

**PROPOSED
MIXED USE DEVELOPMENT
134 – 144 PITT STREET, REDFERN
*Assessment of Traffic and
Parking Implications***

June 2017
(Rev E)

Reference 16079

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1. INTRODUCTION

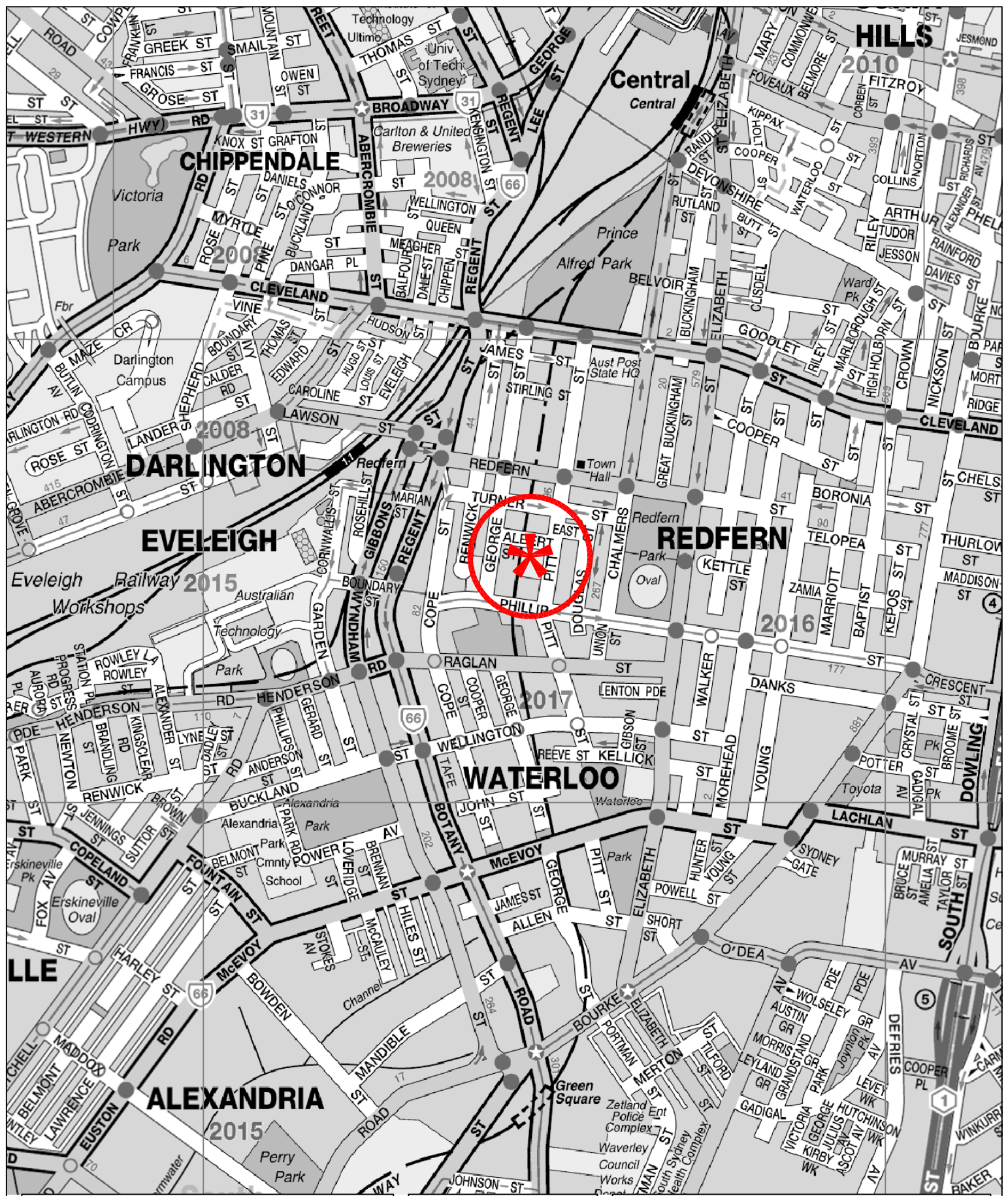
This report has been prepared for Kaymet Pty Ltd to accompany revised plans for an application to the Department of Planning and Environment for a proposed mixed use development on the former Rachel Forster Hospital site at 134-144 Pitt Street in Redfern (Figure 1).

The hospital was closed in 2000 and apart from a Community Health facility, which operated from the building fronting Albert Street and the site has largely remained disused.

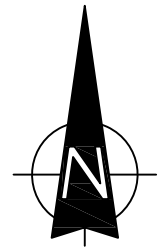
The large site occupies an area of some 6,923m² and the proposal will involve a group of 4 buildings accommodating a total of 213 apartments, with neighbourhood retail and café elements together with basement parking.

The purpose of this report is to:

- * describe the site and the proposed development scheme
- * describe the road network serving the site and the prevailing traffic conditions
- * assess the adequacy of the proposed parking provision
- * assess the potential traffic implications
- * assess the suitability of the proposed vehicle access, internal circulation and servicing arrangements



LEGEND



LOCATION

FIG 1

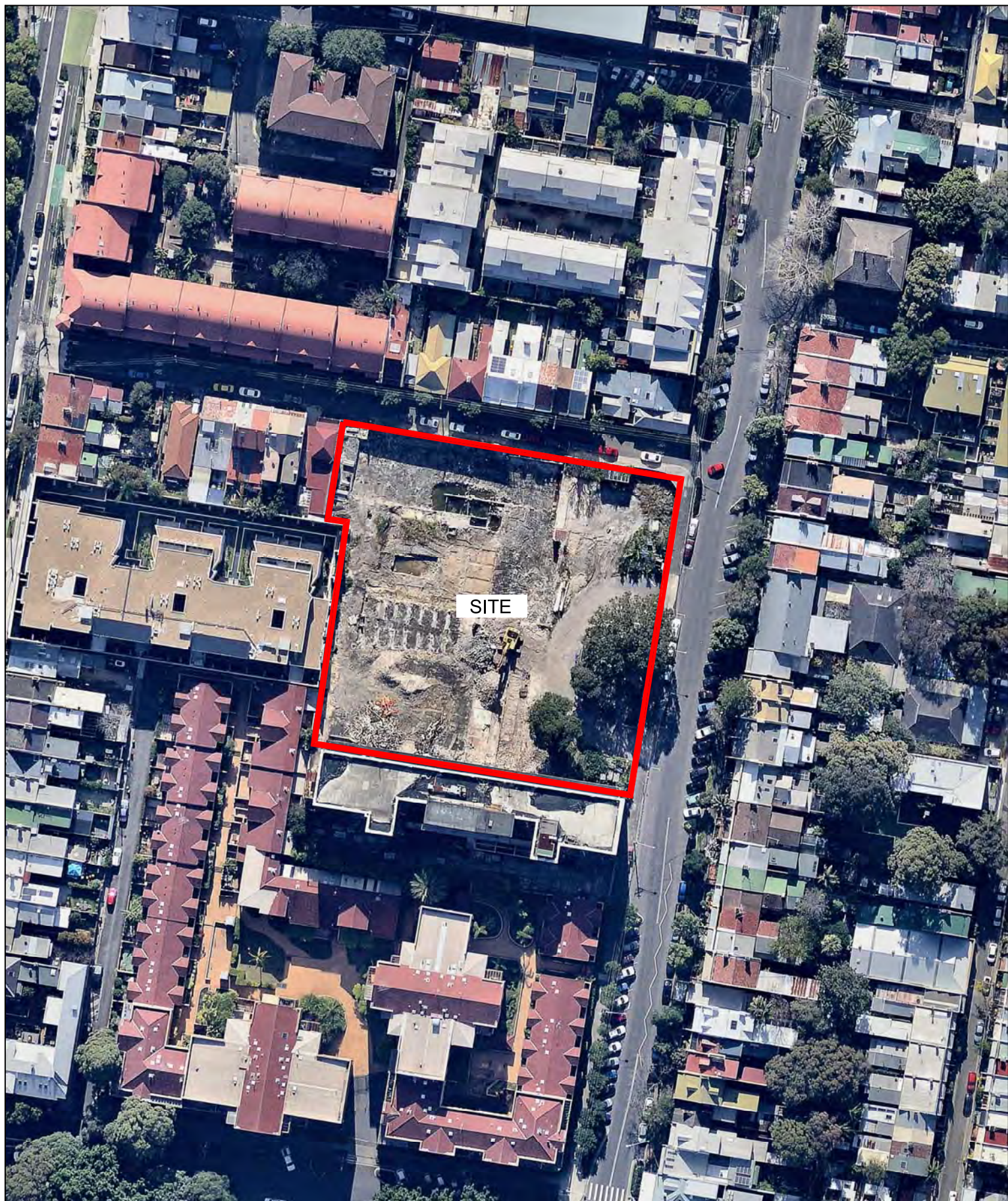
2. PROPOSED DEVELOPMENT SCHEME

2.1 SITE, CONTEXT AND EXISTING USE

The site (Figure 2) has frontages to western side of Pitt Street and southern side of Albert Street whilst vehicular access is provided by 3 driveways located on the Pitt Street frontage and one on Albert Street.

