

# Appendix 3 Draft Development Control Plan

Peter Andrews + Associates Pty Ltd

June 2008



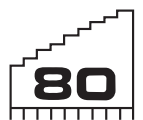
## Warner Industrial Park Concept Plan and Project Application

Precinct 14 WEZ

Sparks Rd and Hue Hue Rd

Warnervale

June 2008



TERRACE  
TOWER  
GROUP

Warner Business Park Pty Ltd  
Part of the Terrace Tower Group

# Draft Development Control Plan



## Warner Industrial Park

Sparks Rd and Hue Hue Rd  
Warnervale  
June 2008



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This plan shall be known as:

Draft Development Control Plan Warner Industrial Park 2008

This Development Control Plan (DCP) may be amended from time to time.

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### 1.0 PRELIMINARY

#### 1.1 Introduction

Precinct 14 is located at the Sparks Road – F3 Freeway Interchange in Wyong Shire. The site encompassed by the Warner Industrial Park forms the major part of Precinct 14 within the proposed Wyong Employment Zone. Precinct 14 is bounded by the F3 Freeway, Sparks Road, Hue Hue Road and Kiar Ridge Road. Warner Industrial Park forms the southern section of the Precinct with an approximate area of 104 hectares and is the subject of this Draft Development Control Plan.

Creation of new employment opportunities in this part of the Central Coast has been a major objective of State, Regional and Local planning for a number of years. The draft Central Coast Regional Strategy identifies Precinct 14 as employment lands.

An appropriate mix of uses is important in achieving job forecasts, a quality development and economic viability.

It is envisaged that the Warner Industrial Park will comprise a mix of employment generating uses. This DCP provides guidelines for development within the public and private domain, including controls for the siting and design of buildings and further subdivision to meet market demand, together with specific controls in relation to Water Sensitive Urban Design.

#### 1.2 Name of Plan and Commencement

This plan may be cited as the "Draft Development Control Plan Warner Industrial Park 2008".

This DCP was adopted by the ### on ### and came into effect on ##.

#### 1.3 The Consent Authority

Clause 3 of the *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 21)* identifies the relevant consent authority for future development of the Warner Industrial Park DCP Area.

Each Development Application will be assessed having regard to the aims and objectives of the *Environmental Planning and Assessment Act 1979*, *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 21)*, this DCP, and any other relevant policies.

#### 1.4 Land and Development Covered by This DCP

This DCP applies to the land known as the Warner Industrial Park located in Precinct 14 as shown on Figure 1. This will now be referred to as the DCP area.

#### 1.5 Purpose of This Development Control Plan

This DCP has been prepared in accordance with Part 3 of the *Environmental Planning and Assessment Act 1979* and with Part 3 of the *Environmental Planning and Assessment Regulation 2000*. The DCP provides more detailed provisions than in the *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 21)*.

Under Section 79C of the Act, the consent authority is required to take into consideration the relevant provisions of this DCP in determining an application for development.

## **1.6 Relationship with other Planning Documents**

Precinct 14 of the Wyong Employment Zone is a State Significant Site. *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 21)* is the principal Environmental Planning Instrument that applies to the DCP area.

In the event of any inconsistency between this DCP and other technical policies, guidelines or codes, this DCP will prevail in relation to development of the DCP area.

State Environmental Planning Policies apply to the DCP area where relevant. The statutory provisions of any SEPPs and Regional Environmental Plans will prevail over this DCP, unless otherwise stated.

## **1.7 Explanatory Notes**

Terms used in this DCP are defined in the *Standard Instrument – Principal Local Environmental Plan*. Section 6.0 also provides guidance to applicants for the lodgment of development applications. Further advice on lodgment procedures can be obtained from the consent authority.

## **1.8 Aims and Objectives**

The general aims relating to the role and function of the DCP area are to:

- promote the orderly use of land as a regionally significant site for employment generation;
- reduce the impact of development on natural eco systems including downstream waterways;
- encourage energy efficient design and principles of Ecologically Sustainable Development (ESD);
- encourage quality urban design and public domain appropriate to a contemporary industrial park; and
- manage the traffic and parking within the DCP area and the adjoining road network.

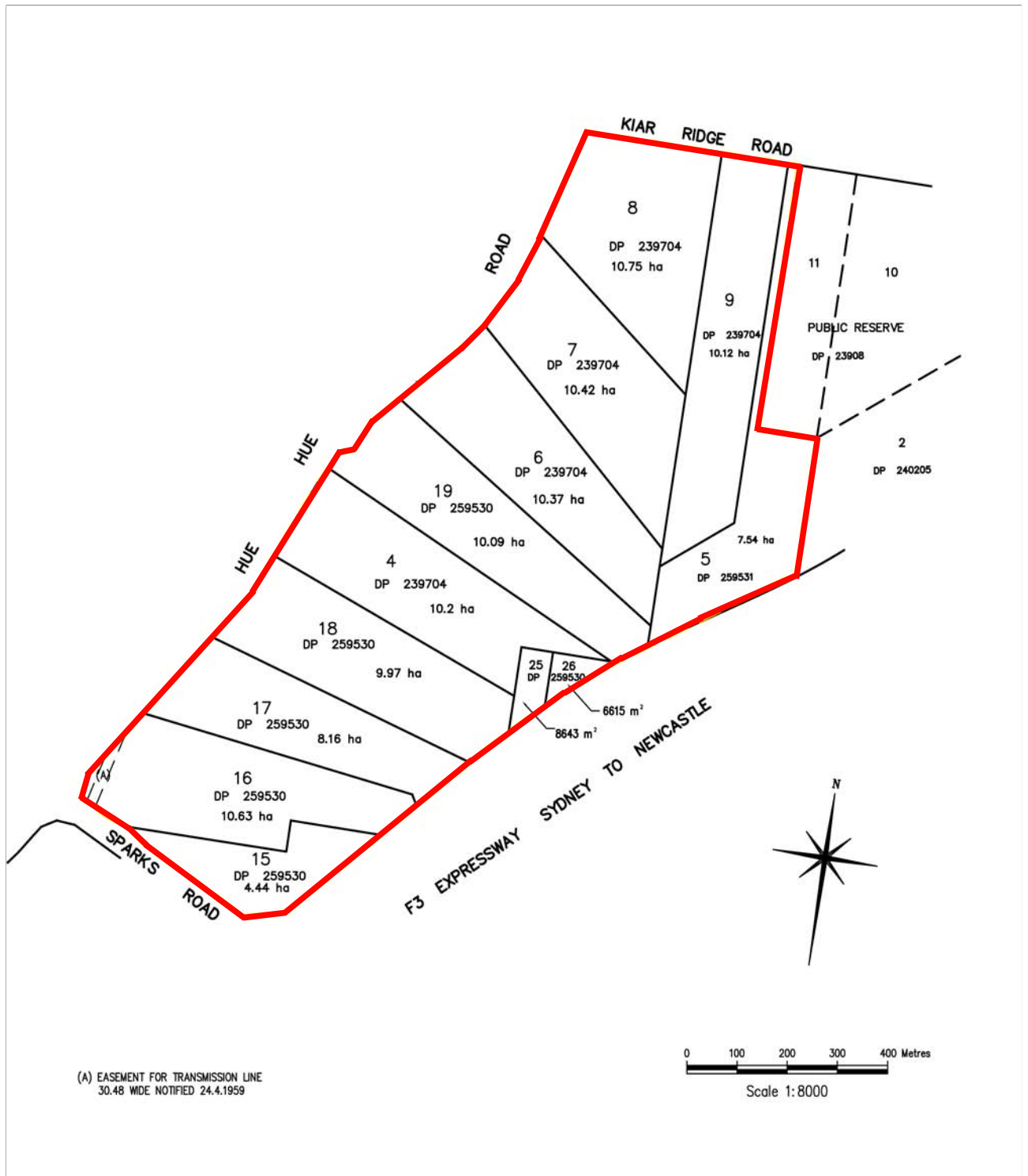
## **1.9 Development Controls**

The requirements of this DCP are expressed as Objectives and Controls. Objectives are statements that define the overriding intention of each control element. Controls provide standards that must be met by a proposal, unless the applicant can demonstrate that the objectives can be better met through the application of a different standard than that provided in the controls.

### 1.10 Variations to the Plan

In special circumstances, the Consent Authority may allow minor variations arising from detailed design considerations, if the applicant has demonstrated that such variation does not contradict the aims and objectives of this Plan and the *SEPP (Major Projects) 2005 (Amendment No 21)*.

Variations and amendments shall be supported in writing and accompanied by explanation, demonstrating the way in which the requirements of these provisions have been satisfied.



Source: Trehay Ingold Neate 2007

#### LEGEND



Land to which this Plan applies

## **2.0 PROPOSED DEVELOPMENT**

### **2.1 Development Concept and Subdivision Plan**

The Concept Plan (Figure 2) and Landscape Masterplan (Figure 3) generally guide development of the DCP area. The location of the site in relation to the F3 Freeway and its proximity in relation to other centres is likely to see a greater emphasis on industrial, warehousing and distribution type uses, but it is intended that a mix of employment generating uses should be encouraged including:

- Boat Repair Facility
- Brothels
- Conference facilities
- Depots
- Freight and Transport Facility
- Hardware and building supplies
- High Tech and Research and Development Industries
- Industries
- Industries Retail Outlet
- Landscape and garden supplies
- Light Industries
- Manufacturing
- Places of public worship
- Recreation facilities (indoor)
- Self storage units
- Transport depots
- Truck depots
- Vehicle body repair workshop
- Vehicle repair station
- Warehousing and Distribution Centres
- Ancillary support uses, such as child care centres, food and drink premises, kiosks, neighbourhood shops, general store and recreation facilities such as a gymnasium.

The subdivision pattern is intended to create regular shaped parcels where possible with flexibility to consolidate or subdivide to meet market demands

### **2.2 Landscape Masterplan**

Landscape will play an important role in ensuring that the environmental and visual quality of the estate is maintained at a high standard. Landscape treatment will be a major unifying element within the area and is intended to create a distinctive character. The Landscape Masterplan guides treatment of the public domain and external areas to individual development lots.

The landscape treatment proposed will eventually mature to achieve a theme of urban landscape around the developed parts of the site, graduating to a native landscape character at the perimeters of the DCP area and especially along Buttonderry Creek .

All landscape designs and works shall be carried out in accordance with the Landscape Masterplan (Figure 3) and the Visual and Landscape Management Strategy attached in Appendix 1.



## **2.2      Retailing and display in association with goods manufactured or processed on site**

Retailing and showroom display of goods manufactured, processed, assembled, serviced or warehoused on sites within the DCP area may be carried out subject to development consent and where the consent authority is satisfied that such retailing is an ancillary use.

## **2.3      Recreational Facilities**

Commercial recreational facilities (such as indoor cricket, tennis and soccer centres, bowling alleys and the like) and educational or recreational training facilities may be carried out within the DCP area subject to consent. A Development Application for such uses will require the applicant to submit a parking and traffic study to address the greater car parking demands often generated by such uses.

Employer-provided recreational facilities for employees of businesses within the DCP area (such as tennis courts, cycleway/running track network) are encouraged.

## **2.4      Child Care Café General Store and Tenant Services**

The provision of child care centres and convenience services (such as café/food outlets, photocopying centres, ATMs) are encouraged in the DCP area.

These facilities should be located and signposted so that they are predominantly for the use of employees and users of the DCP area and do not require exposure to the surrounding main roads to be viable.

## **2.5      Brothels in the Industrial Zone**

Requirements for brothels are outlined in Wyong Shire Council's *DCP Chapter No 71 Location Criteria for Brothels*.



Source: Trehly Ingold Neate 2007

**Figure 2 - Concept Plan**  
Draft Development Control Plan  
Warner Industrial Park  
June 2008





#### LEGEND

- 5m SETBACK
- 10m SETBACK
- 15m SETBACK
- STORMWATER BASINS
- WETLANDS

Figure 3 - Landscape Masterplan  
Draft Development Control Plan  
Warner Industrial Park  
June 2008

### 3.0 PRIVATE DOMAIN

The private domain comprises all land within private ownership. This DCP is intended to ensure that all private development reflects the expectations of the local community and is appropriate for the locality and the intended use of the DCP area as a high quality industrial estate. Following are the objectives and controls for the Private Domain, however other sections should also be referred to in relation to the Private Domain.

#### 3.1 Subdivision

**Objectives:**

- To create lots that are of a size and shape that is compatible with employment generating uses.

**Controls:**

- Subdivision of land shall satisfy the following criteria:
  - For irregular shaped lots, the minimum width shall apply at the building line setback.
  - All lots shall be adequately drained and shall be reticulated to the public water and sewerage systems.
  - All servicing shall be underground. All service installations shall be suitably screened from public areas by landscaping and be designed so as to not impact on significant vegetation.
  - Lots shall have an area not less than 2,000m<sup>2</sup>.
  - Frontages shall not be less than 30 metres and the width to length ratio is to be no more than 1:3.

#### 3.2 Community Title and Strata Subdivision

**Objectives:**

- To enable multiple individuals and corporate entities to manage site and buildings on a collective basis.

**Criteria:**

- Any application for strata subdivision shall:
  - provide capacity for sustainable use of the site for purposes permitted in the applicable land use zone;
  - include appropriate allocation of car parking by retaining the approved ratio of floor space to car parking spaces;
  - include appropriate allocation of loading/unloading space;
  - not be in the form of a battle-axe lot; and

#### 3.3 Built Form

**Objectives:**

- To encourage well designed attractive buildings appropriate to their function.
- To ensure that built form responds to the industrial landscape in an innovative and considered manner.
- To ensure buildings sit within a landscaped setting and within their street context.
- To encourage buildings which are water and energy efficient.
- To reduce stormwater flows from the site to assist downstream environmental flows.
- To encourage activity in areas that adjoin open space.

**Controls:**

- Building siting design and construction shall incorporate ESD principles and implement best practice approaches to water and energy efficiency including solar access.
- Buildings incorporating on site reuse of stormwater for passive heating and cooling are strongly encouraged. This DCP contains specific requirements in relation to stormwater management (Refer Section 5.0).
- Buildings and structures should be appropriately modulated and articulated to reduce the visual impact of large industrial buildings.
- Public entries to buildings should be clearly expressed and visible from the street.
- Shading devices to reduce solar heat load are encouraged.
- Large expanses of blank walls and curtain walling should be avoided.
- Minor stepping of the street frontage and articulation of building facades is encouraged.
- Areas such as administration offices, staff rooms and the like should be located to face onto open space to provide active areas in these locations.

### **3.4 Floor Space Ratio and Site Coverage**

**Objectives:**

- To regulate the density and size of buildings.

**Controls:**

- Maximum floor space ratio is 0.8:1.
- Maximum site coverage (building only) is 50%.

### **3.5 Height**

**Objectives:**

- To ensure a high quality appearance is achieved, particularly as viewed from the F3 Freeway, Sparks Road and Hue Hue Road.
- To ensure buildings and structures do not penetrate the Obstacle Height Limits for Warnervale Airport.

