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Montefiore Aged Care Facility - 36 Dangar Street, Randwick Part 3A Application

STATEMENT OF HERITAGE IMPACT

Prepared for: Montefiore Jewish Home

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Introduction

This Statement of Heritage Impact (SOHI) provides an analysis of a proposal for additions to the existing development at the Montefiore Jewish Home, Randwick. The current proposal is for a new addition to the southern portion of the site, adjoining the existing development and is subject to a Part 3A Application through NSW Department of Planning.

The subject site is adjacent to the former Boiler House Chimney Stack associated with the former Randwick Tramways Workshop and the site is located south of the North Randwick Heritage Conservation Area.

This report was prepared by Hector Abrahams and Kate Denny of this office and follows generally the methodology recommended by the NSW Heritage Office in *Statement of Heritage Impact* (Revised 2002). The site was inspected on a number of occasions in 2009.

Clive Lucas, Stapleton & Partners previously prepared a *Statement of Heritage Principles and Impact* (dated 26th November 2001) for the preparation of the original Master Plan for the property.

History of the Site

The Montefiore site is bounded by King Street to the south, Dangar Street to the west, Govett Lane to the north and a multi unit development fronting King Street to the west. The site was formally part of the Randwick Tramways Workshop.

The Randwick Tramways Workshop dates from circa 1881 and was built on land excised from Centennial Park and was formerly part of Lachlan's Swamp. The area to the north of the tramways site (now residential development) was further subdivided in the early 1900s to fund works to Centennial Park.

The Randwick Tramways Workshop developed into the largest and most comprehensive of Sydney's tramway workshops. The workshop continued on this site until its closure in the early 1960s, when the workshop site underwent a reduction in site area and loss of original buildings. The western portion of the site was handed over to the University of New South Wales and the eastern portion became a State Transit Authority bus depot.

The Montefiore site occupies approximately one third of the original workshop site. From the workshop period the place retains some topography and archaeology. The former Boiler House

Chimney Stack is located adjacent to the subject property, fronting King Street, directly in front of the multi-unit development at 88-98 King Street, Randwick.

Refer to Figure 1 for a location plan and Figures 2 to 6 for historic plans of the site.

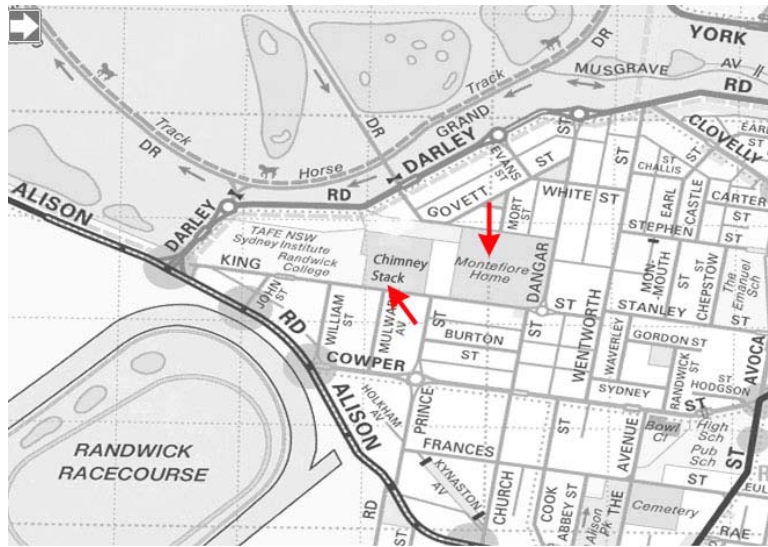


Figure 1: Location Plan showing the location of the subject site and the former Boiler House Chimney Stack.
(Source: Street Directory website < <http://www.street-directory.com.au> >)



Figure 2: Plan of the Botany and Lachlan Watersheds, signed 10th June 1875. The former Randwick Tram and Bus Depot site is circled in red.
(Source: Mitchell Library, M2 811.186ghhd/1875?/1)