The surrounding landuses comprise of a mix of contemporary medium/high density residential and older terrace style housing. Other notable landuses in the vicinity of the site include:

- * the retail/commercial strip along Redfern Street (to the north) and Regent Street (to the west)
- * the former Redfern Public School, which occupies a large parcel of land on the corner of George Street, Phillip Street and Cope Street
- * the high rise Public Housing towers on the southern side of Phillip Street
- * Redfern Railway Station which is located some 500 – 600 metres north-west of the site
- * the South Sydney Rugby League Club and large Redfern Park a short distance to the east



LEGEND



SITE

FIG 2

2.2 PROPOSED DEVELOPMENT

The development scheme involves the demolition of existing buildings and excavation of the greater part of the site to provide for basement parking and level building platforms. The proposed development comprises:

6 x Studio apartments

95 x one-bedroom apartments

94 x two-bedroom apartments

18 x three-bedroom apartments

Total: 213 apartments (some 50 affordable)

Retail / Café 320.1m²

A total of 155 parking spaces (plus 2 car share and 3 service spaces) will be provided in two basement levels with vehicular access on Pitt Street.

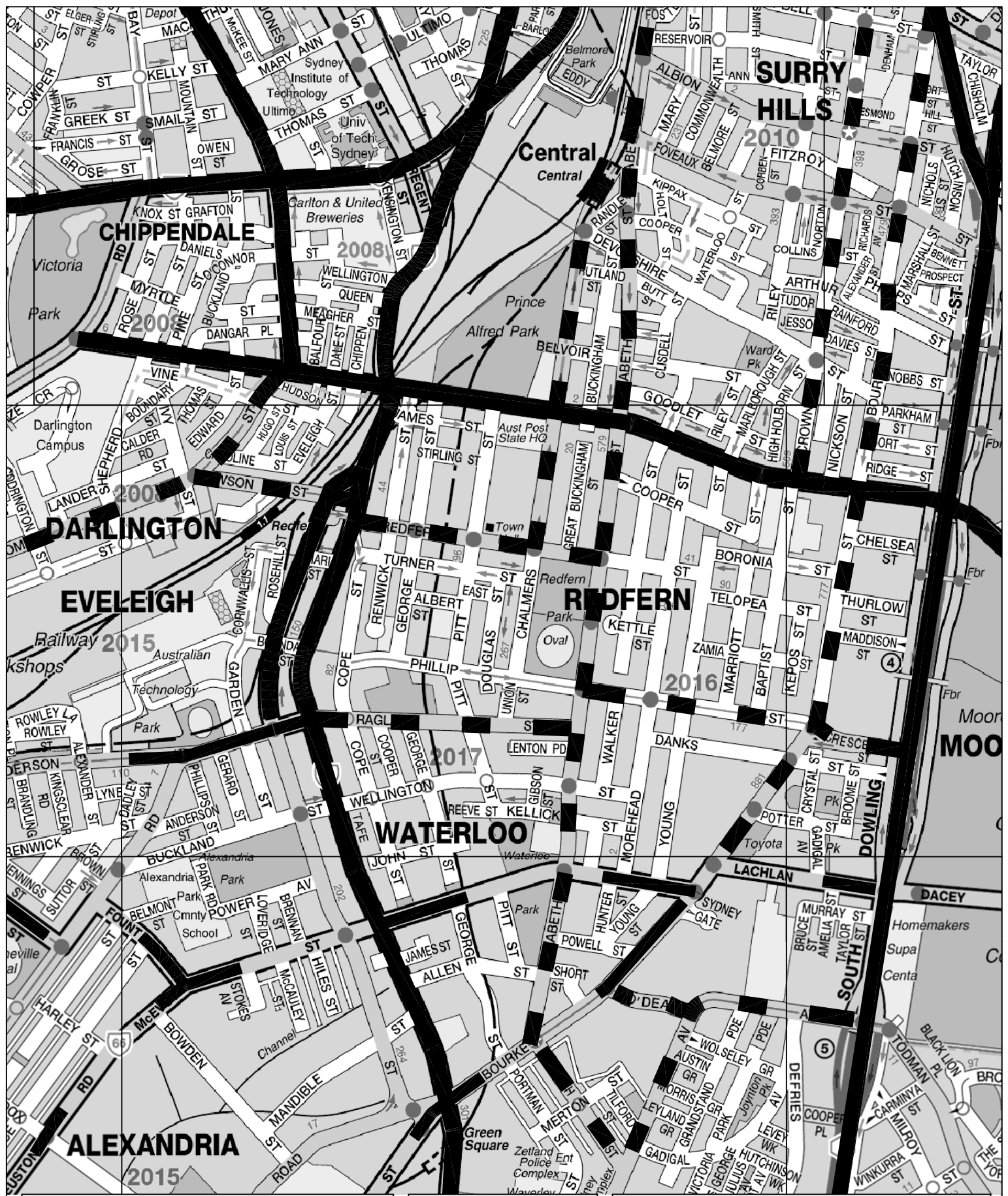
Details of the development scheme are provided on the revised plans prepared by Tony Owen Partners which accompany the Development Application and are reproduced in part in Appendix A.

3. ROAD NETWORK AND TRAFFIC CONDITIONS

3.1 ROAD NETWORK

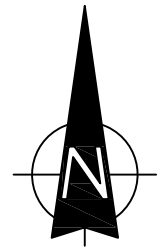
The road network serving the site (Figure 3) comprises:

- * *Cleveland Street* – a State Road and east/west arterial route linking with Anzac Parade in the east and City Road in the west
- * *Regent Street / Botany Road* – a State Road and north/south arterial route which provides connection to/from the City
- * *Elizabeth Street* – a Regional Road and north/south collector route which provides connection to and from the City
- * *Chalmers Street* – Regional Road and one-way (northbound) collector route between Redfern Street and Eddy Avenue while to the south of Redfern Street it is a local access road
- * *Redfern Street* – a Regional Road and collector route between Elizabeth Street and Regent Street
- * *Pitt Street* – a local access road
- * *Phillip Street* – a local access road
- * *Albert Street* – a local access road



LEGEND

- ARTERIAL**
- SUB-ARTERIAL**
- COLLECTOR**



ROAD NETWORK

FIG 3

3.2 TRAFFIC CONTROLS

The traffic controls which have been introduced to the road network both within and in the immediate vicinity of the Study Area are detailed in Figure 4 overleaf and comprise:

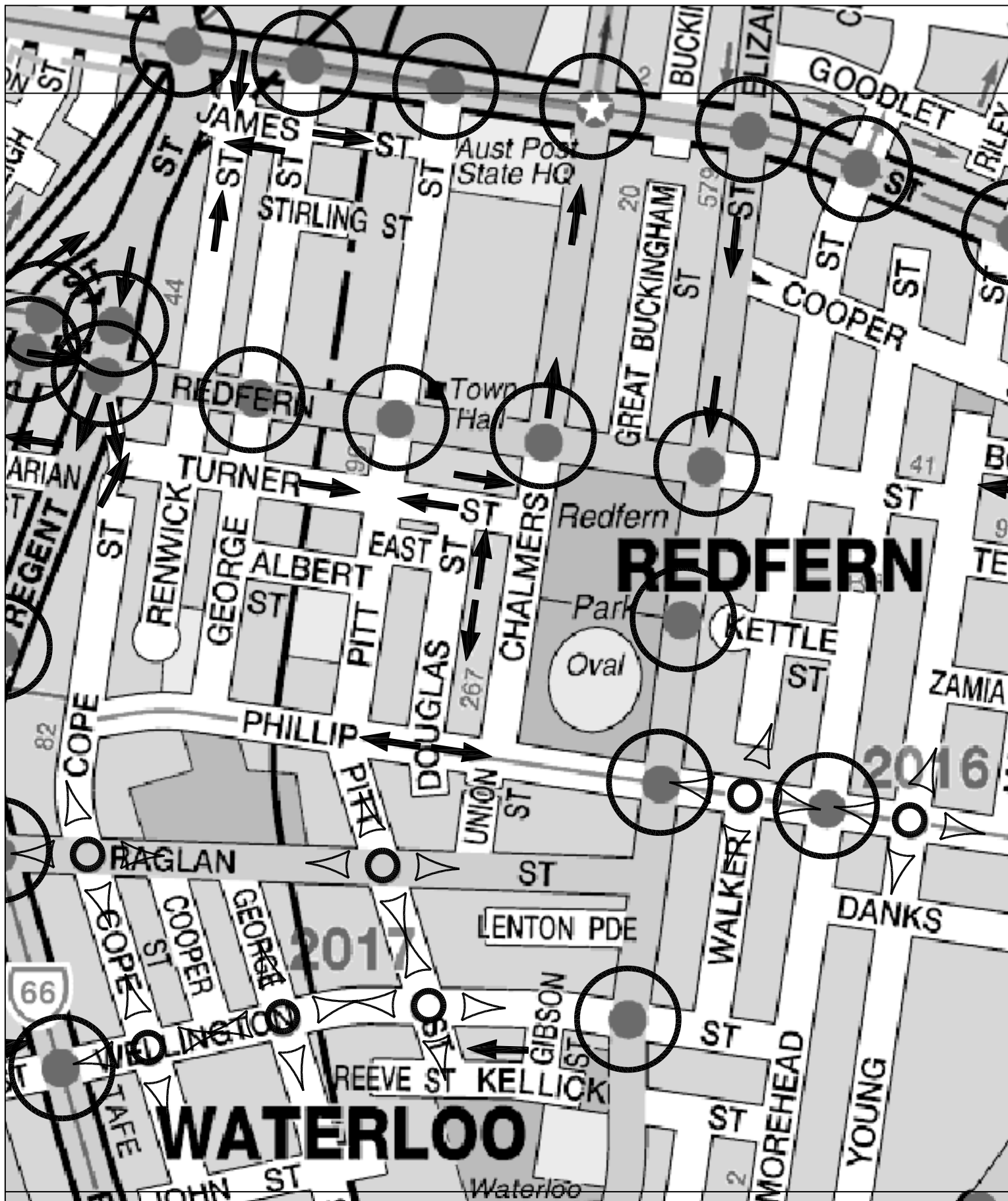
- * the traffic signal control at the intersections of:
 - Redfern Street and Regent Street
 - Regent Street and Lawson Street
 - Redfern Street and George Street
 - Redfern Street and Pitt Street

- * the ONE WAY traffic restrictions in:
 - Chalmers Street north of Redfern Street (northbound)
 - Phillip Street (westbound) between Chalmers Street and Pitt Street
 - Wells Street (eastbound) between Regent Street and Pitt Street
 - Wells Street (westbound) between Pitt Street and Chalmers Street
 - Turner Street (eastbound) between Cope Street and Pitt Street
 - Turner Street (westbound) between Douglas Street and Pitt Street

- * the roundabouts at the intersections of Raglan Street and Cope Street and Raglan Street and Pitt Street

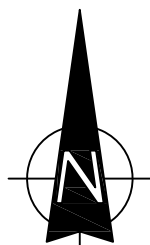
- * the GIVE WAY and STOP sign control in:
 - Renwick Street at Redfern and Turner Streets
 - Turner Street at George and Pitt Streets
 - Wells Street at Pitt Street
 - Renwick Street at Redfern and Turner Streets
 - Turner Street at George and Pitt Streets
 - Phillip Street at Pitt Street

- * the marked foot crossings in:
 - George Street north of Phillip Street
 - Phillip Street east of George Street
 - Pitt Street north of Phillip Street



LEGEND

- TRAFFIC SIGNAL CONTROL
- ROUNDABOUT
- RESTRICTED TURNING MOVEMENT



TRAFFIC CONTROLS

FIG 4

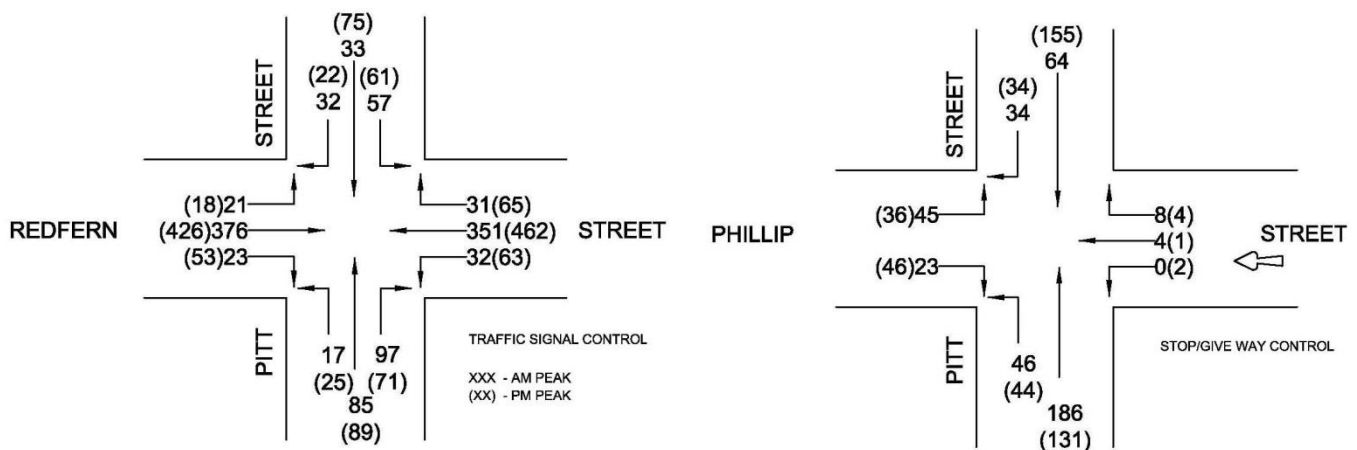
3.3 TRAFFIC CONDITIONS

An indication of the prevailing traffic conditions on the road network in the vicinity of the site is provided by traffic volume data published by the Roads and Maritime Services and traffic surveys undertaken as part of this study.

The Roads and Maritime Services data is expressed in the form of Average Annual Daily Traffic (AADT) and relevant to the site are the following:

Location	AADT
Botany Road, south of Raglan Street	25,331
Regent Street, South of Cleveland Street	48,856

Traffic surveys have been undertaken at the intersections of Pitt Street/Redfern Street and Pitt Street/Phillip Street during the AM and PM peak periods, and the results are summarised in the following diagrams:



An assessment of the operational performance of the Pitt Street and Redfern Street intersection was modelled using the computer software model SIDRA. The result of this assessment is summarised in the table overleaf whilst the criteria for interpreting the results is shown overleaf.

Criteria for Interpreting Results of SIDRA Analysis

1. Level of Service (LOS)

LOS	Traffic Signals and Roundabouts	Give Way and Stop Signs
'A'	Good	Good
'B'	Good with acceptable delays and spare capacity	Acceptable delays and spare capacity
'C'	Satisfactory	Satisfactory but accident study required
'D'	Operating near capacity	Near capacity and Accident Study required
'E'	At capacity; at signals incidents will cause excessive delays. Roundabouts require other control mode	At capacity and requires other control mode
'F'	Unsatisfactory and requires additional capacity	Unsatisfactory and requires other control mode

2. Average Vehicle Delay (AVD)

The AVD provides a measure of the operational performance of an intersection as indicated on the table below, which relates AVD to LOS. The AVD's listed in the table should be taken as a guide only as longer delays could be tolerated in some locations (ie inner city conditions) and on some roads (ie minor side street intersecting with a major arterial route).

Level of Service	Average Delay per Vehicle (secs/veh)	Traffic Signals, Roundabouts	Give Way and Stop Signs
A	Less than 14	Good operation	Good operation
B	15 to 28	Good with acceptable delays and spare capacity	Acceptable delays and spare capacity
C	29 to 42	Satisfactory	Satisfactory but accident study required
D	43 to 56	Operating near capacity	Near capacity and accident study required
E	57 to 70	At capacity; at signals incidents will cause excessive delays. Roundabouts require other control mode	At capacity and requires other control mode

3. Degree of Saturation (DS)

The DS is another measure of the operational performance of individual intersections.