**Controls:**

- The visual impact of the height, bulk, and scale of a proposed building will be considered to ensure that a high quality appearance is achieved, particularly as viewed from Sparks Road.
- Heights of buildings and all structures are not to exceed the maximum building height standards provided on the Obstacle Limitation Surface map for Warnervale Airport.
- The height of buildings and structures should not be visible above the tree line along the Sydney-Newcastle Freeway (refer to Wyong Shire Council's *Development Control Plan Chapter No 66 – Subdivision.*)

### **3.6 Setbacks**

**Objectives:**

- To ensure adequate separation between buildings and public spaces.
- To ensure buildings are setback to provide visual amenity to the surrounding road network and adjoining land uses.
- To provide, where necessary, setbacks to reduce the risk from bushfire, as per the Bushfire Assessment Report (June 2008).

### Controls:

- The following minimum building setbacks as shown in the Visual and Landscape Management Strategy in Appendix 1 shall apply:

#### General

- 15 metres to the F3 Freeway, Hue Hue Road and Sparks Roads.
- 10 metres to all internal roads.
- Corner lots require special consideration of the secondary street setback where a 10 metre setback is not practical. This will involve demonstrating that a reduced setback will not reduce the ability to achieve an acceptable presentation and functional considerations such as sight lines and parking can be met.
- 5 metres side boundary setback. This may be reduced on one side only with regard to small sites if the building is suitably fire rated as required by the Building Code of Australia. If the building on the adjacent site is built to the boundary a nil setback will be encouraged. For all other cases, setbacks shall be in accordance with the provisions of the Building Code of Australia.
- 5 metres rear setback other than lots adjoining the F3 Freeway, Hue Hue Road, Sparks Roads and Buttonderry Creek Corridor.
- Where the side or rear boundary is likely to be less than 2.5 metres, the building should be set on the boundary unless appropriate provision is made for maintenance of landscaping and general upkeep of the area within the setback.
- Justification is required for proposals to encroach upon setback and landscape areas with carparking and in those instances, sites are to be densely planted to be screened from public places.
- Parking and or storage within setbacks to the F3 Freeway, Hue Hue Road, Sparks Roads and the Buttonderry Creek Corridor is not permitted.
- A 10 metre setback is required on lots adjoining the reserve to the east of the DCP area. This area is to be maintained as an APZ. Refer to the Bushfire Assessment Report (June 2008).
- A 10 metre setback is required on lots adjoining the environmental corridors where it is vegetated. This area is to be maintained as an APZ. Refer to the Bushfire Assessment Report (June 2008).

#### Riparian Buffers

- Quality presentation and active spaces are encouraged.
- Development adjoining the Buttonderry Creek corridor shall incorporate a minimum five (5) metre buffer and in some cases 10 metres.
- Buildings should address the street frontage and also address the environmental corridors.
- No structures, car parking or external storage is permitted within this buffer area.

#### Variation to Setbacks

- Development seeking a variation in setbacks is required to demonstrate the following:
  - The visual impact of the variation is minimal;
  - The development is well integrated to ensure visual impact is properly addressed; and
  - The functioning of the development including traffic manoeuvrability and location of car parking and services areas is suitable.



### **3.7 Minimum Floor Level**

**Objectives:**

- To ensure buildings and sites address potential flood impacts.
- To ensure adequate site drainage.

**Controls:**

- The floor levels of buildings are to be 300mm above the 1% flood level.
- In determining finished levels for internal roads, car parking and external areas, ensure adequate falls to enable the site to drain.

### **3.8 Building Materials, Finishes and Energy Efficiency**

#### **3.8.1 Building materials and energy efficiency**

**Objectives:**

- To create attractive durable buildings.
- To encourage the minimisation of energy consumption as an integral part of building design and construction, with the suggested use of the Green Building Council's Green Star rating system.
- To encourage the minimisation of post-occupancy energy consumption, with the suggested use of a voluntary system such as the Australian Building Greenhouse Rating scheme.
- To have regard to potential risk from bushfire.

**Controls:**

- A preference for high quality, durable, UV stabilised/resistant building materials is favoured. Where lightweight cladding materials are used they are to be appropriately integrated into the design, particularly where visible from a public place.
- Building construction and materials are to comply with the Bushfire Assessment undertaken for the site (June 2008).

#### **3.8.2 Reflective surfaces**

**Objectives:**

- To minimise the effects of glare and reflected heat load.

**Controls:**

- The reflectivity index for glass used externally in construction of a building (as a curtain wall or the like) shall not exceed 20%.

#### **3.8.3 Colours**

**Objectives:**

- To ensure external colour schemes are appropriate to the setting of the DCP area.

**Controls:**

- A colour palette, which reflects the existing natural landscape is encouraged generally with neutral greys, grey greens and blue greys.
- Allow the use of appropriate bold highlight colours where it can be demonstrated they will not detract from the overall aesthetic quality.

### 3.9 Roofscape

#### Objectives:

- To minimise the visual impact of buildings when viewed from surrounding areas including the F3 Freeway and Buttonderry Creek.

#### Controls:

- Simple roof forms are preferred which relate to the structural form of industrial buildings.
- Buildings should not dominate the skyline and should include roof lines and facades which provide visual interest and an appropriate sense of scale.
- Air-conditioning units, roof top pergolas, lift over-runs, telecommunications devices and other roof mounted equipment which protrude above the general roof line of the building shall not be permitted except where they are appropriately integrated into the design of the building so as not to detract from the appearance of the development or dominate the skyline particularly when viewed from any public road and the Buttonderry Creek Corridor.
- Structures such as masts, stacks, distilling towers, silos and the like shall not be permitted, except where:
  - they are appropriately integrated with the design of the building in the manner described above; and
  - they do not exceed twelve (12) metres in height.

### 3.10 Access and Car parking

#### Objectives:

- To ensure carparking requirements are appropriate for the proposed use.

#### Controls:

- Car parking shall be provided in accordance with Wyong Shire Council's *Development Control Plan Chapter No 61- Car parking*.

### 3.11 Signage

Signage should provide structure and legibility to reinforce the Precinct's image and identity in an integrated manner. Signage should not contribute to visual clutter.

#### Objectives:

- To integrate signage motifs and promotional requirements in an integrated, consistent and image conscious format.
- To ensure visibility of signage is considered in the context of each site's landscape design.

#### Controls:

- Signs are to be generally in accordance with Wyong Shire Council's *Development Control Plan No 50 Guidelines for Advertising Signs*, and with Figure 4.
- Placement of all signs should be carefully considered to ensure that they fit the environment and intended identity and character of the site. In this regard, a sign system with a degree of uniformity in design, colour and style is encouraged.
- One directory board will be permitted at each entrance at Sparks Road and Hue Hue Road (refer to Figure 4).

- Advertising signs will not be permitted in the setbacks to Sparks Road, or Hue Hue Road, unless it is presented as a free-standing (totem) style sign which complements and positively contributes to the streetscape.
- A maximum of one (1) free-standing sign is permitted per allotment (existing, consolidated or otherwise). The sign is to be made available to all businesses occupying that allotment for business identification, display or advertising purposes.

### **3.12 Servicing and Storage**

#### **3.12.1 Service Vehicles and Loading Docks**

**Objectives:**

- To ensure adequate arrangements for servicing and vehicles.
- To ensure loading docks are well integrated.

**Controls:**

- Where practical, loading and manoeuvring areas for service vehicles should be separated from car parking and pedestrian access.
- Where shared access is provided, no loading or unloading shall be carried out over car parking spaces and access aisles.
- On-street loading and unloading will not be permitted.
- Service areas shall not be visible from the F3 Freeway, Sparks Road or Hue Hue Road, Buttonderry Creek Corridor and where possible should be screened from internal roads.
- Each development lot should satisfactorily cater for the turning movements of a semi-trailer or demonstrate capacity to accommodate the largest service vehicles likely to use the site. Where B Doubles are required for a particular use, the application should clearly describe how turning movements and parking requirements are to be managed.

#### **3.12.3 External Storage**

**Objectives:**

- To regulate storage in external areas.

**Controls:**

- External storage of material or goods must be identified in the Development Application.
- Any external display area shall:
  - Be screened from view from any public road, the Buttonderry Creek Corridor and Freeway buffer zone;
  - Be attractively designed and arranged; and
  - Integrate with the overall landscape theme for the proposed development.



### 3.13 Landscaping

This section is to be read in conjunction with the Visual and Landscape Management Strategy attached in Appendix 1.

#### Objectives:

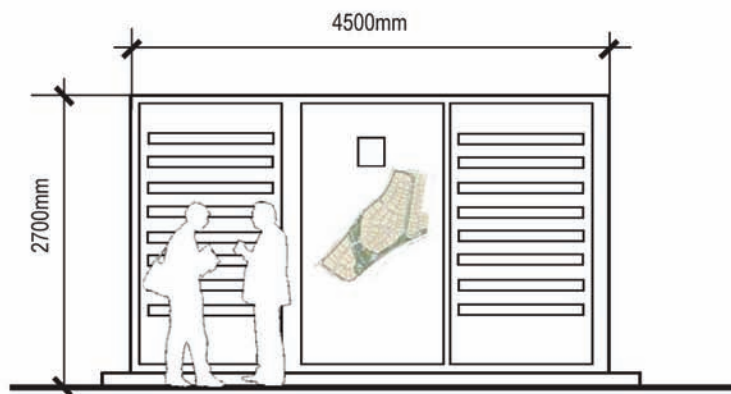
- To ensure landscaping on individual lots are of a high standard.
- To reduce the extent of hardstand areas and provide for on site absorption of stormwater. Landscape designs should aim to minimise the amount of water leaving the site through a combination of plant take, ground water absorption, evaporation and reuse. Noting that the DCP area will incorporate a dual pipe system for reticulation of reclaimed stormwater.
- To ensure landscaping is complementary to the rest of the DCP area.
- To enhance the appearance of the development.
- To provide a human scale and recreation facilities for staff.
- To define and/or screen site and building entries and car parking areas.

#### Controls:

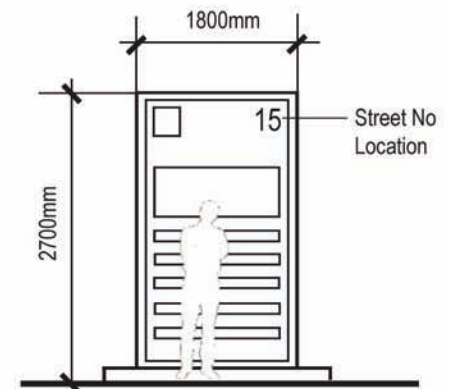
- All development proposals shall include a landscape plan and details of all landscape works associated with the development.
- The scale of landscaping required will be determined by the bulk, scale, visibility and character of the proposed development. Landscaping shall complement:
  - The proposed building;
  - Adjoining development; and
  - Adjoining landscaping.
- Landscape Design Reports if required shall address the following matters:
  - A strategy for dealing with surface flood flow across the site;
  - The integration of sediment pollution control into the landscape design;
  - General planting themes within the site and indicative species;
  - Visual penetration into site and control of views;
  - A thematic strategy for hard surface materials within the site including paving, terracing, ornaments, ponds, street furniture, lighting and the like;
  - Landscape management strategies for the short and long term;
  - Proposed protection measures of adjoining wetland and riparian systems; and
  - Protection and treatment of vegetation proposed to be retained on site.
- Planting to be in accordance with the Visual and Landscape Management Strategy in Appendix 1.
- A Category 3 landscape report under Wyong Shire Council's *Landscape Policy* shall accompany development that adjoins the Buttonderry Creek Corridor, F3 Freeway or F3 buffer zone, Sparks Road or Hue Hue Road.
- Where car parking and loading docks are visible from the street, they shall be screened with species from the Recommended Species list (see Appendix 1).
- Where the placement of electricity poles, easements for access or drainage corridors hinder the provision of screen landscaping within the five (5) metre setback, the setback should be widened to achieve the required level of screening.
- Security fencing is to be provided along the Freeway, Sparks Road, Hue Hue Road and Kiar Ridge Road frontages.
- Security fences are to be 2.4m high and black coated mesh.
- Fences are generally not to be located in front of the building line.
- Fences are not to obstruct stormwater flows.



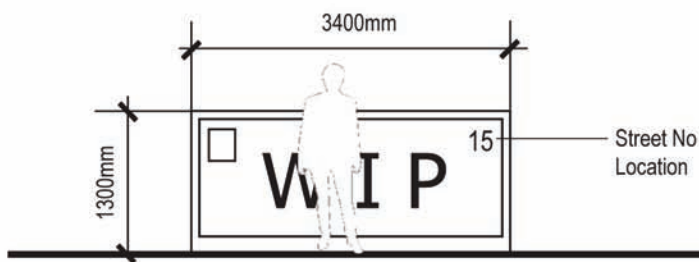
ELEVATION: Warner Industrial Park,  
corporate directory signage



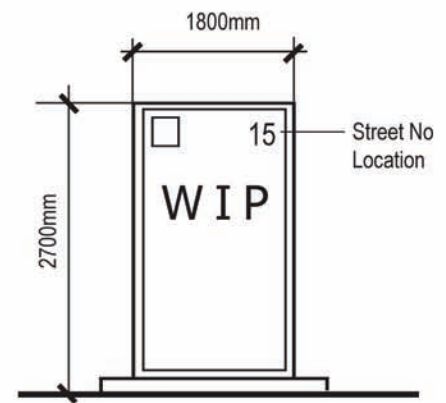
ELEVATION: Multiple tenant signage



ELEVATION: Individual Corporate Signage,  
horizontal format



ELEVATION: Individual Corporate Signage,  
vertical format



### 4.0 PUBLIC DOMAIN

Both public and private development have the potential to contribute to, or detract from, the visual presentation and aesthetic quality of the public domain. Publicly owned lands include local roads, drainage and open space reserves and appropriate landscape treatment and management is important to the presentation of the employment lands, and ongoing functioning of water quality systems. Other sections should also be referred to in relation to the objectives and controls for the Public Domain.

#### 4.1 Traffic and Transport

##### Objectives:

- To ensure the safe and efficient movement of traffic on the road system within the site.
- To ensure that new development is supported by provision of public transport, cycleways and pedestrian pathways.

##### Controls:

- New intersections are to be implemented at the entry to Precinct 14 at Sparks Road and Hue Hue Road as shown in Appendix 1, Visual and Landscape Management Strategy.
- Road and intersection works shall be designed and constructed to the requirements of the consent authority and the RTA.
- Direct vehicular access from lots to and from Sparks Road, Hue Hue Road and the F3 Freeway will not be permitted.
- Internal roads and intersections must be designed to accommodate B-Double movements whilst maintaining lane discipline.
- Areas are to be provide for the unhitching and temporary parking of B-double bogies as shown on the Concept Plan (Figure 1).
- Public bus transport is encouraged. Bus shelters are to be provided along the major route through the DCP area as shown on the Concept Plan (Figure 1).

#### 4.2 Road Widths

##### Objectives:

- To ensure that road widths are compatible with the employment generating uses.