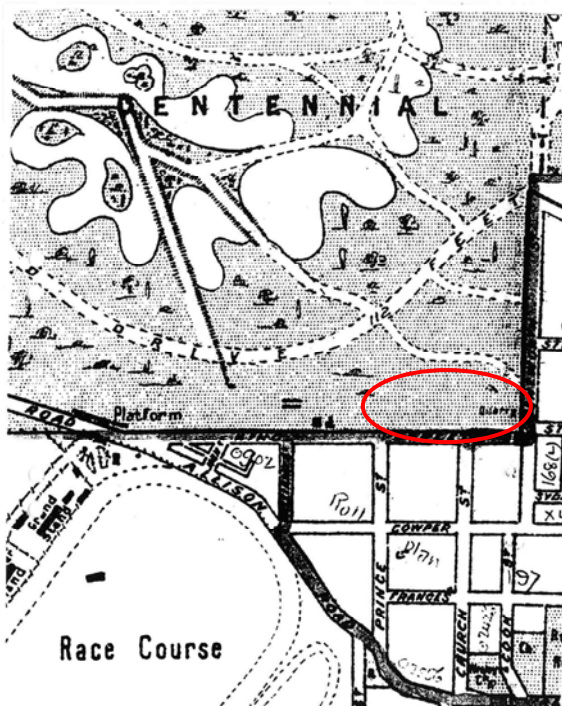


Figure 3: Detail of Randwick Subdivision Plan, undated. The former Randwick Tram and Bus Depot site is circled in red.
(Source: Mitchell Library, Randwick SP-R2/168)

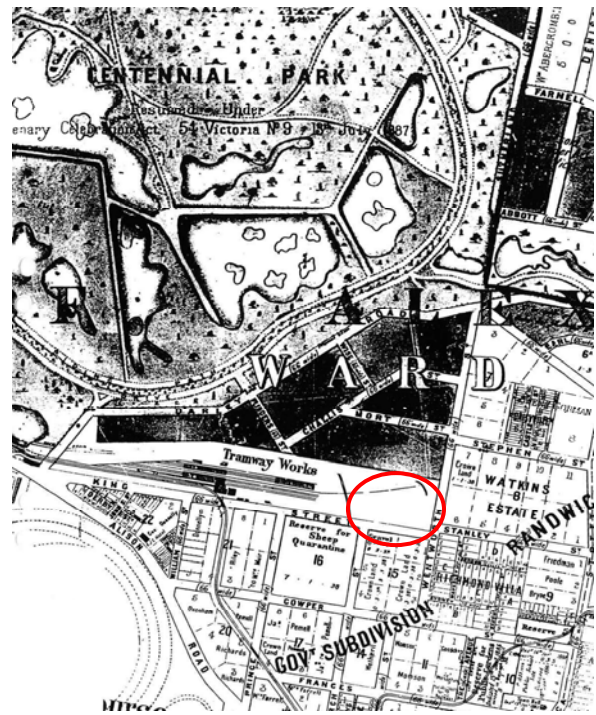


Figure 4: Detail of Higginbotham and Robinson Municipality of Randwick, parishes of Alexandria and Botany, 1892. The former Randwick Tram and Bus Depot site is circled in red.
(Source: Mitchell Library, M4 811.181/1892/1)

