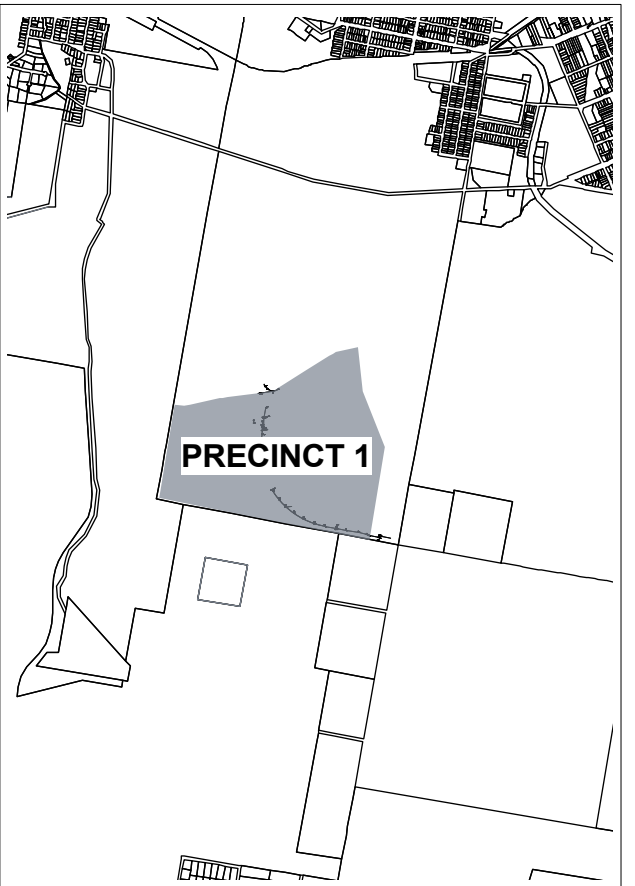
TOTAL HUNTER LOWLAND REDGUM FOREST TO BE REMOVED
TOTAL 59396.15 sq m

TOTAL HUNTER LOWLAND REDGUM FOREST TO BE RETAINED
TOTAL 66881.14 sq m

LEGEND

- Indicative Lot Boundary
- Precinct One boundary
- Existing 2 metre contour
- Proposed sewer trunk main footprint
- Existing boundary for DEC Deferred Area 1
- 7(b) Environmental Protection Zone
- Hunter Lowland Redgum Forest (EEC)
- Lower Hunter Spotted Gum-Ironbark Forest/Hunter Lowlands redgum Forest
- Hunter Lowland Redgum Forest (EEC) to be retained in Precinct 1
- Hunter Lowland Redgum Forest (EEC) to be removed in Precinct 1
- Lower Hunter Spotted Gum-Ironbark Forest/Hunter Lowlands redgum forest to be retained in Precinct 1
- Lower Hunter Spotted Gum-Ironbark Forest/Hunter Lowlands redgum forest to be removed in Precinct 1
- Rutidosis heterogama to be retained
- Mature trees to be retained
- Mature trees to be potentially removed

KEYPLAN



- Hollow bearing trees to be retained
- Hollow bearing trees to be potentially removed
- Corymbia maculata to be retained
- Corymbia maculata to be removed
- Eucalyptus tereticornis to be retained
- Eucalyptus tereticornis to be removed
- Retained vegetation
- Reinstatement Zone
- Reinstatement Zone to unvegetated areas
- Potential drainage easement
- 20 metre IPZ
- 20 metre temporary APZ

REFERENCES

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Drawing	CONCEPT PLAN - PRECINCT ONE 20mtr Conservation Setback Option
Scale	1:3000 @ A1 / 1:6000 @ A3
Project No.	07502428.10
Drawn	CY

2297.10
L03K



Bushfire Protection

The subdivision layout and design has included the provision of Asset Protection Zones (APZ's) that are suitable for the management of bushfire hazard. These bushfire requirements have been addressed in detail in the accompanying bushfire assessment prepared by RPS HSO (Appendix N).

The provision of the subdivision layout has allowed the APZ's to be integrated into the design to maximise the efficiency of the layout and the provision of a framework that avoids the creation of conflict with bushland that is desirable to be retained. That is areas of vegetation that are identified to be retained can be retained and will not be threatened by a later imposition of APZ's as is the case with the current approach to the management of approval on the site.

At the northern end of the HEZ Pty Ltd lands a connection to Weston is proposed, described as Station Street extension. This new section of road connects the currently under construction road that will terminate at the unformed Lang Street road reserve with the existing termination of Station Street at the northern boundary of the HEZ Pty Ltd lands. The connection to Station Street provides a second exit from the site and connects the estate into the existing road network. This access has been contemplated as part of the masterplanning of the estate since the inception of the 4(h) zone. The connection into Weston has been assessed on its impact on the traffic networks capacity to accommodate traffic. The connection has been pursued as it is an exiting legal access to the land and provides the only viable access over which control is available and has the least impact upon residential areas.

Industrial and Employment Growth

The approval of the subdivision layout will facilitate the provision of Industrial and employment growth due to the increased level of certainty provided. The current development of the estate is hindered by the uncertainty engendered by the planning framework and the restrictive approach to subdivision.

The upfront estate design providing the required and desirable vegetation buffers, comprehensive water management strategies and integrated solutions flora and

fauna and bushfire strategies will deliver industrial lots upon which development can be undertaken.

The provision of all of the required trunk utility services is well advanced meaning serviced lots will be provided to the market. Without the certainty of a road and lot pattern the provision of reticulated services to individual lots is highly constrained and difficult to deliver in an efficient and cost effective manner.

The provision of the layout for Precinct 1 delivers this certainty and maximises the efficiency of the delivery of these necessary utility services.

Ensuring the Precinct layout maximises efficiencies having regard to environmental and utility considerations provides employment generating land to meet the projected needs of the Hunter Region