##### Controls:

- Road widths, cycleways and cross sections are to be in accordance with the Concept Plan (Figure 2), and the cross sections shown in Appendix 1, Visual and Landscape Management Strategy.

### 4.3 Landscape and Open Space

The major elements which define the structure and characteristics of the landscape are Buttonderry Creek and the interface with the F3 Freeway corridor. Landscaping standards have been formulated upon the principle of visibility (i.e. vehicles per day on adjoining roads), and/or proximity to Buttonderry Creek.

The principles which comprise this streetscape and landscape character are:

- Development and associated landscape treatment shall appropriately respond to road hierarchy/function and/or visual prominence/sensitivity.
- Major (indigenous) avenue planting of trees.
- Avenue trees to have straight symmetrical boles with a high crown to allow a view of the establishments which occupy the sites fronting these roads.
- Low ground cover vegetation to allow views of buildings, accent entries and enable observation of parking areas within the sites.

#### Objectives:

- To ensure that each development contributes to the overall landscape character of the DCP area.
- To ensure high quality views from the F3 Freeway, Sparks Road and Hue Hue Road and potential visual impacts are minimised.
- To integrate buildings with the open space corridors.

#### Controls:

- The principal roads are to be landscaped in association with the development of the sites that front these roads.
- Landscaping is to be in accordance with the principles as described in this section and the Visual and Landscape Management Strategy (Appendix 1).
- The visual impact of development as seen from the F3 Freeway, Sparks Road and Hue Hue Road should be considered in the design and the proposals should demonstrate how these potential impacts will be mitigated.
- Proposed buildings on sites adjoining land zoned for open space along the Buttonderry Creek Corridor shall have regard to the visual and functional opportunities presented by the location and orientate buildings to face onto the primary street frontage as well as the Buttonderry Creek Corridor.
- Landscape treatment within the Buttonderry Creek Corridor and F3 Freeway Buffer Zone is to be in accordance with the *Draft Wyong Employment Zone Ecological Plan of Management* (10 Feb 2008) and the *DWE Guidelines for Riparian Zones*.
- Where possible the proposal should maximise views to the open space and encourage its appropriate use by employees.

### 5.0 ENVIRONMENTAL MANAGEMENT

#### 5.1 Storm Water Drainage and Water Sensitive Urban Design (WSUD)

An integrated storm water and water quality management system has been developed for Precinct 14.

#### 5.2 Soil and Stormwater Management

##### Objectives

- To ensure stormwater runoff is designed to manage downstream environmental flow regimes.
- To ensure development minimises soil erosion.

##### Controls:

- Each development shall meet the objectives stated in the Wyong Shire Council's *Integrated Water Cycle Management Strategy* (2006) for the WEZ in relation to onsite reuse of surplus stormwater, however allotments do not have to allocate 25% of land for tank water irrigation. Warner Industrial Park allocates over 25% of the site for environmental lands, and it is intended that the required 25% landscaped area on individual allotments be accounted for within these environmental lands.
- Erosion and sediment control shall be in accordance with the requirements of the NSW Environment Protection Authority (EPA) and Wyong Shire Council's *DCP Chapter 67 Engineering Requirements for Development*.
- A Soil and Stormwater Management Plan shall be prepared which addresses soil erosion, sediment control and stormwater management for the proposed development during construction and operation.
- Reducing excess stormwater flows from each site is important in maintaining downstream environmental flow regimes. Innovation in building design, landscape design and potential applications for on site reuse are important considerations. Applications will be required to be supported by a detailed water management plan describing initiatives aimed at reducing excess stormwater leaving the site. This should include initiatives such as rainwater tanks or alternative systems for collection and reuse of storm water noting that Precinct 14 will incorporate on site reuse of stormwater for suitable industrial applications. Reuse for passive heating and cooling as well as toilet flushing, irrigation and wash down are all matters which should be considered.
- The design, construction and implementation of the stormwater drainage systems for each development lot shall:
  - be in accordance with the requirements of Wyong Shire's *Engineering Requirements for Development*;
  - incorporate relevant requirements of the *Flood Prone Development Policy*;
  - incorporate relevant requirements of Wyong Shire Council's *Erosion and Sediment Control from Building Sites Policy*;
  - integrate effectively with other design elements of the DCP area such as streets, utility services, open space networks and landscaping; and
  - make use of best practice environmental engineering techniques.
- Reuse the surplus volume on site in addition to the rainwater tank reuse.
- Each development is to provide storage for 1 kL per 100 m<sup>2</sup> of roof area, for toilet flushing, irrigation and external wash down.

### 5.3 Water Quality and Water Reuse

An on site water reuse reticulation system will be incorporated to provide recycled storm water to each lot for irrigation, toilet flushing, approved industrial applications and wash down purposes, and for irrigation of the public domain areas. The ongoing operational and maintenance costs are to be managed through a "Warner Industrial Park Management Deed", which each lot owner will be required to enter into at the time of purchase.

Surplus storm water flows over and above existing environmental flows are to be collected treated and transferred off site via a separate water management system.

### 5.4 Riparian Zone Management

A comprehensive integrated stormwater and water quality system has been developed for the DCP area and this is to be considered in conjunction with the *Draft Wyong Employment Zone Ecological Plan of Management* (10 Feb 2008) and the *DWE Guidelines for Riparian Zones*.

#### Objectives:

- To ensure that Buttonderry Creek Corridor is managed to address the wetland in a meaningful way and ensure environmental amenity is maintained and/or rehabilitated.

#### Controls:

- Development adjoining environmental corridors is to provide a buffer area on site, providing a transition between the development and adjoining environmental values.
- Appropriate pollution control measures are to be provided.
- A large, integrated development may have one facility for a number of sites where this can be shown to be more effective.
- Treatment and management of Buttonderry Creek and the F3 Freeway Buffer Zone is to be in accordance with the *Draft Wyong Employment Zone Ecological Plan of Management*, (10 Feb 2008) and the *DWE Guidelines for Riparian Zones*.

## 6.0 EXEMPT AND COMPLYING DEVELOPMENT

### 6.1 Introduction

**Exempt development** This applies to very minor developments which once identified, do not require approval from Council, provided that they meet various requirements which are listed in the Schedule.

**Complying development** is identified in this document. It is capable of certification by a private certifier or by Council. This document contains standards that relate to complying development. If a proposal complies with these standards, a complying development certificate must be issued by the consent authority to an accredited private certifier within seven (7) days. If the development does not comply with the standards, it is not classified as complying development.

### 6.2 Aims and Objectives

The aims of this plan are:

#### Exempt Development

- (a) to identify the type of development that is exempt development for the purpose of Clause 13 of the *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 21)*; and
- (b) to identify the requirements for exempt development.

#### Complying Development

- (a) to identify the type of development that is complying development for the purpose of Clause 14 of the *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 21)*;
- (b) to identify the specifications, requirements and outcomes for complying development;

### 6.3 Exempt Development

#### 6.3.1 Exemption Requirements

Clause 13 of the *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 1)* provides specific requirements for exempt development. Unless otherwise noted Exempt Development under this DCP is to be in accordance with Wyong Shire Council's *Development Control Plan Chapter No 85 - Exempt Development*.

### 6.4 Complying Development

#### 6.4.1 Complying Development Requirements

Development is complying development only if it meets the requirements specified in this DCP and Clause 14 of the *State Environmental Planning Policy (Major Projects) 2005 (Amendment No 1)*.

Unless otherwise noted, Complying Development under this DCP is to be in accordance with Wyong Shire Council's *Development Control Plan Chapter No 86 – Complying Development*.

If these and the requirements of this DCP cannot be met then the development may only be carried out with the consent of the consent authority.

Development referred to in column 1 of Schedule 2 is complying development if it meets the requirements and achieves the outcomes referred to in columns 2 and 3 of the table respectively.

It should be noted that the proposal must comply with the preset standards contained in this DCP. It is not possible to use conditions to amend a proposal so that it qualifies as complying development.

#### SCHEDULE – WARNER INDUSTRIAL PARK COMPLYING DEVELOPMENT

USE	REQUIREMENT	OUTCOMES
Industrial Uses Industrial and warehouse buildings, alterations and additions	<ul style="list-style-type: none"> <li>On sites which have road access and are serviced and located in Zone IN1 General Industrial in <i>SEPP (Major Projects) 2005 (Amendment No 1)</i>.</li> <li>Does not involve ancillary retailing, retailing of bulky goods or where food is prepared or manufactured.</li> </ul>	Part A
Different use of building and Internal Fitout or Alterations, e.g. Industry to Light Industry	<ul style="list-style-type: none"> <li>Complies with requirements of <i>SEPP (Major Projects) 2005 (Amendment No 1)</i> and this DCP.</li> <li>Existing approved use.</li> <li>Where not defined as exempt development.</li> <li>Does not apply to any premises on which food is prepared or where there is a change of building use.</li> <li>Will not result in any premises being used for potentially hazardous or offensive industries as defined by <i>SEPP No 33</i>.</li> </ul>	Part B
Fences and retaining walls	<ul style="list-style-type: none"> <li>Where not identified as exempt development.</li> </ul>	Part C
Advertising Structures	<ul style="list-style-type: none"> <li>One per property.</li> <li>Must relate to the use of the land.</li> <li>Applies only to industrial use zones.</li> <li>Where not identified as exempt development.</li> </ul>	In accordance with this DCP

#### PART A – Complying Development - Buildings

- Industrial and warehouse buildings, additions and alterations, on lots with public road access and are serviced with reticulated water, sewer, electricity and telephone and located in Zone IN1 General Industrial in *SEPP (Major Projects) 2005 (Amendment No 1)* and results in the following:

##### General

- The development shall comply with the standards contained within this DCP unless otherwise varied by this Part.

##### Setbacks

- The building complies with the setback requirements in the DCP including the side setback boundaries, other than buildings which encroach within the side boundary setbacks permitted under the DCP (eg zero setbacks) .

### Stormwater Drainage and WSUD

- The stormwater drainage design including onsite WSUD is carried out by a qualified engineer with experience in WSUD, and a statement is provided certifying that the design and works are in accordance with the requirements of this DCP.

## PART B – Complying Development – Land Use

### Different Use of Building Internal Fit Out and Alterations

- Industry to a light industry.
- Where not defined as exempt development and results in the following;

### Food Shops

- If the new use is a food shop, then it must comply with the *Food Act 1989* and Council's *Code*, and a Trade Waste Agreement must be entered into with Council.

**Note:** Food shops do not include restaurants, cafes and the like or any premises which involves the preparation of food for sale or consumption.

### General

- Complies with requirements of the SEPP and DCP applying to the land.
- Does not involve an increase in total floor area of the building.

## PART C – Complying Development - Fences

### Fences and retaining walls

- Where not identified as exempt development and results in the following;

### Bulk and Scale

- Maximum height of a retaining wall combined with a side or rear boundary dividing fence to be 2.1 m above existing ground level.
- Maximum height of a dividing fence 1.8 m.
- Maximum cut or fill to retaining walls must not exceed 1.0m above or below the existing ground level (for the purposes of this, the maximum cut and fill dimensions include any batters incorporated into the retaining wall).

### Structural Integrity

- Fences of a continuous design, such as paling or brick fences are not erected in flood liable areas.
- Fencing in flood liable areas allows for the unimpeded flow of flood waters.

### Stormwater

- Fences do not redirect the overland flow of stormwater.



## **7.0 DEVELOPMENT APPLICATIONS**

The following procedures need to occur in the preparation of a development application within the DCP area.

### **7.1 Pre-Application Consultation**

Consultation with the consent authority prior to submitting a development application is recommended.

### **7.2 Designing Proposals and Preparing Applications**

Applicants are encouraged to use the services of town planners, urban designers, architects, engineers, landscape architects and professional designers to undertake the site analysis, design their development and prepare the supporting documentation.

### **7.3 Application Requirements**

The following documentation is required to be submitted as part of the development application:

- Completed application form with relevant fees.
- Site analysis.
- Statement of Environmental Effects.
- Landscape Design Report (if required).
- Plans, including architectural plans, survey plan, a landscape plan, erosion and sedimentation plan and a stormwater and soil management plan and information on site flooding.
- Other information may be required to support an application.

The completed application form must be either signed by the owner of the land or accompanied by the written authority of the owner to lodge the application, including where appropriate, the company seal or seal of the body corporate.

#### **7.3.1 Site Analysis**

The site analysis should be prepared for consideration during the pre-application discussion and lodged at the same time as the development application. Investigation of the site should identify:

- Site dimensions:
  - length
  - width
- Topography:
  - spot levels &/or contours
  - north point
  - natural drainage
  - any contaminated soils or filled areas
- Services:
  - existing service locations
  - easements/connections for drainage and utility services.

- Existing Vegetation:
  - location
  - species
  - height and width
- Location of:
  - buildings & other structures
  - heritage & archaeological features
  - fences
  - property boundaries
  - pedestrian and vehicle access
  - wetlands
- Neighbouring buildings:
  - location
  - height
  - use (especially potential hazards)
- Walls built to the site's boundary:
  - location
  - height
  - materials
- Difference in levels between the site and adjacent properties at their boundaries.
- Major trees on adjacent properties, particularly those within 9m of the subject site.
- Street frontage features:
  - poles
  - trees
  - kerb crossovers
  - bus stops
  - other services
- The built form and character of adjacent development, including:
  - architectural character
  - front fencing
  - landscaping
- Public open space:
  - location
  - use
- Adjoining bushland or environmentally sensitive land.
- Sources of nuisance:
  - noisy roads or significant noise sources
  - polluting operations

### 7.3.2 Statement of Environmental Effects

The Statement of Environmental Effects should address the following:

- How the proposal responds to issues raised by the site analysis.
- How the proposal responds to the relevant items contained in s.79C of the Environmental Planning and Assessment Act 1979.
- How the proposal responds to the objectives and development requirements of this, and other relevant policies and plans.



### 7.3.3 Plans

The plans submitted to the consent authority are the principle means of describing a proposal. It is important that they are clearly presented and contain enough detail so that well informed decisions can be made. Excessive time and expense can be avoided by clarifying issues and details before a development application is submitted.

Plans prepared by an appropriately qualified professional should show:

- dimensions, levels of all floors and heights of roofs;
- detailed floor plans;
- a site plan, including the north point; and
- all elevations and relevant sections.

Site survey plan prepared by a qualified surveyor should show:

- spot levels and contours on the site and, where necessary, adjoining sites to indicate changes of levels;
- the position of the buildings on the site and adjoining sites;
- the position of significant trees;
- levels of the road and footpath fronting the site;

This can be done in conjunction with the preparation of the site analysis.