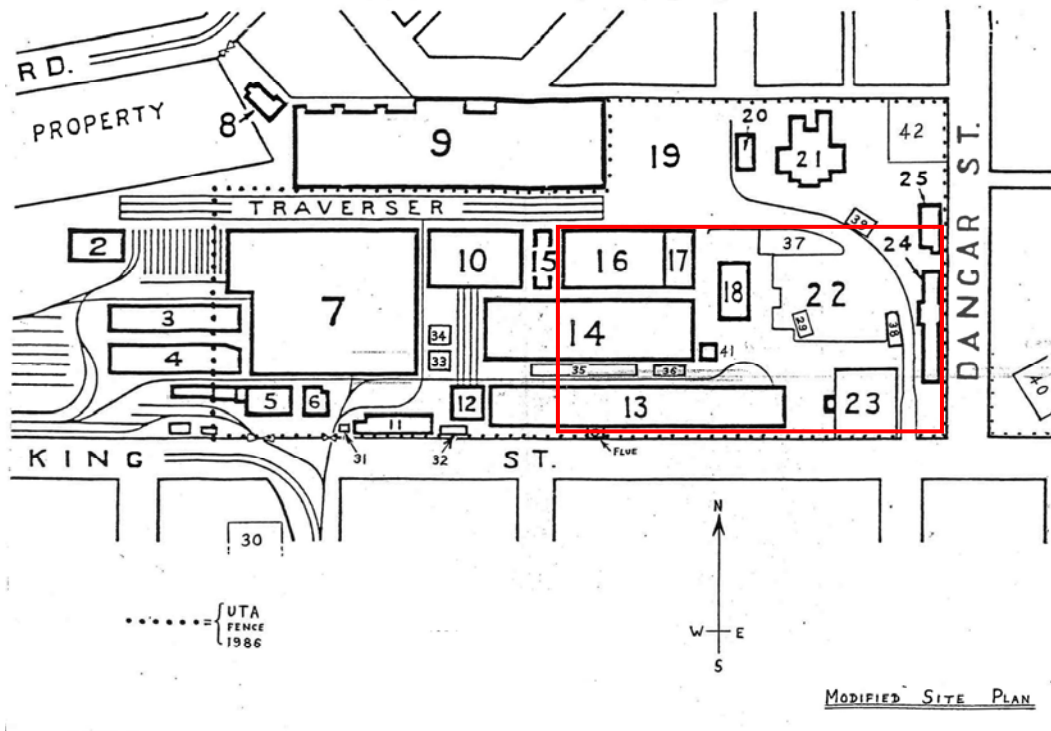


Figure 5: Site Plan circa 1896, showing a record of the former Randwick Bus Depot buildings. The subject area for the new development is outlined in red. The remainder of the site has been redeveloped as part of the Master Plan in c.2001. Building 13 is identified as the Blacksmith, Boiler and Welding Shop.
(Source: Reproduced from Carol Doring report, originally from Department of Planning Library)

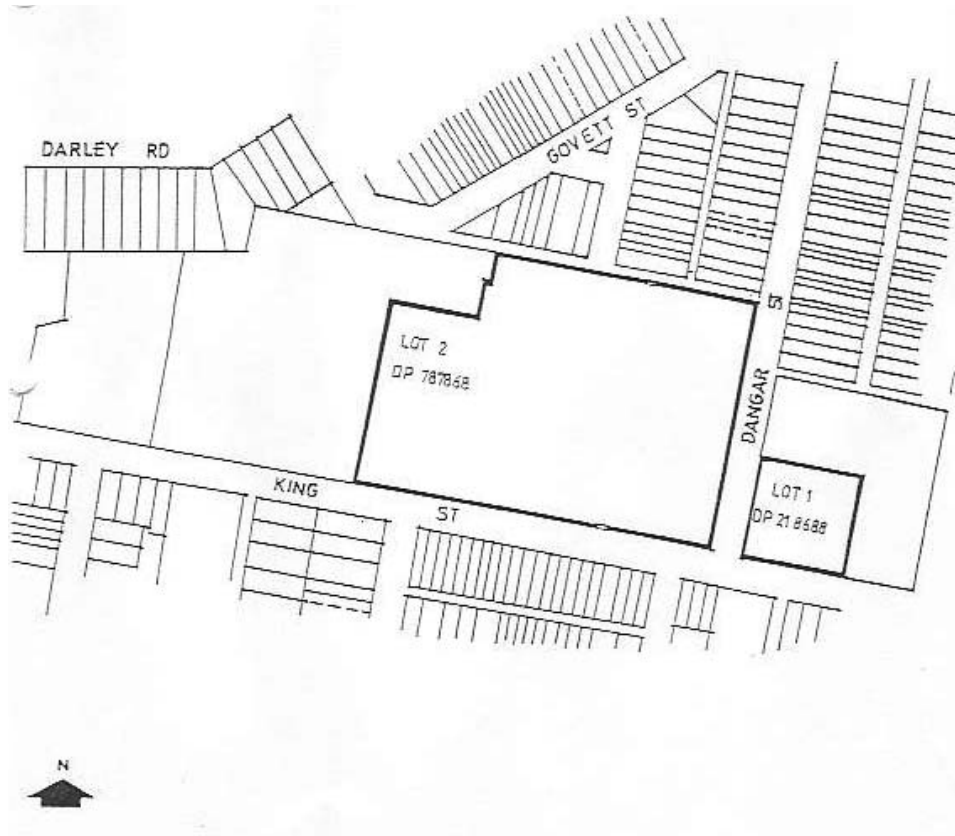


Figure 6: Plan of the subject site.
(Source: Randwick Development Control Plan no. 18, 1995)

Heritage Status

The former Boiler House Chimney Stack associated with the Randwick Tramway Workshop is listed as a heritage item on the Randwick Local Environment Plan (LEP) 1988 (Schedule 3).

The Montefiore site is located to the south of the North Randwick Heritage Conservation Area, as Govett Lane forms part of the southern boundary of the conservation area. The boundary of the conservation area extends down Dangar Street to Tramway Lane, opposite the north-eastern corner of the subject site (see Figure 7).