For intersections controlled by **traffic signals**¹ both queue length and delay increase rapidly as DS approaches 1, and it is usual to attempt to keep DS to less than 0.9. Values of DS in the order of 0.7 generally represent satisfactory intersection operation. When DS exceeds 0.9 queues can be anticipated.

For intersections controlled by a **roundabout or GIVE WAY or STOP signs**, satisfactory intersection operation is indicated by a DS of 0.8 or less.

¹ the values of DS for intersections under traffic signal control are only valid for cycle length of 120 secs

Location	AM			PM		
	LOS	DS	AVD	LOS	DS	AVD
Pitt Street at Redfern Street	B	0.55	11.5	B	0.77	13.0

The analysis indicates that this intersection is operating with a good level of service and with negligible congestion or delay apparent on any vehicle approach.

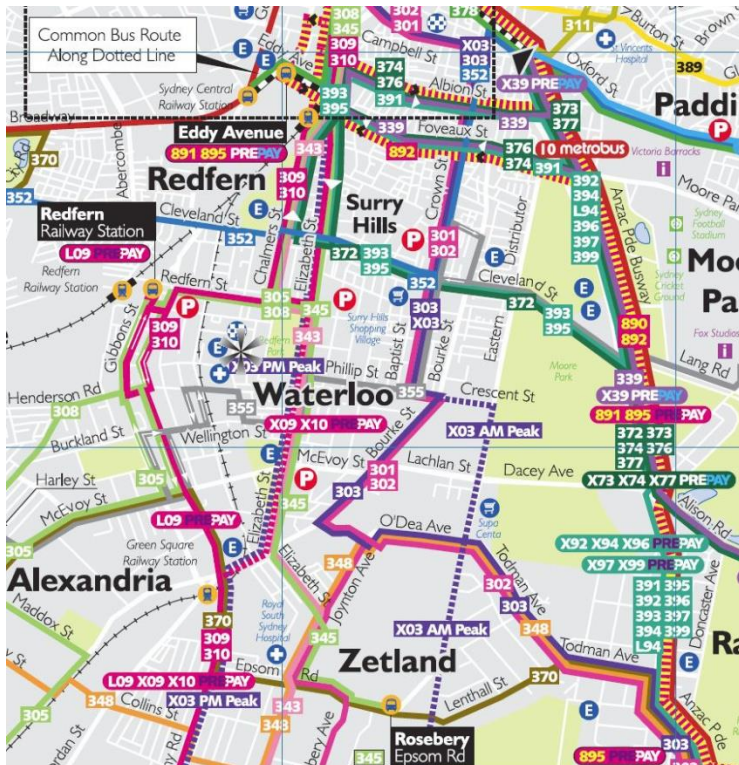
3.4 TRANSPORT SERVICES

The development site is highly accessible to both road and rail based public transport services being less than 400 metres from high frequency bus services on nearby Redfern Street and Regent Street and approximately 500-600 metres from Redfern Railway Station.

Details of the bus routes which operate in the vicinity of the site are outlined in the table below.

Route №	Service Frequency	Route Description
305	Mon-Fri (Peak Hour Services)	Railway Square to Mascot via Redfern, Alexandria and Beaconsfield
308	Daily Daytime Services	Marrickville to City via St Peters, Alexandria and Redfern
309	Daily Full Time Service	Port Botany to City via Broadmeadow, Botany, Mascot and Redfern
310	Daily Full Time Service	Eastgardens to City via East Botany, Mascot and Redfern
355	Daily Daytime Service	Marrickville to Bondi Junction via Enmore, Newtown, Waterloo, Redfern, Surry Hills and Moore Park.

An extract from the overall route map is presented below, while the route maps for each service are presented in Appendix B.



Nearby Redfern Station provides access to 10 of the 11 lines on the City Rail network as well as convenient connection to Central Station and the extensive network of inter urban and country link services which operate to/from this station.



Due to the site's high level of accessibility to public transport, residents and visitors are expected to make good use of the services provided particularly for journey to work purposes.

4. PARKING

The appropriate level of carparking to be provided for the development is indicated by Council's LEP and DCP documents which specify a maximum provision as follows:

Residential Apartments

Studio	0.2 space
One-bedroom	0.4 space
Two-bedroom	0.8 space
Three-bedroom	1.1 spaces
Visitors	<30 – 0.167 space, 30-70 – 0.1 >70 – 0.05

Retail / Café 1 space per 60m² GFA

Car Share 1 space per 60 spaces

Service

Residential 1 space per 50 apartments and
0.5 space per every 50 apartments after

Retail 1 space per 350m²

Application of these rates to the proposed development scheme indicates the following:

Residential Apartments

6 x studio	1.2 space
95 x one-bedroom	38 spaces
94 x two-bedroom	75.2 spaces
18 x three-bedroom	19.8 spaces
Total:	134.2 spaces (134)
Visitors (213)	16.25 spaces (16)
Retail 320.1m ²	5.3 spaces (5)
Total:	155 spaces
Car Share	2 spaces
Service	3 spaces
Grand Total:	160 spaces

It is proposed to provide a total of 155 parking spaces including 32 'accessible' spaces plus 2 car share and 3 service spaces in the development reflecting the Council criteria.

The DCP also specified a requirement for bicycle and motorcycle parking of:

Bicycles

Residential Apartments

1 space per apartment for residents

1 space per 10 apartments for visitors

Retail/Café

Staff - 1 per 250m²

Customers - 2 plus 1 per 100m²>100m²

Motorcycles

1 space per 12 car spaces

Accordingly, provision will be made to accommodate 245 bicycles (213 resident, 21 res. visitors and 11 for retail) and 14 motorcycle spaces.

5. TRAFFIC

The traffic generation associated with the proposed development has been determined with reference to the Roads and Maritime Services Technical Direction TDT 2013-4b.

For residential developments with access to rail and bus services, the RMS document specifies the following peak hour generation rate per apartment:

AM	PM
0.19 vtpd	0.15 vtpd

N.B. Because of the constrained parking provision and “affordable” element the traffic generation outcome would be somewhat lower in reality.

Application of this rate to a development of 213 apartments indicates a peak period traffic generation of some 41 and 32 vtpd respectively. The proposed retail/café spaces will essentially be utilised by tenants arriving in the morning and departing in the evening with overall traffic distribution being as follows:

	AM		PM	
	IN	OUT	IN	OUT
Residents	9	32	26	6
Retail / Café	4	-	-	4
Total	13	32	26	10
Grand Total:	45 vtpd		36 vtpd	

Consent was previously granted for a development containing 158 apartments with no “affordable” apartments and 170 parking spaces. The Traffic Assessment¹ which accompanied that application assessed the projected traffic generation as follows:

		AM		PM	
		IN	OUT	IN	OUT
		11	35	34	12
Total:		46 vtp		46 vtp	

Whilst it is now proposed to provide more apartments than in the previously approved development there will now be only 150 resident and visitor parking spaces (compared to 170 spaces) and there will be a significant element of “affordable housing” (i.e. residents with lower car ownership/use). It is apparent from this assessment that the traffic generation will be slightly less (i.e. to that of the previously approved development) in the AM peak but significantly less in the PM peak.

It follows that the traffic impact on the access intersections will be less than that of the previously approved development and significantly less than that of the previous hospital use and therefore there is no basis for the request to undertake traffic modelling as suggested by Council.

¹ *Proposed Residential Development
Former Rachel Foster Hospital
134-144 Pitt Street, Redfern
Assessment of Traffic and Parking Implications
TTPA 2006*

6. ACCESS, INTERNAL CIRCULATION AND SERVICING

ACCESS

Vehicle access for the basement car park will be provided by a 6.0m wide combined ingress/egress driveway located on Pitt Street at the southern site boundary. This driveway will accord with the design requirements of AS2890.1 and there will be good sight distances available at this location.

INTERNAL CIRCULATION

The design of the carpark will provide generous aisles and maneuvering areas with flexible two-way circulation arrangements. The ramps, aisles, bays, head room etc will comply with the requirements of AS2890.1 and 6.

SERVICING

Refuse will be collected from the Building A Storage room on Pitt Street by Council's Collection vehicle as indicated in the Waste Consultant report and as shown on the plan in Appendix A. There will be 3 service vehicle spaces provided in the basement which can be used by delivery vans and small furniture vans (up to B99's), while service personnel will also be able to use the visitor spaces. Occasional larger service vehicles (e.g. furniture movements) will be reliant on the available kerbside parking in the area.