A landscape design prepared by a landscape consultant should outline design principles and:

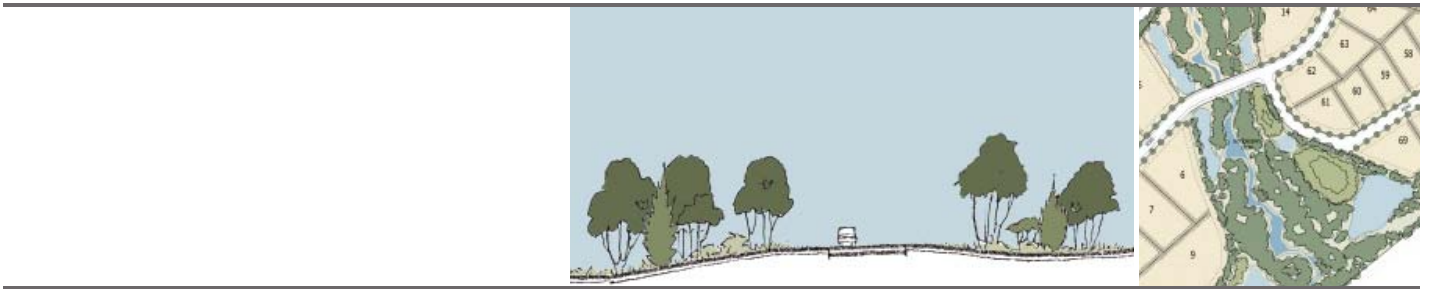
- the location, height and species of all existing vegetation;
- the location, height (at maturity) and species of proposed vegetation;
- cross-sections through the site showing level changes and proposed landscape works;
- areas proposed for the retention or re-use of stormwater
- ground levels such as spot levels or contours; and
- general information on fencing, access points, furniture, pavement materials and on-going maintenance requirements

## REFERENCES

- |  |   |
|--|---|
| Port Macquarie – Hastings Council (2006) | <i>Hastings Development Control Plan No. 36 – Exempt and Complying Development as adopted 26 May 2003 and amended 27 March 2006</i> |
| Wyong Shire Council (2005)               | <i>Development Control Plan 2005</i>  |
| Wyong Shire Council (2008)               | <i>Draft Wyong Employment Zone Ecological Plan of Management February 2008</i>  |
| Department of Water and Energy (2008)    | <i>Guidelines for Riparian Zones February 2008</i>  |

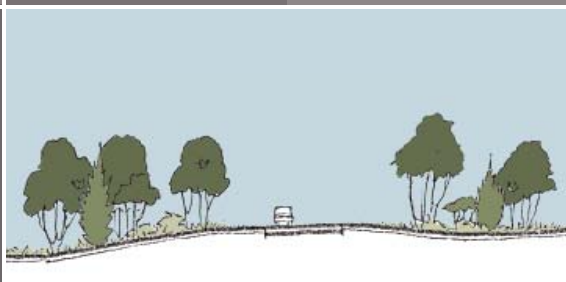
# Appendix 1

## Visual and Landscape Management Strategy



## Warner Industrial Park Draft Development Control Plan

# Visual and Landscape Management Strategy



# Warner Industrial Park Draft Development Control Plan



TERRACE  
TOWER  
GROUP

Warner Business Park Pty Ltd  
Part of the Terrace Tower Group



## **CONTENTS**

### **Executive Summary**

### **Site Description**

### **Visual Assessment**

- Introduction and Context
- Visual catchment and views from surrounding lands
- Views from the surrounding road network and within the site
  - Views from the F3 Freeway
  - Views from Sparks Road
  - Views from Hue Hue Road and Kiar Ridge Road
  - Views within the Site

### **Visual Management and Landscape Strategy**

- Visual Management Principles
- Landscape Design Guidelines
- Landscape Maintenance Guidelines
- Landscape Masterplan
- Planting schedule and recommended species
- Typical landscape treatments



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## EXECUTIVE SUMMARY

Warner Industrial Park is located adjacent to the F3 Freeway and forms part of the Warnervale growth corridor, which includes the Wyong Employment Zone and Warnervale Town Centre.

The site is visible from the F3 Freeway and the surrounding road network. This report assesses the visual impacts and describes measures to mitigate those impacts. These measures are explained in the Visual Management and Landscape Strategy section. Mitigating the visual impacts of development involves the retention and enhancement of existing vegetation, particularly Buttonderry Creek and the proposed buffer along the freeway boundary. These elements also form part of the Water Sensitive Urban Design and general landscape management for the site. Additional measures include guidelines to control building placement and design, including setbacks from the Freeway and surrounding road network.

This Visual and Landscape Management Strategy is an integral part of the Draft Warner Industrial Park Development Control Plan (DCP). The DCP provides the mechanism to manage development and ensure the provision of a high quality built and natural environment, within the context of creating new employment opportunities for the Central Coast.

## SITE DESCRIPTION

Warner Industrial Park comprises Precinct 14 of the Wyong Employment Zone. The site is bounded by the F3 Freeway, Sparks Road, Hue Hue Road and Kia Ridge Road. Land to the north east is owned by Wyong Shire Council and is to be retained as open space (Figure L1).

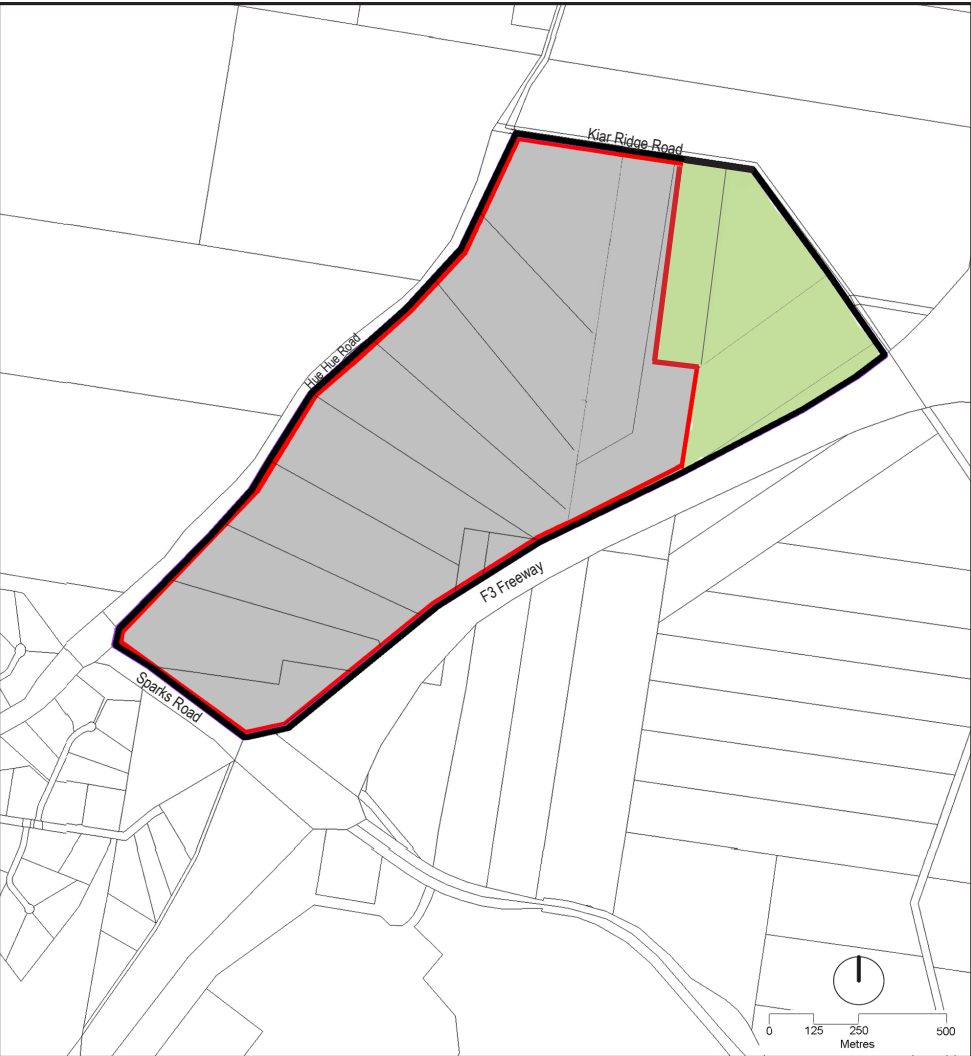





Figure L1 - Site Plan

-  Council-owned proposed conservation land
-  Warner Industrial Park
-  Precinct 14

## **VISUAL ASSESSMENT**

### **Introduction and Context**

The site is located on a major intersection of the F3 Freeway and Sparks Road, and is known as Precinct 14 within the Wyong Employment Zone. Development of the Wyong Employment Zone will change the existing rural character of the northern part of the Central Coast.

### **Methodology**

This visual assessment is based on onsite inspections carried out by paa design, supported by oblique and vertical photography and cross section analysis at various locations around the site perimeter. The cross sections developed by paa design for the visual analysis and proposed landscape treatment, are based on survey and cross sections provided by Trehy Ingold Neate.

An assessment of surrounding land uses, visual and landscape character was developed from field investigations, and a general review of:

- existing land uses and land ownership based on Council's LEP and documentation supporting the proposed Wyong Employment Zone;
- proposed land uses surrounding the site based on the draft Central Coast Regional Strategy and the WEZ documentation;
- terrain and landform based on 1:25,000 contour maps; and
- aerial photography.

This data was used to determine the general visual catchment of the Warner Industrial Park and identify potential visual impacts arising from the development. In particular, where the site is visible from major public spaces and thoroughfares or where there are likely to be significant impacts on sensitive uses close to the site, such as existing residences.

A series of measures were then developed to mitigate those impacts through:

- retention of existing vegetation;
- provision of buffer zones;
- use of landscaped setbacks within proposed allotments; and
- controls on the siting and design of buildings.

## Visual catchment and views from the surrounding lands

The site is located in the middle reaches of the Buttonderry Creek catchment. The visual catchment is generally defined by Sparks Road, the F3 Freeway, elevated land to the north and north west, and rural land to the west of Hue Hue Road. The steeper terrain to the north and north west form the northern extent of the Buttonderry Creek catchment, and are part of a significant landscape element defining the northern boundary of the Warnervale / Kiar Ridge precinct. Buttonderry Creek is an important landscape element in its own right.

There is little surrounding development overlooking the site. Wyong Council's Buttonderry Waste Management Facility occupies a significant area to the north west. Wyong Council also owns an area of heavily vegetated land to the north of the site which is to be retained as part of the Biodiversity Strategy for the WEZ.

Land to the west is zoned non-urban constrained lands, open space and special uses, and areas to the east of the freeway are proposed to be incorporated into the environmental corridors identified in the Biodiversity Strategy. Land to the south comprises the freeway offramp, a limited number of rural hobby farms and conservation zoned land. The dwellings are set well back from Sparks Road (refer to Figures L2 and L3).

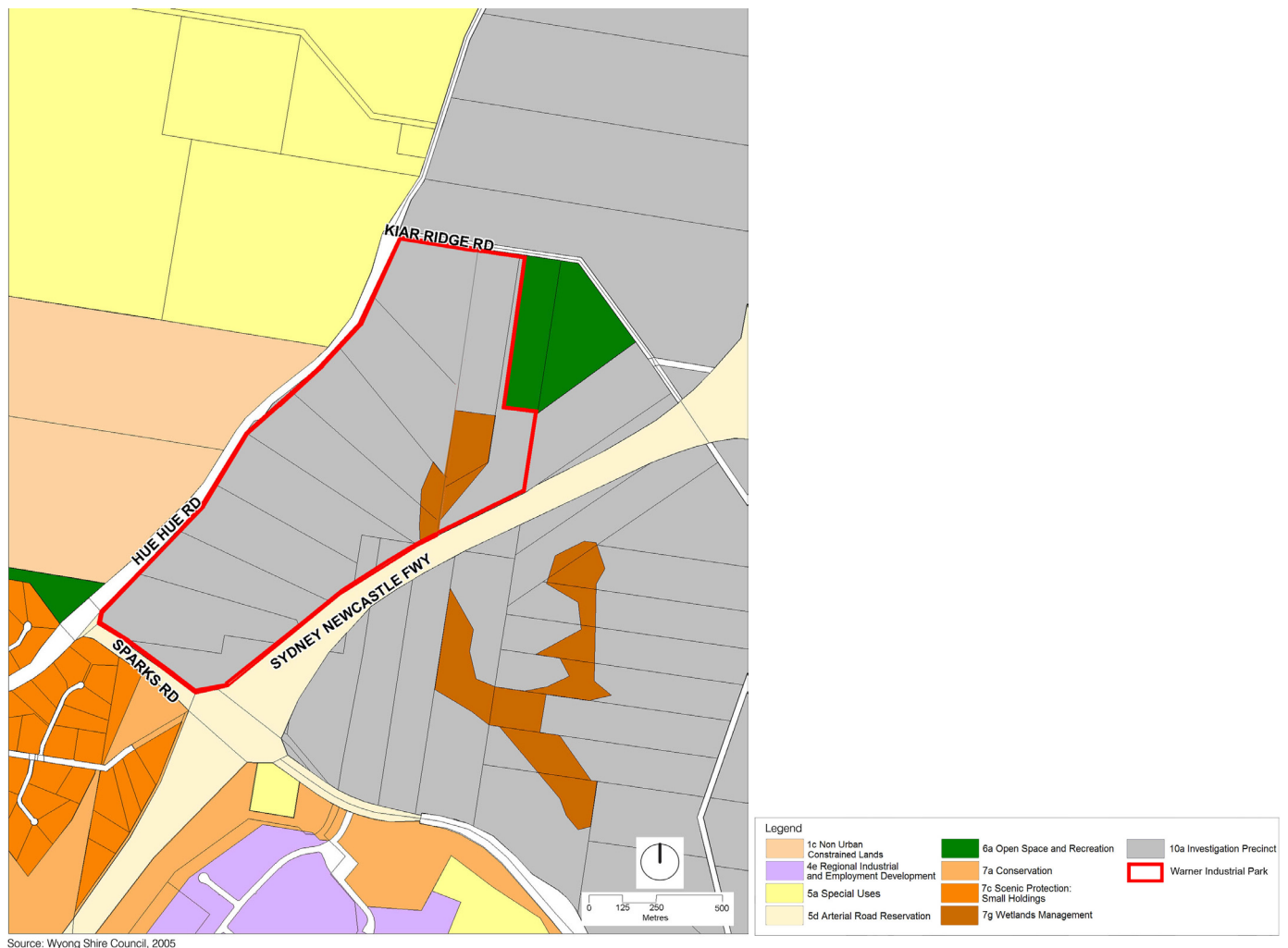


Figure L2 - Existing Zoning Plan



Figure L3 - Aerial photograph of the Site

### Views from the surrounding road network and within the site

The Sparks Road F3 Interchange is an important entry into Warnervale, and the northern part of Wyong Shire. Consideration is to be given to mitigating the exposure of the site to both of these roads and in particular, the north bound lane of the F3 Freeway.

Views into the site are partly mitigated by existing vegetation around the perimeters (refer to Figures L4 to L7 for existing views to and within the site). Along the F3 corridor, this vegetation is to be retained and enhanced in accordance with the Landscape Masterplan included in the latter part of this document. This corridor, together with retention of the Buttonderry Creek corridor, provides significant green links between the rural lands to the west and north in line with the site's Conservation and Water Sensitive Urban Design Strategy.