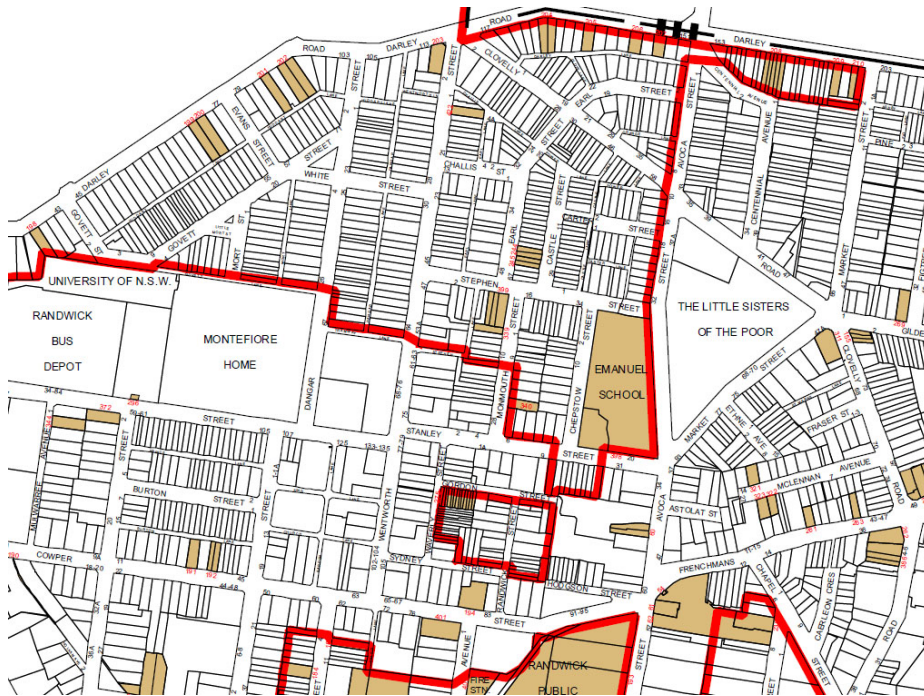


Figure 7: Detail of North Randwick Conservation Area showing location of the southern boundary of the Conservation Area and its relationship to the subject property.

(Source: Heritage Map/Randwick Local Environmental Plan 1998 (Consolidated)

http://www.randwick.nsw.gov.au/Places_for_people/Building_and_development/Planning_strategies_and_controls/Local_Environment_Plan/index.aspx)

Site Development Guidelines

The heritage principles for the site as set out in the *Statement of Heritage Principles and Impact* (dated 26th November 2001) are as follows:

The former brick chimney stack and base should be preserved and interpreted as one of the major industrial chimney stack surviving in NSW, a local landmark and the most substantial remains of the former tram Depot.

Whilst the major part of the tram depot has been demolished, the opportunity remains for the new development on the site to have regard to the past history of the site and its major use as the principle tram depot of the Sydney system. The chimney should not be obscured.

The Proposal

Documents Describing the Proposal

The proposed works are described in the following drawings prepared by Jackson Teece Chesterman Willis Pty Ltd:

- *Montefiore Aged Care Facility Part 3A Concept Plan, dated July 2010*
- *Project No. 2008116 Drawing nos. DA120- DA127 Issue A, dated 7 July 2010*
- *Project No. 2008116 Drawing nos. DA310-311 Issue A and DA350-351 Issue A, dated 7 July 2010*
- *Project No. 2008116 Drawing nos. DA412 Issue A and DA413 Issue A, dated 7 July 2010*

- *Project No. 2008116 Drawing nos. 00010 Issue 3, 000100 to 000105 Issue 3, 000200 and 000201 Issue 3 and 000300 Issue 3, dated 29 November 2010*

Summary of the Proposal

In brief, the proposal is a staged development to extend the existing facilities on site to accommodate a range of aged care, health and support facilities for the Montefiore Jewish Home.

The current proposal involves the development of new buildings to the southern portion of the site fronting King Street and accessed via King Street and Dangar Street. The main additional buildings (Buildings D and E) consist of a large complex intersected by the existing Building C with two internal courtyards adjoining the existing development to the north. The development of Building D and the alterations and additions to Building C forms Stage 1 of the development.

An additional building (Building E) is proposed to the west, fronting King Street and adjoining Building C with a driveway tunnel between the buildings at ground level to maintain access from King Street.