7. ISSUES

The COS letter to Planning and Environment NSW raised a number of parking and traffic issues and they are responded to in this report and in the following.

CAR PARKING

Council stated that under SLEP no more than 154 parking spaces are permitted. The apartment mix has since been amended and this reflected a revised upper limit of 155 spaces. It is therefore proposed to provide 155 resident, visitor and retail spaces along with 2 car share and 3 service spaces to accord with these criteria.

SERVICING

Council stated that for 218 apartments and 628m² of retail provision should be made for refuse to be collected within the site. The proposal is for 213 apartments and some 320m² of retail and it is not considered to be feasible to accommodate Council's large refuse truck in the basement due to the headroom required and the ramp grades required.

SUSTAINABLE TRANSPORT

The site is located within easy walking distance of railway services (Redfern and future Waterloo Stations), bus services, retail and entertainment facilities. The bicycle and motorcycle provision will comply with SLEP and the carparking provision will be constrained. The mode share and sustainable transport outcomes will be determined by these overriding influences and there is no point in specifying a modal target.

TRAFFIC MODELLING

The projected traffic generation of the modified development scheme is less than that assessed for the previously approved development. It follows that no additional modelling as suggested by Council is required.

8. CONCLUSION

The traffic and parking assessment undertaken for the proposed mixed use development on the site of the former Rachel FASTER Hospital at 134-144 Pitt Street has concluded that:


- * the traffic generation of the proposed development will be less than that associated with the former use and will not present any adverse traffic implications
- * the proposed parking provision will comply with Council's LEP and DCP criteria
- * the proposed access, internal circulation and parking arrangements will be suitable and appropriate

APPENDIX A

DEVELOPMENT PLANS

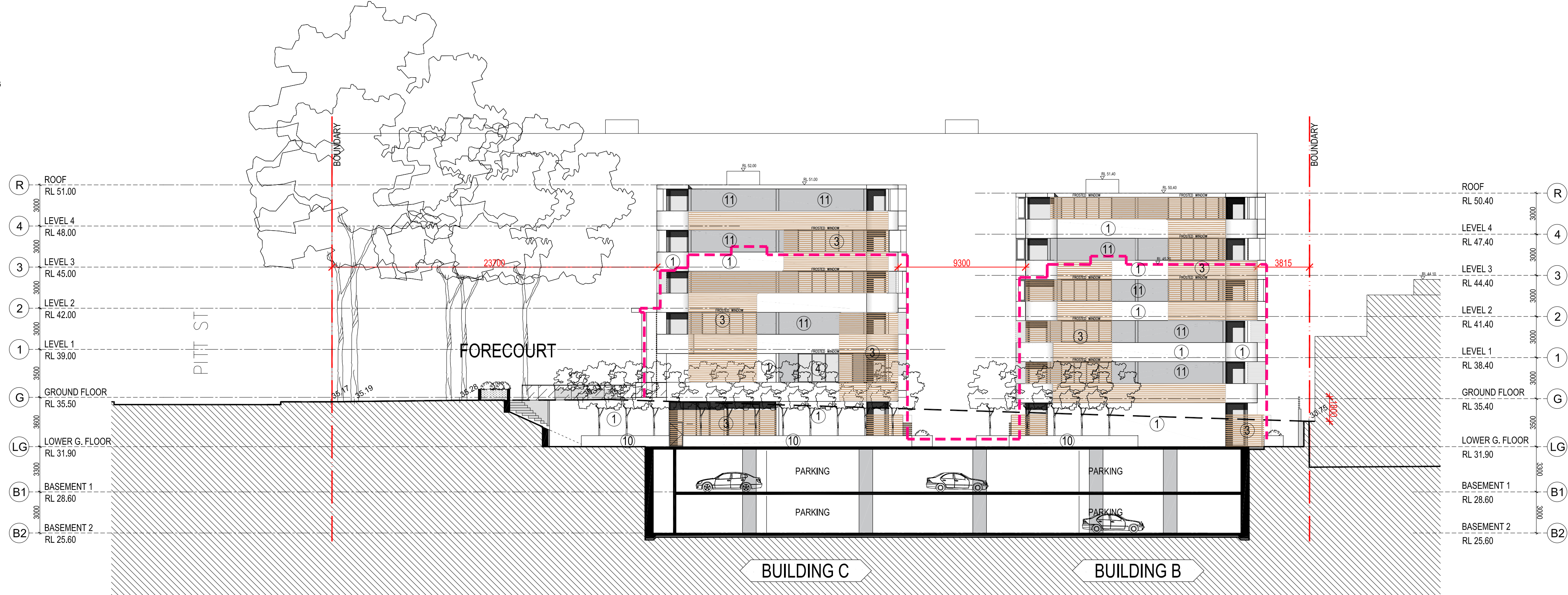
- ① RENDERED AND PAINTED MASONRY / CONCRETE WALL - DULUX COLOUR NATURAL WHITE
- ② ALUMINUM PANEL - ALUCOBOND LIGNO RUBRA ULTIMIS D 8003
- ③ POWDER COATED ALUMINUM LOUVRE - DULUX COLOUR HOT GINGER P10F9
- ④ POWDER COATED ALUMINUM WINDOW FRAME DULUX COLOUR OTTO'S BOY PG1G7 - CLEAR GLAZING
- ⑤ POWDER COATED ALUMINUM FENCE - DULUX COLOUR HOT GINGER P10F9
- ⑥ RENDERED AND PAINTED MASONRY / CONCRETE - DULUX COLOUR NATURAL WHITE
- ⑦ RENDERED AND PAINTED MASONRY / CONCRETE COLUMN - DULUX COLOUR HOT GINGER P10F9
- ⑧ POWDER COATED ALUMINUM VENTILATION LOUVRE - DULUX COLOUR NATURAL WHITE
- ⑨ POWDER COATED ALUMINUM BALUSTRADE DULUX COLOUR OTTO'S BOY PG1G7 - CLEAR GLAZING
- ⑩ RENDERED AND PAINTED MASONRY / CONCRETE PLANTER BOX - DULUX COLOUR NATURAL WHITE
- ⑪ RENDERED AND PAINTED MASONRY / CONCRETE WALL - DULUX COLOUR GRAINGER PG1G3



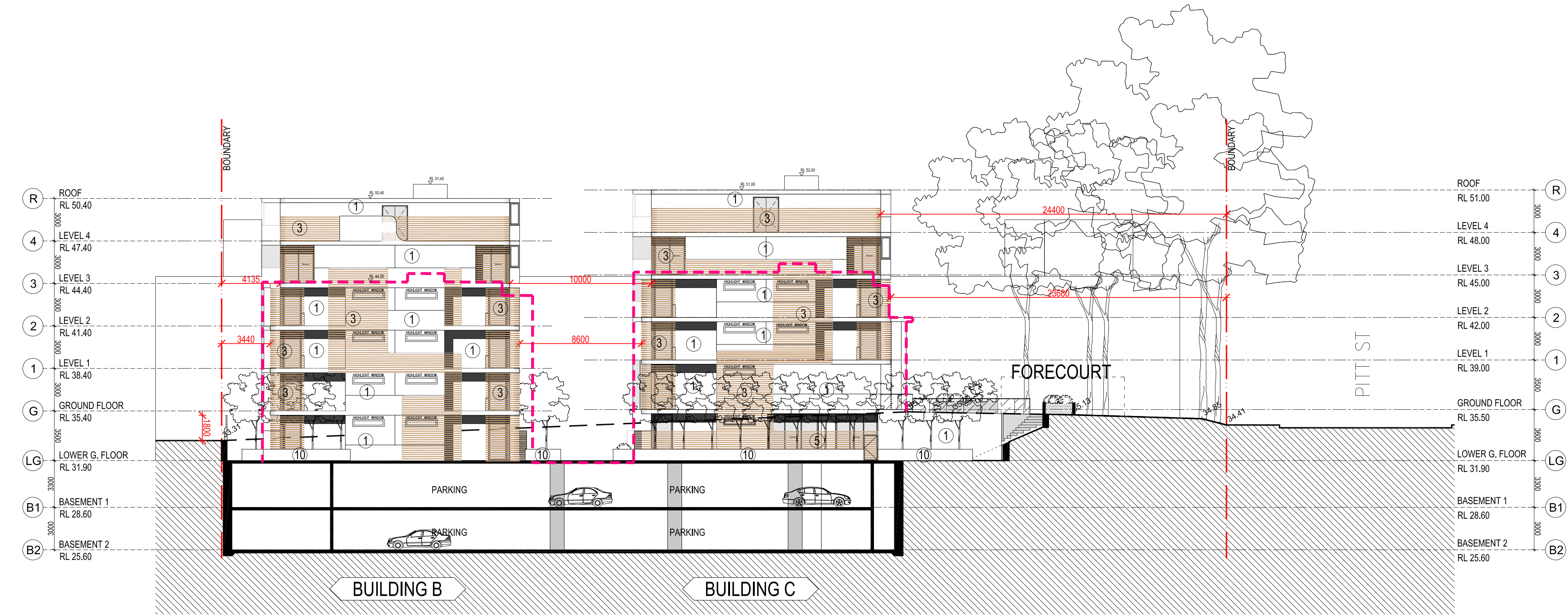
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⑪ RENDERED AND PAINTED MASONRY / CONCRETE WALL - DULUX COLOUR GRAINGER PG1G3