As can be seen in the Typical Landscape Treatments (Figures L8 - L23), overlooking into and out of the site varies. Travelling north from the Sparks Road off-ramp, the site is below the road level of the F3 Freeway, around the location of Buttonderry Creek. Further north, the site is above the pavement level of the F3. The site is generally below the level of Sparks Road, Hue Hue and Kiar Ridge Roads.

Wyong Council's exhibited landscape strategy for the WEZ highlights the importance of retaining existing vegetation along Sparks Road and the F3 corridor wherever possible.

Landscape treatment of the site's entries at Sparks Road and Hue Hue Road, the internal street network, and general open space along Buttonderry Creek and the F3 boundary is to be carried out as part of the initial site development. Landscaping within setbacks is to be provided by individual developers, in accordance with the site's Development Control Plan and the Landscape Masterplan.



Views from F3 Freeway

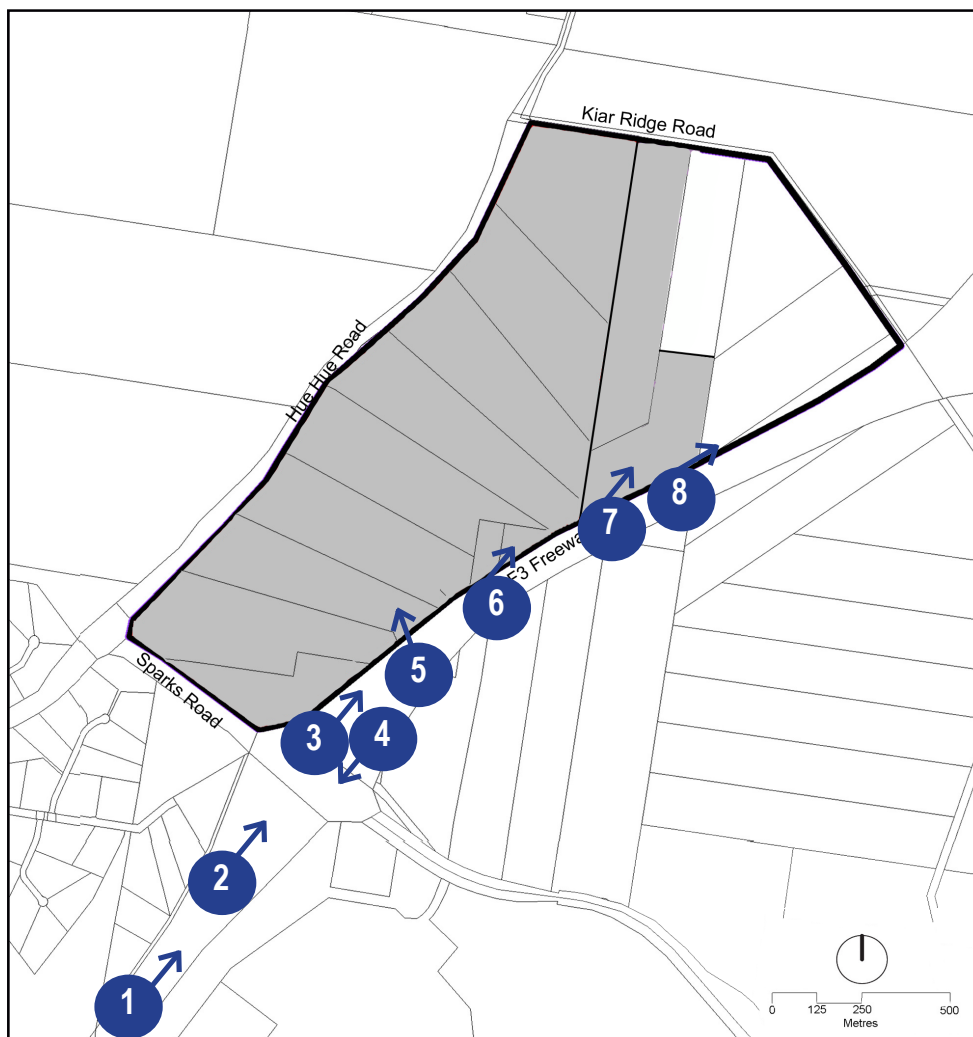


Figure L4 - Views of site from the F3 Freeway

Views from F3 Freeway

1



View of F3 northbound, with Sparks Road off-ramp at left

5



View from F3 of existing vegetation to southern boundary, near Buttonderry Creek

2



View of Sparks Road off-ramp, F3 at right

6



View of existing vegetation to southern boundary, adjacent to F3

3



View of F3 northbound, southern corner of site at left

7



View of existing vegetation to southern boundary, north of Buttonderry Creek

4



View of F3 southbound at right, with Sparks Road off-ramp at left

8



View of existing vegetation to the north



Views from Sparks Road

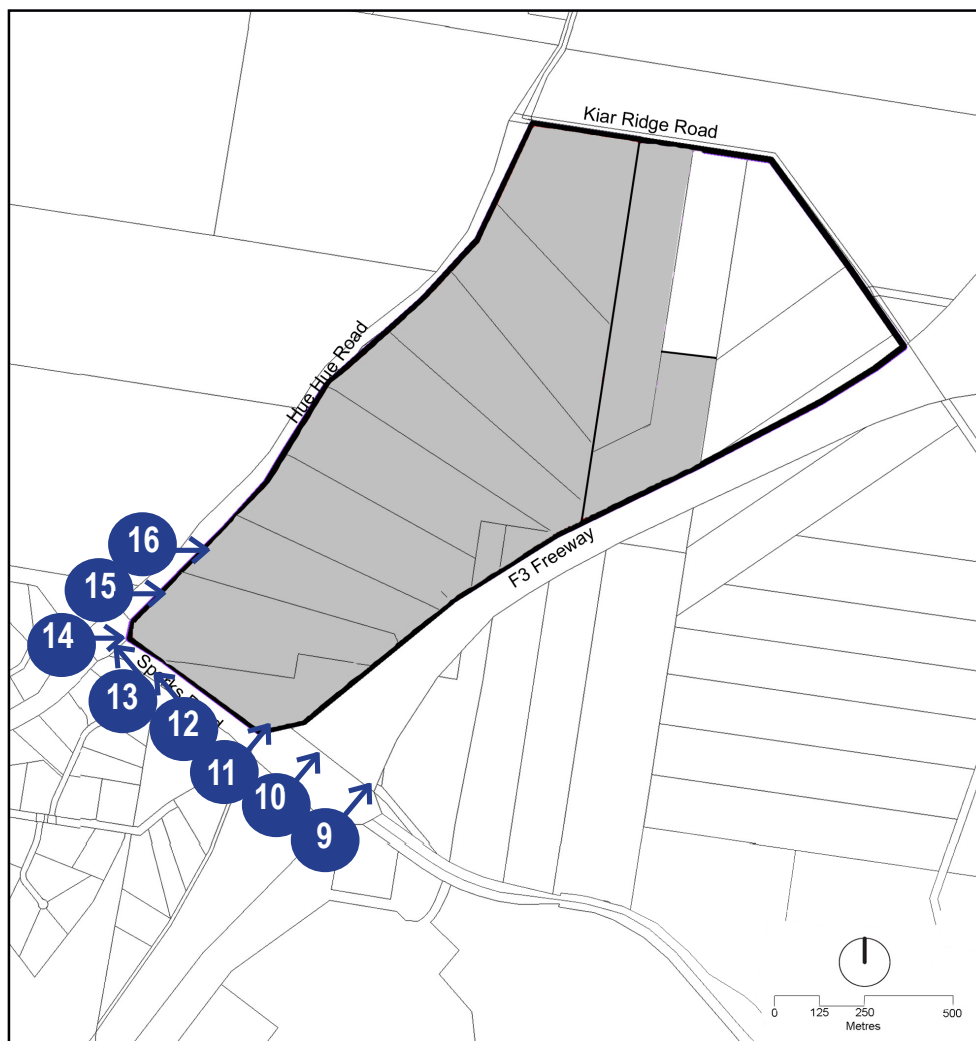


Figure L5 - Views of site from Sparks Road

Views from Sparks Road

9



View of the site from Sparks Road crossing of F3

13



View along Sparks Road, to intersection with Hue Hue Road

10



View of Sparks Road and F3 northbound on-ramp intersection

14



View east looking across site, at corner of Sparks Road and Hue Hue Road

11



View from Sparks Road, to southern most corner of the site

15



View east looking across site, north of intersection of Sparks and Hue Hue Roads

12



View north west down Sparks Road, with site at right

16



View east looking across site, north of intersection of Sparks and Hue Hue Roads

Views from Hue Hue Road and Kiar Ridge Road

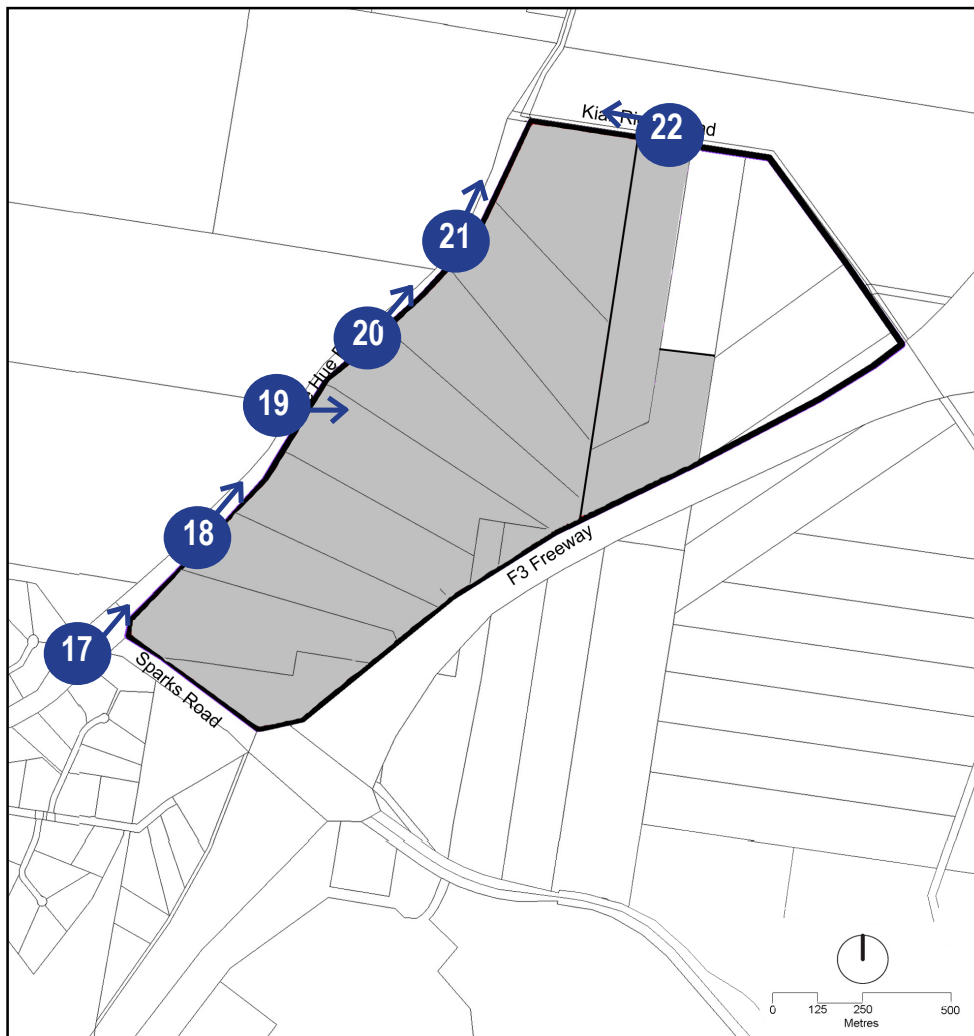


Figure L6 - Views of site from Hue Hue Road and Kiar Ridge Road



Views from Hue Hue Road and Kiar Ridge Road

17



View looking north along Hue Hue Road

21



View looking north along Hue Hue Road, with the site to the right

18



View looking north along Hue Hue Road

22



View from Kiar Ridge Road looking towards intersection with Hue Hue Road and Buttonderry Waste disposal site

19



Looking east into the site, from Hue Hue Road

20



View looking north along Hue Hue Road, with the site to the right

Views from within the site

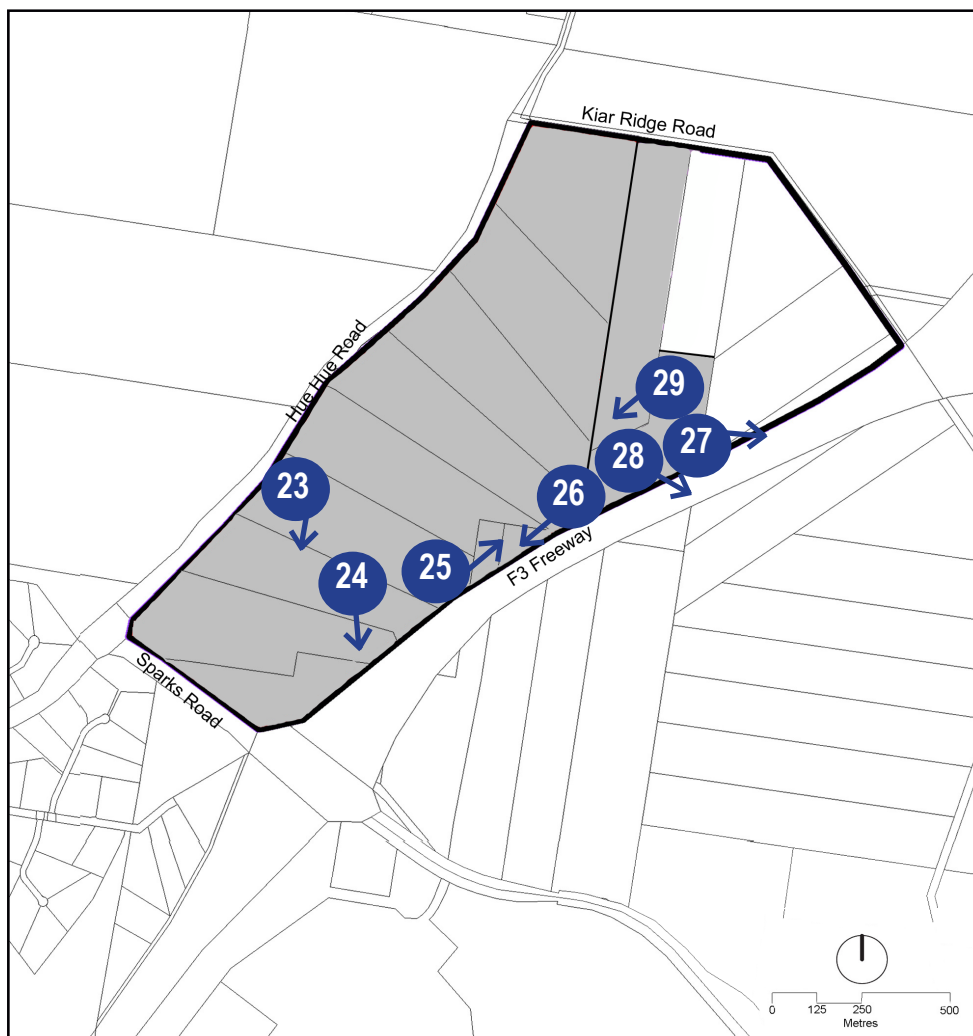


Figure L7 - Views from within site



Views from within the site

23



Views along Buttonderry Creek

27



View towards freeway from northeast corner of site

24



Views along Buttonderry Creek at F3 boundary

28



View from north east corner of site, adjacent to freeway reservation near existing spoil

25



View of existing services easement, looking north, parallel to F3 boundary

29



View in north east corner of site, adjacent to freeway showing spoil from freeway construction

26



View of existing services easement, looking south, parallel to F3 boundary

## VISUAL MANAGEMENT AND LANDSCAPE STRATEGY

### Visual Management Principles

The primary objective of the Visual Management and Landscape Strategy is to mitigate the visual impacts of development when viewed from surrounding major roads, public areas or other sensitive land uses, and ensure that major landscape elements within the site are retained and managed as part of the regional landscape.