NORTH ELEVATION - BUILDING B.C



SOUTH ELEVATION - BUILDING B.C

Rev.	Description	By	Date
A	DA SUBMISSION	DC	SEPT 2016

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Legend

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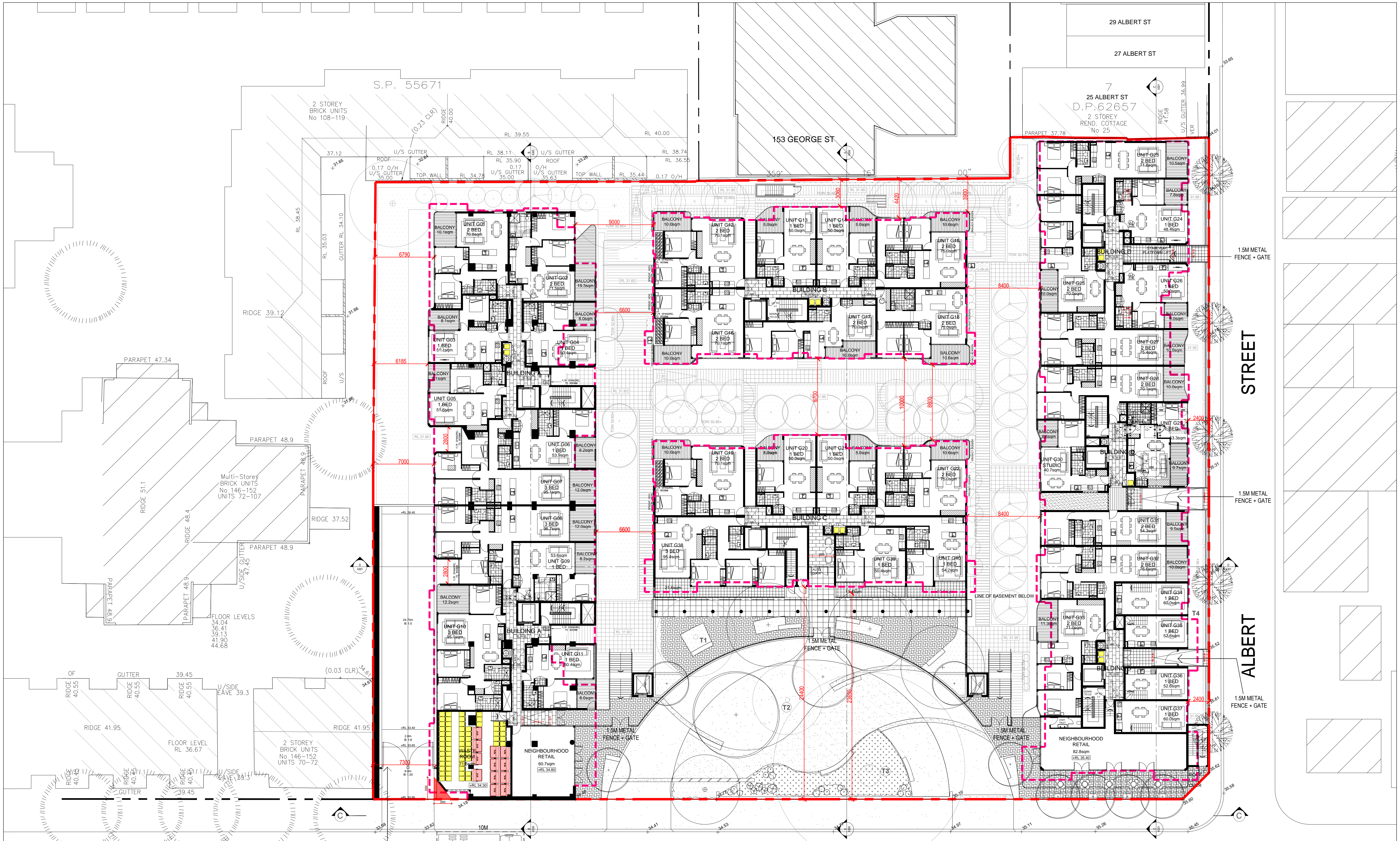
Key plan

Scale 1:200 @ A1 - 1:400 @ A3
Scale 1:200 @ A1 - 1:400 @ A3
Date: SEPT 2016
Rev: 10

Drawing title
NORTH & SOUTH ELEVATION
Project no. 956
Drawing no. A204
Rev A
Drawing status: DEVELOPMENT APPLICATION

Project no. 956
Drawing no. A204
Rev A
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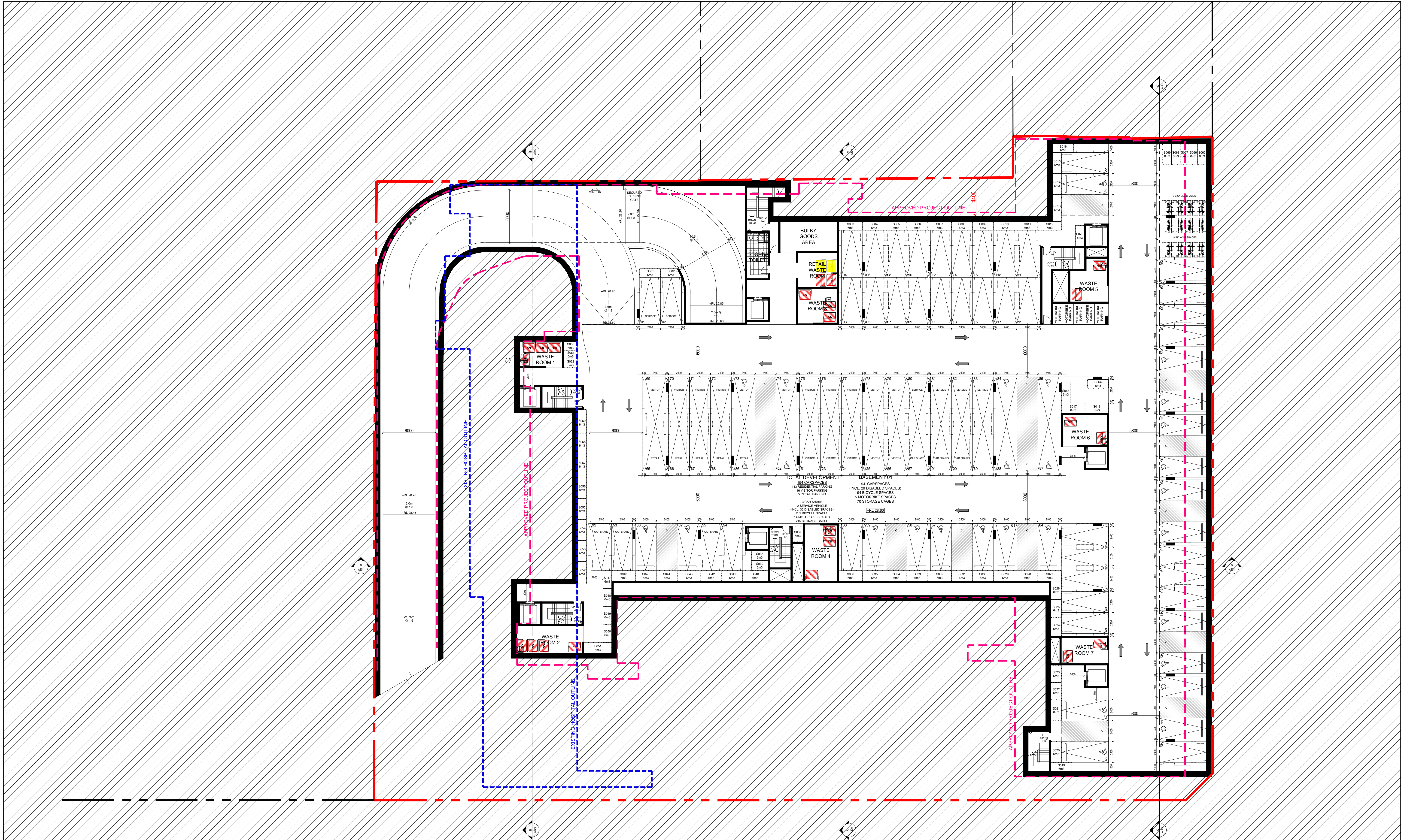
PITT STREET

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GROUND FLOOR PLAN

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UPPER BASEMENT PLAN

Rev.	Description	By	Date	Disclaimer	Notes	Legend	Consultants	Client	Architects	Project	Key plan	Scale 1:200	Project no. 956	Drawing no. A091	Rev. B
A	MODIFIED PROJECT SUBMISSION	DC	SEPT 2016	"THESE DRAWINGS ARE PRELIMINARY DRAWINGS AND ARE SUBJECT TO CHANGE WITHOUT NOTICE DURING THE COURSE OF THE PROPOSED DEVELOPMENT AND IN CONSULTATION WITH COUNCIL. SUBMISSION OF THE DRAWINGS DOES NOT CONSTITUTE A REPRESENTATION OR WARRANTY BY THE DEVELOPER OR ITS SERVANTS, AGENTS OR CONTRACTORS THAT THE DRAWINGS ARE FINAL AND THAT THE PROPOSED DEVELOPMENT WILL TAKE PLACE IN ACCORDANCE WITH THESE DRAWINGS."	WHERE ANY DISCREPANCY EXISTS BETWEEN FIGURED AND SCALED DIMENSIONS, THE FIGURED DIMENSIONS SHALL PREVAIL. BUILDER TO CHECK ALL SITE DIMENSIONS PRIOR TO FABRICATION OF TUNNELS		dmpps	KAYMET CORPORATION PTY LTD	tony brown ptbns	RACHEL FORSTER		Scale 1:200	Project no. 956	Drawing no. A091	Rev. B
B	FOR COORDINATION	TL	MAY 2017	BEFORE CARRYING OUT ANY WORK ENSURE THAT THE DRAWINGS USED CARRY THE LATEST AMENDMENT No.	THESE DOCUMENTS AND ANY WORKS EXECUTED AS A RESULT OF THESE DOCUMENTS SHALL BE SUBJECT TO THE CURRENT CONDITIONS SET OUT IN THE CONDITIONS OF ENGAGEMENT					Project address 134 - 144 PITT STREET, REDFERN					MODIFIED PROJECT

APPENDIX B

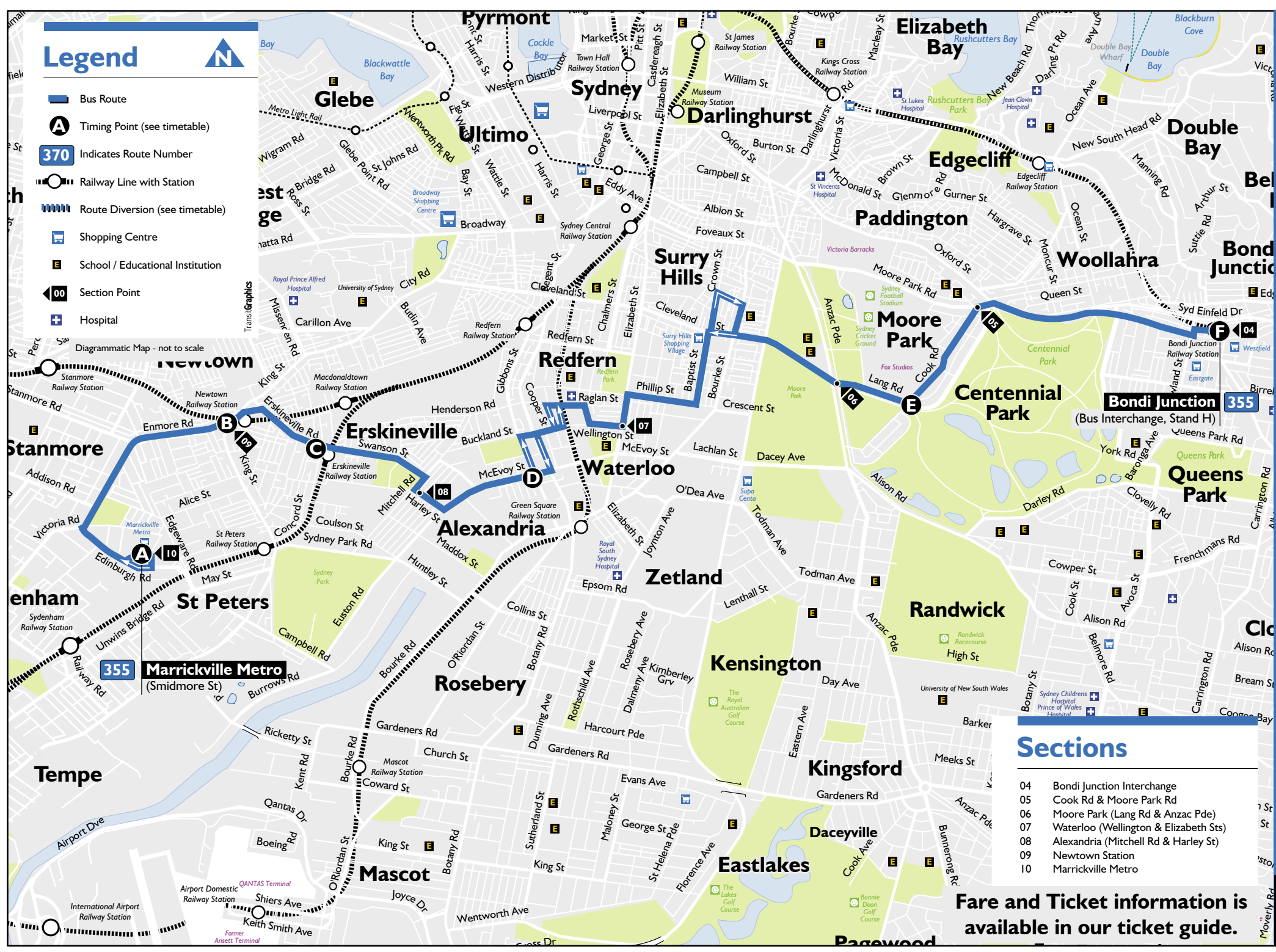
BUS ROUTES

Legend



- Bus Route
- Timing Point (see timetable)
- Indicates Route Number
- Railway Line with Station
- Route Diversion (see timetable)
- Shopping Centre
- School / Educational Institution
- Section Point
- Hospital

Diagrammatic Map - not to scale



Sections

- 04 Bondi Junction Interchange
- 05 Cook Rd & Moore Park Rd
- 06 Moore Park (Lang Rd & Anzac Pde)
- 07 Waterloo (Wellington & Elizabeth Sts)
- 08 Alexandria (Mitchell Rd & Harley St)
- 09 Newtown Station
- 10 Marrickville Metro

Fare and Ticket information is available in our ticket guide.