Mitigating the impact of development with respect to regional landscape considerations will be achieved by incorporating major open space elements into the estate masterplan, including:

- **Buttonderry Creek** - retaining the Buttonderry Creek corridor with appropriate setbacks and supplementary planting of native species in accordance with the Landscape Masterplan; and
- **Freeway buffer** - creating a buffer zone along the F3 Freeway boundary to ensure adequate separation and screening of future buildings. The buffer zone will provide a landscape link between Buttonderry Creek and the proposed conservation areas to the north of the site.

Mitigating the impact of development when viewed from major roads around the perimeter of the site, and from roads and public spaces within the site, will be achieved by incorporating appropriate controls from the Draft Warner industrial Park Development Control Plan, including:

- **Setbacks** - that provide visual amenity to the surrounding road network and adjoining land uses, as well as to ensure adequate separation between built form and public space. Setbacks are generally to be 15 metres to the F3 Freeway, Hue Hue Road and Sparks Road, with 10 metres to all internal roads, and a 5 metre side boundary setback. The Buttonderry Creek corridor requires a minimum 5 metre, and up to, 10 metre buffer. Landscape within the setbacks is to be in accordance with the Typical Landscape Treatments and the Recommended Species list, with special consideration to be given to the retention of existing vegetation;
- **Landscape** - that ensures a high quality landscape within the site, minimises built form impacts on the local environment, and provides screening for the site's entries and buildings;
- **General Building Controls** - that ensure the provision of well-designed built form, that is appropriate to the landscaped setting of Warner Industrial Park.

## **Landscape Design Guidelines**

### **Introduction**

Landscape works are intended to play an important role in ensuring that the environmental and visual quality of the Warner Industrial Park develops and is maintained at a high standard. Landscaping will be a major unifying element within the area and give it a distinctive character. Details of the overall landscape themes and intentions for public areas are described below. Figures L8 to L23 provide guidelines for the landscape treatments for Warner Industrial Park.

Warner Industrial Park is based on proper integration of landscape and building design. Removal of existing trees will be offset by new plantings within road reserves, drainage swales and setback areas on development lots.

The Landscape Masterplan identifies potential corridors along side and rear boundary setbacks. It is proposed that these plantings on private lands will integrate with plantings in the public domain to visually link Warner Industrial Park to the surrounding natural landscape along Buttonderry Creek.

### **Streetscape and street tree planting**

The objective is to create a distinctive character for the site which complements the existing natural landscape of the area. A number of distinct precincts will be created with an emphasis on areas such as the roundabout and the main entrance. Street trees will be indigenous to the site and the use of these trees will clearly define the street edge as well as creating a strong visual character within the site.

Vegetation indigenous to the site shall be used to screen views in and out of the development while enhancing the visual appearance and stabilising the embankment in the drainage channel.

The creek bank shall be planted with indigenous trees, shrubs and groundcovers. Weed removal and bush regeneration shall be carried out using accepted procedures for bushland management including working from the least weed infested areas to the most heavily infested, minimal soil disturbance and allowing native plant regeneration to dictate the rate of weed removal.

### **Development Lots**

The following areas of the site shall be landscaped by developers of individual sites:

- front setback excluding driveways;
- side and rear setbacks if visible from a public place;
- car parking and areas adjoining the building to maintain a high quality appearance; and
- screening of service areas, loading and storage areas.

### **Entry Areas**

One free-standing corporate sign designed generally in accordance with the Warner Industrial Park DCP. Signs may be located within the front setback area. The preferred positions are adjacent to the vehicular and / or pedestrian access from the street to the site. The sign should be located 2 metres from the boundary and should be positioned to maintain sight lines for vehicles and pedestrians entering or leaving the site. Visibility of the sign should be taken into account in the landscape design.



## **Setbacks**

The Landscape Masterplan identifies landscape corridors to be created along side and rear setbacks of certain development lots. This planting and planting within setback areas generally should integrate with the streetscape planting and landscape works on adjoining properties.

Planting in front setbacks should relate to the general theme established for the Warner Industrial Park. Areas such as the main entrance to a building and access for pedestrians and vehicles should be emphasised as part of the landscape design.

Canopy trees in landscape corridors should be restricted to local indigenous species. Within general setback areas, use of indigenous species is also preferred due to their general adaptation to conditions, lower water demand, and potential value as habitat. Use of exotic species should be limited to use as features (e.g.: entry point) and lower level planting where there is no equivalent native species and only in circumstances where the introduced species is unlikely to create or exacerbate a weed problem in the Warner Industrial Park or in surrounding bushland.

Local indigenous species are preferred for grasses, other groundcovers and trees.

Surface drainage from car parking areas should be diverted from the open space and where appropriate collected in wetland filters to be constructed at the discharge point.

## **Car parking Areas**

The following landscape objectives should be addressed in the preparation of car parking designs:

- screening from the F3 Freeway, Sparks Road, Hue Hue Road and internal collector roads within the industrial park;
- creating an appropriate scale of landscape works using shrub and tree planting;
- creating an attractive and functional environment;
- erosion and Sediment Control from Building Sites; and
- achieving WSUD objectives.

## **Landscape Maintenance Guidelines**

### **Introduction**

Landowners are responsible for maintaining all landscape areas within private property. Upon completion of construction works, a site shall be properly cleaned up with all rubbish removed in a manner consistent with best practice waste management principles and any approved waste management plan applying to the development. Grassed areas shall be mowed and landscape beds mulched. Any damage to footpaths and the nature strip shall be repaired including replacement of street trees and restoration of grassed areas as required.

Maintenance of buildings, car parking areas signs and services should be carried out without detrimental effect on landscaping works. Where damage does occur it should be rectified immediately upon completion of the maintenance works.

### Landscape Maintenance

Landscape works shall be maintained to achieve the intention of the landscaping to enhance the appearance of a development, provide a human scale and recreation facilities for staff, and to define and / or screen site and building entries and car parking areas.

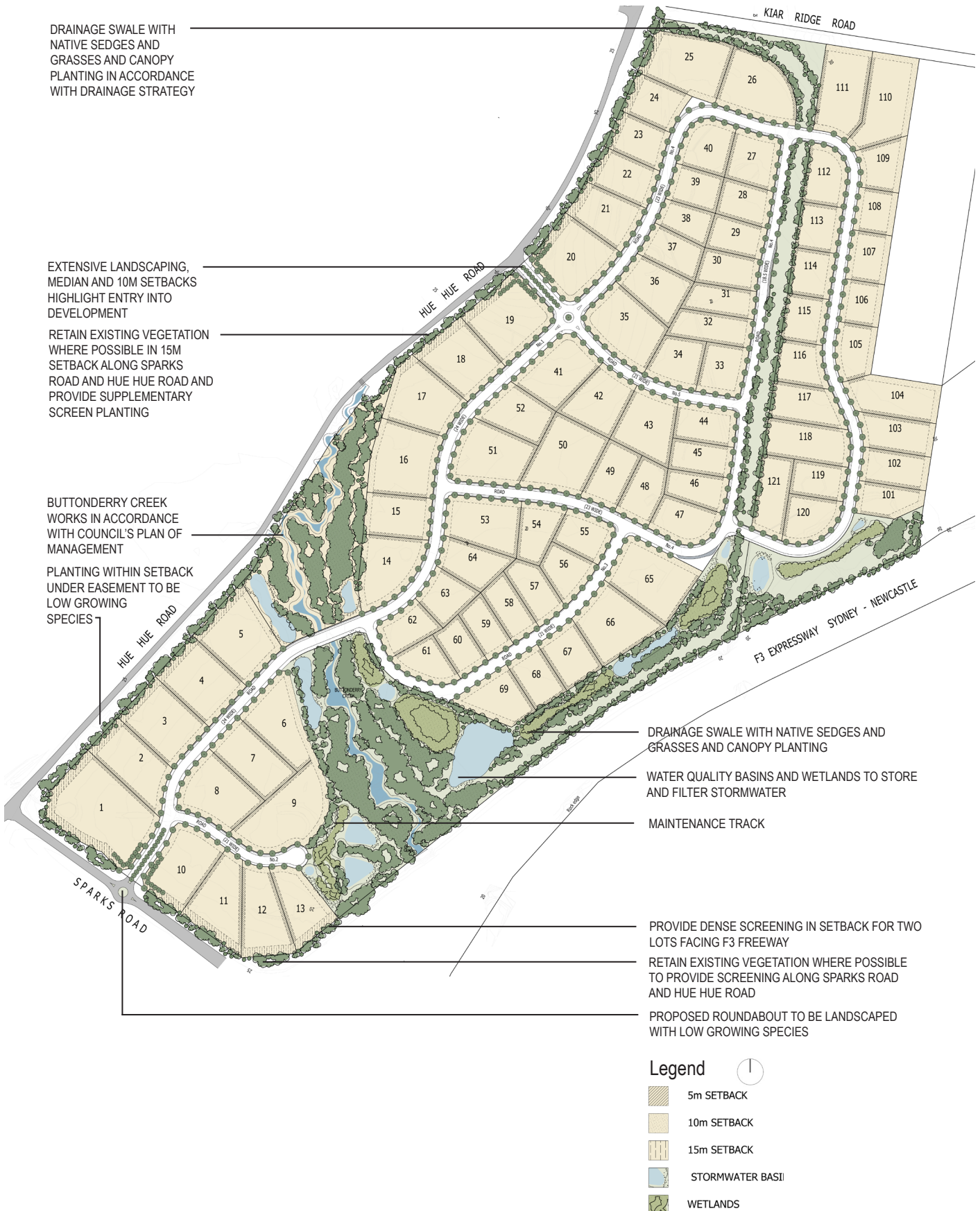
The management of Buttonderry Creek is to be in accordance with the Draft *Wyong Employment Zone Ecological Plan of Management* (February 2008).

The following guidelines are not exhaustive and are intended as a checklist to ensure that landscape works are retained as envisaged in the approved plans.

- **Mowing:** The frequency of mowing should be in response to the climate and seasons throughout the year. A blade height of between 20mm - 75mm is recommended.
- **Edges:** Motorised edgers (e.g.: Whipper snipper) should be used with care and not closer than 100mm to the trunk of any tree as contact may accidentally ring bark the tree. Edges of grassed areas should be trimmed on each occasion that the grass is cut. These include the edges of garden beds, and the junctions of lawn with pavements, kerbs poles, pits and splayed edges around trees on turf.
- **Fertilising:** Fertiliser should be applied with care to avoid the run-off of excess nutrients to the drainage system.
- **Pest and Disease:** Plants should be regularly checked for pests and disease. Where it is identified that a particular pest or disease is likely to cause damage to planting, remedial action should be taken. Natural remedies are preferred but where spraying is required, the pesticide should be specific for the pest rather than broad range and should be of low toxicity and non-residual.
- **Weed Control:** Weeds may lead to the long term degradation of landscape works. There are a number of existing weed sources on the Warner Industrial Park site in the locality so it is expected that weed infestation will be a recurring problem. The most effective means of weed control are effective site restoration and planting (especially mulching and groundcovers) and judicious use of herbicides. A schedule of undesirable plant species is contained in Wyong Council *Development Control Plan No 14 - Vegetation Management* and includes plants such as Privet, Camphor Laurel, Cocos Palms, Cassia, Moth Vine, Asparagus Fern, Honeysuckle, Lantana and Ochna.

## Landscape Masterplan

Figure L8



## Planting Schedules and Recommended Species

### 15M LANDSCAPE SETBACK,

### 10M LANDSCAPE SETBACK,

### 5M LANDSCAPE SETBACK

#### TREES

<i>Callistemon salignus</i>	3m centres
<i>Casuarina glauca</i>	" "
<i>Eucalytus robusta</i>	" "
<i>Eucalytus saligna</i>	" "
<i>Melaleuca quinquenervia</i>	" "

#### SHRUBS

<i>Acacia longifolia</i>	2m centres
<i>Callistemon citrinus</i>	" "
<i>Doryanthes excelsa</i>	" "
<i>Leptospermum polygalifolium</i>	" "
<i>Melaleuca ericifolia</i>	" "
<i>Melaleuca nodosa</i>	" "
<i>Syzygium australe</i>	" "
<i>Westringia fruticosa</i>	" "

#### GROUNDCOVERS

<i>Dianella caerulea</i>	6/m2
<i>Grevillea 'Bronze Rambler'</i>	3/m2
<i>Hardenbergia violacea</i>	3/m2
<i>Lomandra longifolia</i>	6/m2
<i>Pennisetum alopecuroides</i>	6/m2
<i>Poa labillardieri 'Eskdale'</i>	6/m2

### SUPPLEMENTARY NATIVE PLANTING

#### SHRUBS

<i>Acacia ulicifolia</i>	2m centres
<i>Banksia spinulosa</i>	" "
<i>Dodonaea triquetra</i>	" "
<i>Epacris pulchella</i>	" "
<i>Leptospermum polygalifolium</i>	" "
<i>Melaleuca nodosa</i>	" "
<i>Pimelea linifolia</i>	" "



### SUPPLEMENTARY NATIVE RIPARIAN PLANTING

#### TREES

<i>Angophora floribunda</i>	3m centres
<i>Eucalytus pilularis</i>	" "
<i>Eucalytus saligna</i>	" "
<i>Glochidion ferdinandi</i>	" "

#### SHRUBS

<i>Daviesia ulicifolia</i>	2m centres
<i>Dodonaea triquetra</i>	" "
<i>Melaleuca nodosa</i>	" "
<i>Polyscias sambucifolia</i>	" "

#### GROUNDCOVERS

<i>Entolasia stricta</i>	4/m2
<i>Imperata cylindrica</i>	4/m2
<i>Lomandra longifolia</i>	4/m2



### WETLAND PLANTING

#### TREES

<i>Casuarina glauca</i>	3m centres
<i>Eucalytus robusta</i>	" "
<i>Melaleuca quinquenervia</i>	" "