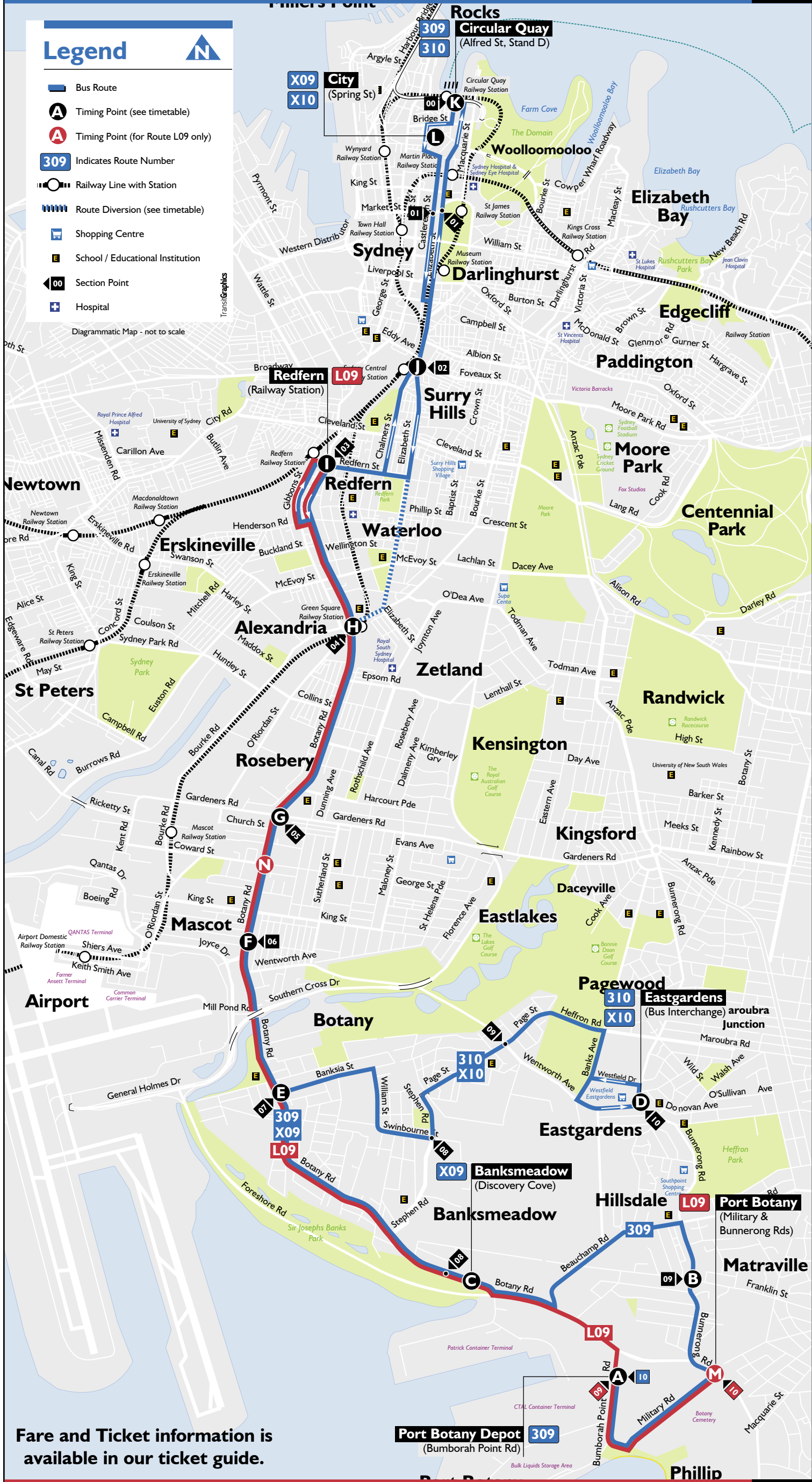
Legend



- Bus Route
- Timing Point (see timetable)
- Timing Point (for Route L09 only)
- Indicates Route Number
- Railway Line with Station
- Route Diversion (see timetable)
- Shopping Centre
- School / Educational Institution
- Section Point
- Hospital

Diagrammatic Map - not to scale

TransitGraphics



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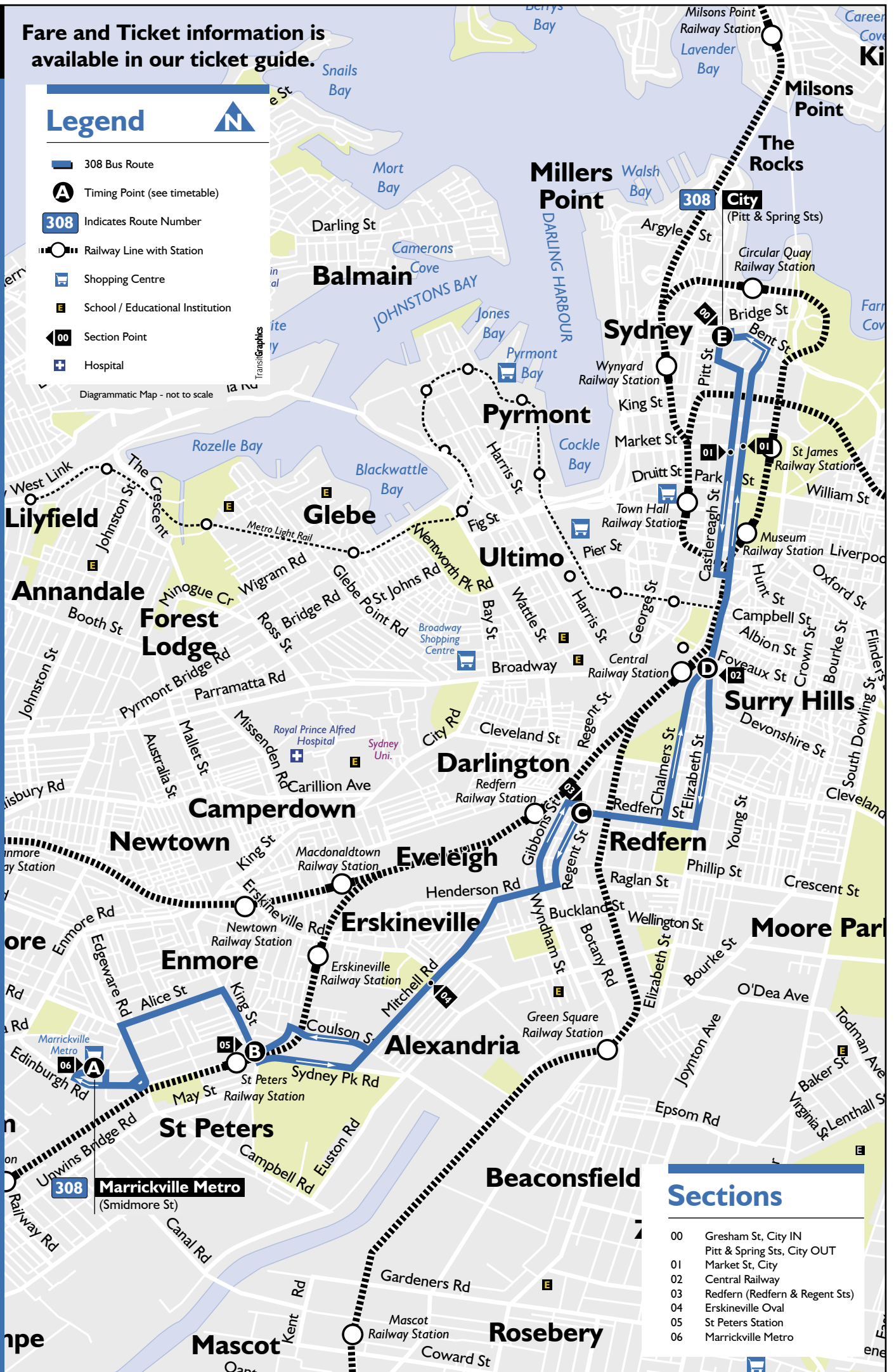
Legend



- 308 Bus Route
- Timing Point (see timetable)
- 308 Indicates Route Number
- Railway Line with Station
- Shopping Centre
- School / Educational Institution
- Section Point
- Hospital

Diagrammatic Map - not to scale

308 Marrickville Metro ◀ Alexandria ▶ City



Sections

- 00 Gresham St, City IN
Pitt & Spring Sts, City OUT
- 01 Market St, City
- 02 Central Railway
- 03 Redfern (Redfern & Regent Sts)
- 04 Erskineville Oval
- 05 St Peters Station
- 06 Marrickville Metro



Legend



305 Bus Route

A Timing Point (see timetable)

305 Indicates Route Number

⚡ Railway Line with Station

🛒 Shopping Centre

E School / Educational Institution

00 Section Point

🏥 Hospital

TransitGraphics

Diagrammatic Map - not to scale

305 Stamford Hotel ◀▶ Alexandria ◀▶ Railway Square



Sections

- 02 Railway Square
- 03 Redfern (Redfern & Regent Sts)
- 04 Alexandria (Bourke & Maddox Sts)
- 05 Mascot (Gardeners & Bourke Rds)
- 06 Stamford Hotel

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