#### MACROPHYTES

<i>Baumea articulata</i>	4/m2
<i>Eleocharis acuta</i>	" "
<i>Gahnia sieberana</i>	" "
<i>Isolepsis nodosa</i>	" "
<i>Juncus usitatus</i>	" "
<i>Schoenoplectus validus</i>	" "



### STREET TREES

<i>Corymbia maculata</i>	20m centres
<i>Eucalytus robusta</i>	" "
<i>Eucalytus saligna</i>	" "
<i>Syncarpia glomulifera</i>	" "
<i>Toona ciliata</i>	" "
<i>Tristaniopsis laurina</i>	" "
<i>Watehousia floribunda</i>	" "



### EXISTING VEGETATION TO BE RETAINED

Existing vegetation typically includes:

*Angophora costata*  
*Allocasuarina torulosa*  
*Corymbia maculata*  
*Eucalytus pilularis*  
*Eucalytus robusta*  
*Syncarpia glomulifera*



## Typical Landscape Treatments

Figure L9 - SITE KEY PLAN: for Landscape Sections



Figure L10 - SECTION 1: Entry off Sparks Road and Hue Hue Road to Precinct 14 (variable as typical)

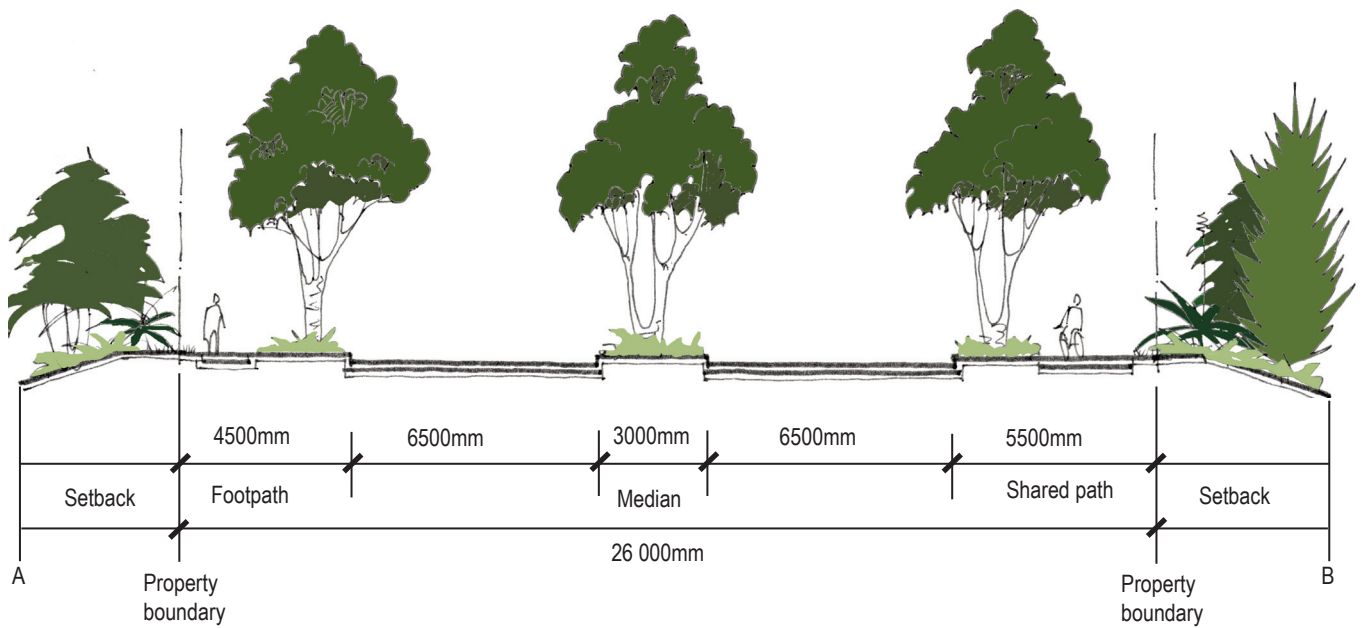


Figure L11 - CONCEPT PLAN: Entry treatment at intersection of Sparks Road and entry to Precinct 14, New entry from Hue Hue Road - similar treatment but seagull intersection design

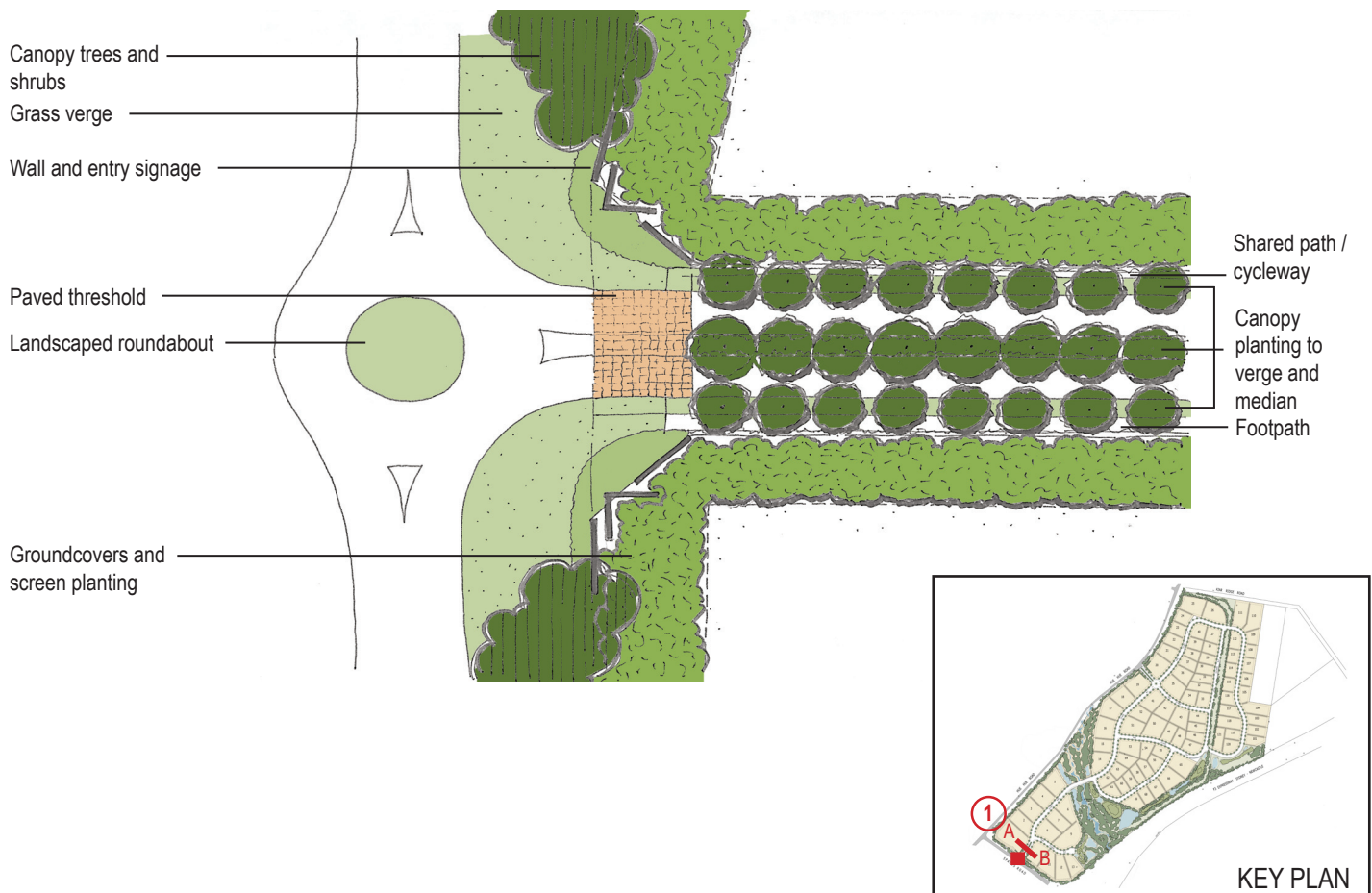


Figure L12 - SECTION 2: Typical Section Road No 1

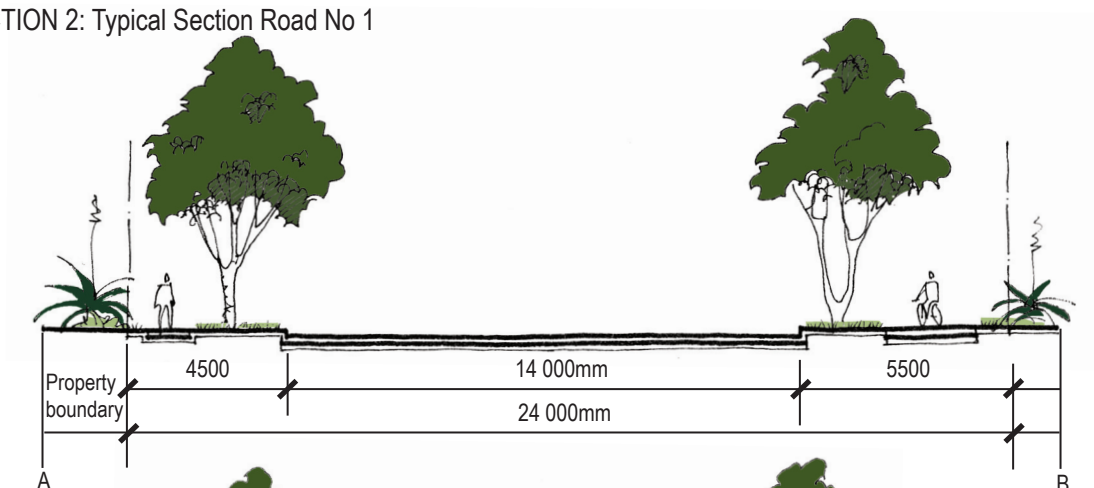


Figure L13 - SECTION 3: Road 3

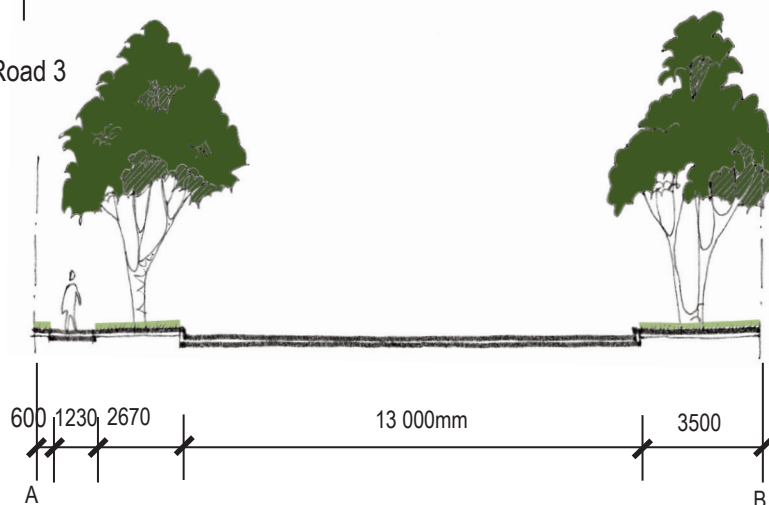


Figure L14 - SECTION 4: Road 4 and Drainage swale

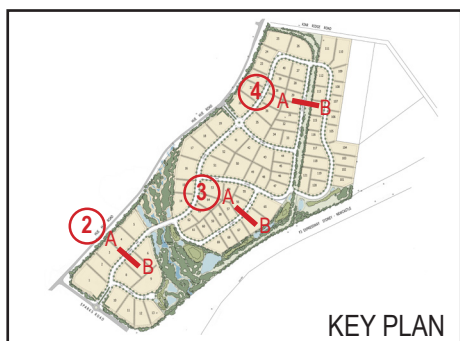
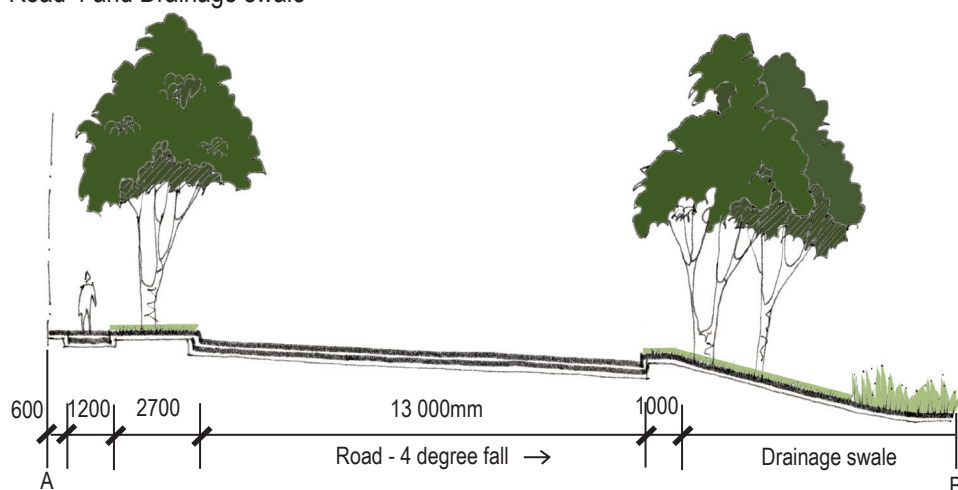


Figure L15 - SECTION 5

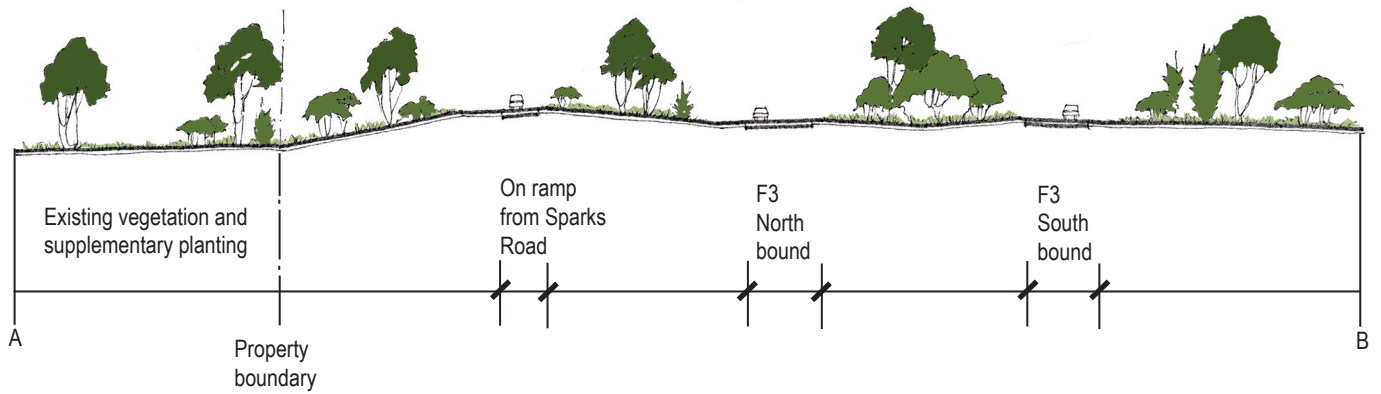


Figure L16 - SECTION 6

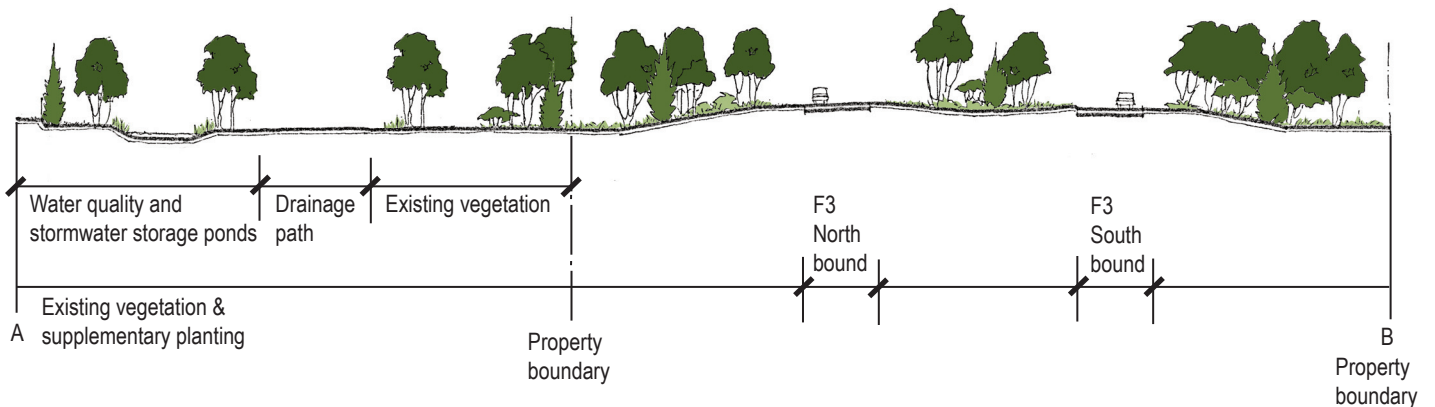


Figure L17 - SECTION 7

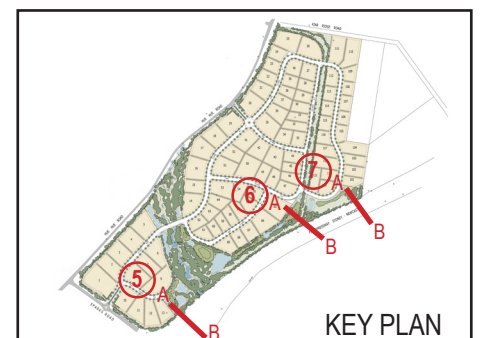
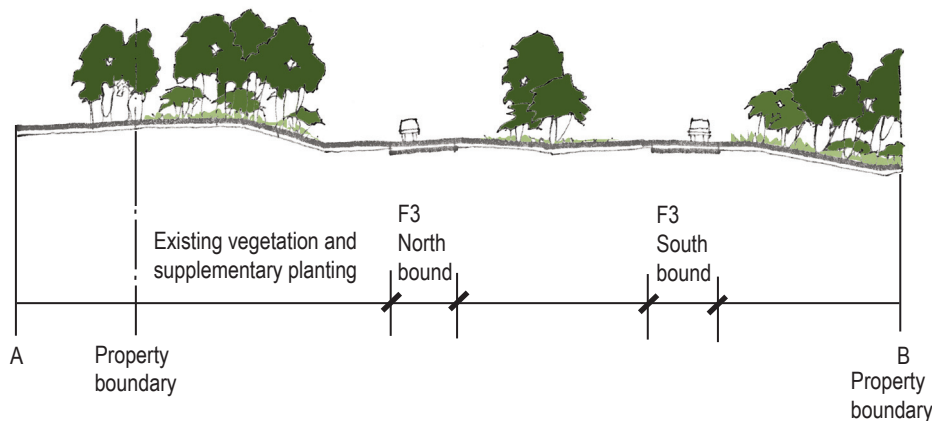




Figure L18 - SECTION 8: Landscape setback along Sparks Road and southern portion of Hue Hue Road

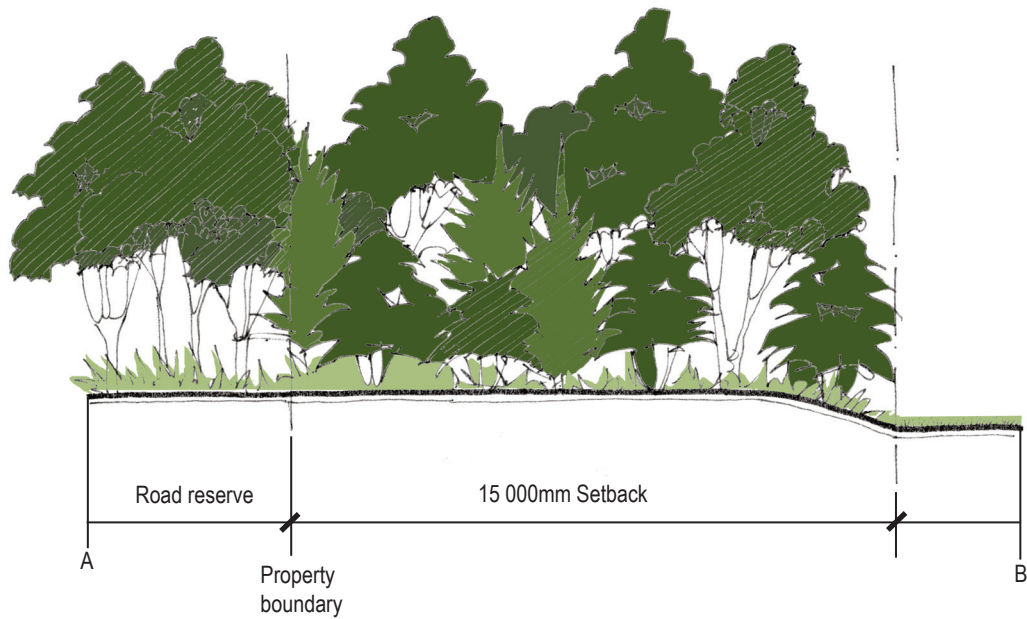


Figure L19 - SECTION 9: Landscape setback along northern portion of Hue Hue Road

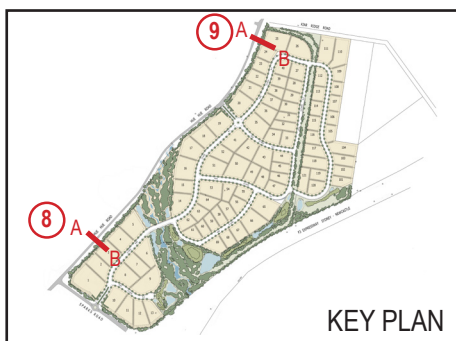


Figure L20 - SECTION 10 - Northern portion of Hue Hue Road

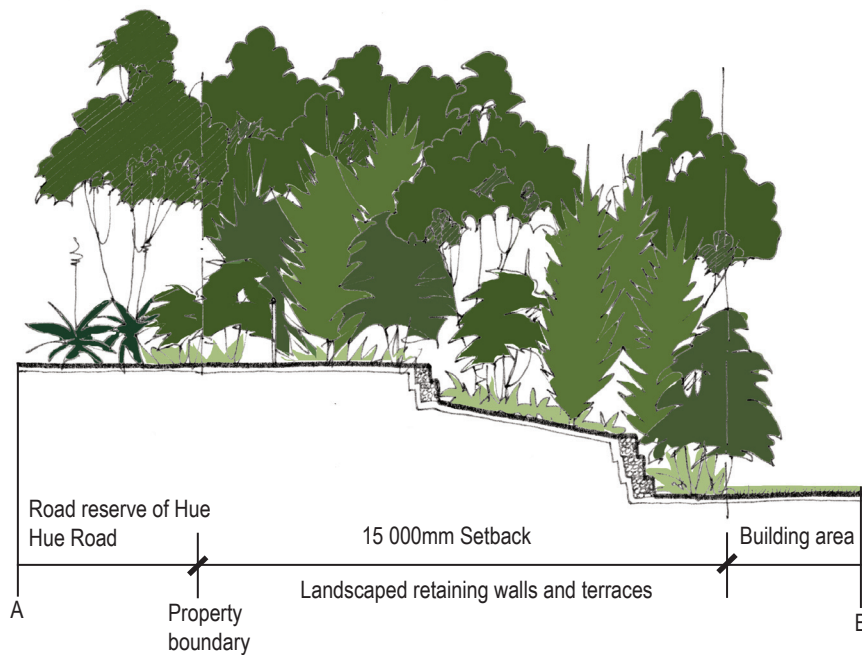


Figure L21 - SECTION 11: Section through back of lots (No 66) showing stormwater storage area

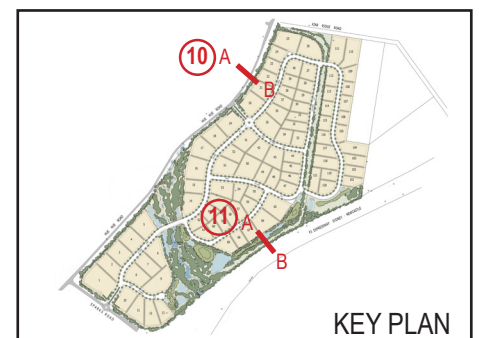
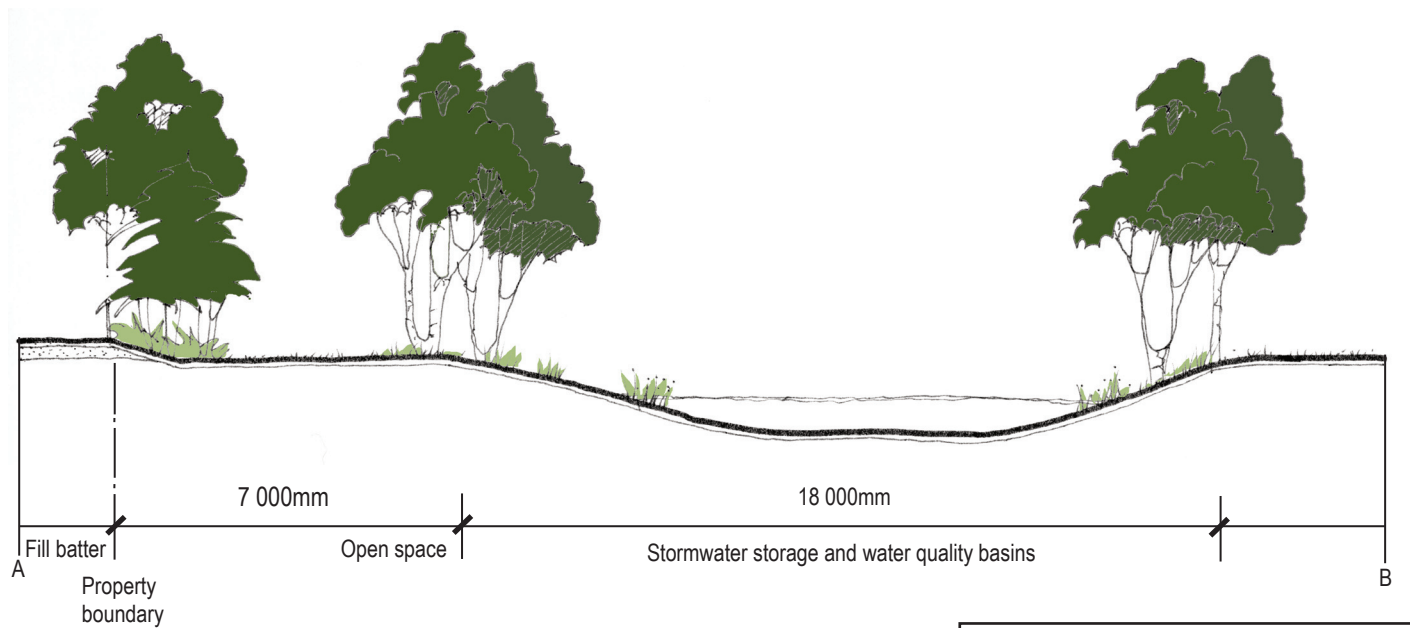


Figure L22 - SECTION 12: Typical 5 metre landscape side setback

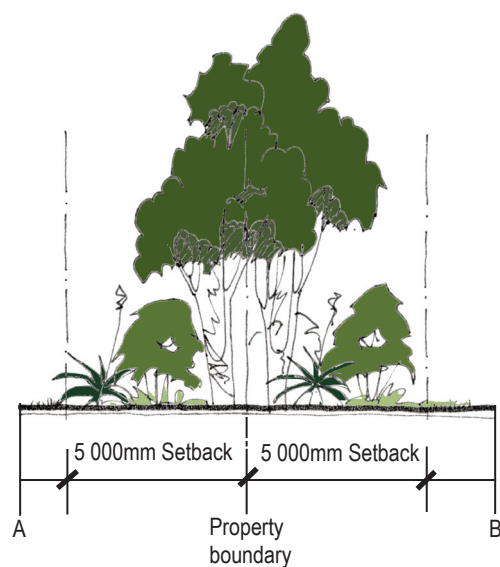
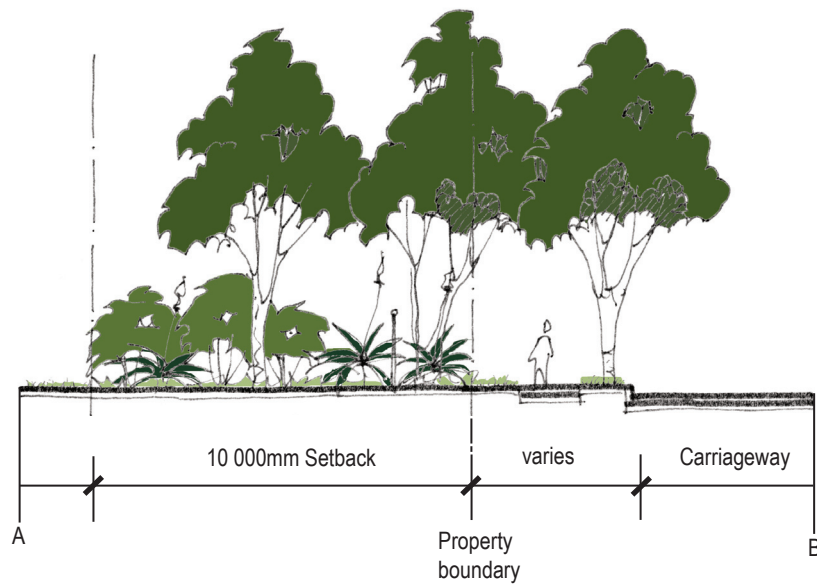


Figure L23 - SECTION 13: Typical 10 metre landscape front setback



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