All new buildings are six storeys (four to five storeys above ground), including basement level carparking. Building E maintains the same setback from King Street as the existing Building C except for the western corner of the site, which is setback from King Street in order to provide additional car parking spaces for the child care centre. In addition, the upper five levels to Building E are substantially set back from the western boundary. Building D is setback from the main building line in order to accommodate a new public space to the corner of King and Dangar Streets. Landscaping works are proposed to the King Street and Dangar Street frontages.

The proposed buildings are to contain the following facilities:

- | | |
|--|---------------------------------|
| • Dementia high and low care accommodation | • Retail spaces |
| • Health and supporting services | • Recreational areas |
| • Residential aged care facility | • Religious facilities |
| • Childcare centre | • Car parking |
| • Administration | • Plant room/store and services |

Assessment Methodology

An appropriate assessment methodology is to consider the details of the proposal and to compare it with a properly prepared Statement of Significance in order to determine whether the proposal detracts from or diminishes the significance of the place.

In this case, the heritage listing does not contain a Statement of Significance for the site. As such, the Site Development Guidelines above will be used for the heritage assessment.

Heritage Assessment

Chimney Stack

The proposed Building D has been set back to a minimum of 19.5m from the site boundary of King Street and a total of 10 metres from the Dangar Street boundary. Building E maintains a set back of 10m from the site boundary of King Street, except for the western portion of the building, which is setback approximately 15m from the King Street boundary. The proposed buildings are either in line with or setback from the existing building line of Building C.

The development retains the significant views of the chimney stack from King Street. Partial views of the top of the chimney which can be seen from Dangar Street will, however, most likely be obscured by the new buildings. The Dangar Street views are predominantly precinct views, and are one of many afforded from the immediate locality. This is a minor view and will have a minimal impact on the heritage significance of the chimney.

The landmark quality of the chimney stack will be retained. See Figures 8 and 9 below.



Figure 8: View of the chimney stack from corner of King and Dangar Streets. This is retained.



Figure 9: Partial view of the chimney stack from Dangar Street. This is not retained.

The Randwick Bus Depot

The proposed development involves minor excavation to the eastern portion of the site. This will impact the topography of the site from the Randwick Tramways Workshop and bus depot period.

The topography of the site provides little information as to the former use of the site and in regards to heritage matters, is of little significance. This proposal will have no heritage impact.

Surviving archaeology of the former Randwick Tramways Workshop may be recovered during the development works. If any subsurface deposit is revealed during excavation works, an historical archaeologist should be consulted. See the suggested conditions of approval at the conclusion of this report.

North Randwick Heritage Conservation Area

The proposed buildings are located on the southern boundary of the Montefiore site. The northern part of the Montefiore site is adjacent to the North Randwick Heritage Conservation Area, as Govett Lane forms part of the southern boundary of the identified conservation area and the northern end of Dangar Street also forms a boundary of the identified conservation area.

The new buildings will be further away from the conservation area and significantly concealed by the existing four storey building and associated planting located between the new buildings and Govett Lane. As such, the new buildings will have a minimal visual impact on the heritage conservation area located to the north of the proposal (see Figures 12 and 13 below).

The new buildings will be visible from the portion of the conservation area located at the junction of Dangar Street and Tramway Lane. However, the new buildings as viewed from the street are no higher nor of greater bulk than the existing buildings located to the north of the site and the substantial setbacks proposed together with landscaping will reduce any visual impacts.

In addition, as the proposed new buildings are setback from Dangar Street and a large open plaza is proposed to the south-eastern corner of the site at the junction of Dangar and King Streets, view lines

to the conservation area from King Street along Dangar Street will be retained (see Figures 10 and 11 below).



Figure 10: View from Dangar Street close to junction of Tramway Street towards site.



Figure 11: View from junction of King and Dangar Street looking north towards conservation area.

The Immediate Context

In regards to height, mass and scale, the new buildings are a substantial addition to the surrounding streetscapes. The height of the buildings and their alignments will be equal to, and where set back, higher than the adjacent development. Modern flat buildings in King and Dangar Streets have set a precedent for this scale of development in the locality, as well as the existing 4 storey building on the Montefiore site (see Figures 14, 15 and 16 below). The height and scale of the new buildings is the same as the surrounding neighbouring buildings.

In our opinion, the proposal will not result in any loss of significance to the heritage conservation area.



Figure 12: View to the site of proposed buildings on the horizon from corner of Dangar Street and Govett Lane.



Figure 13: View of Dangar Street south from the intersection with Govett Lane. The new building will only be visible on the horizon.



Figure 14: Modern flat building, 113-123 King Street. This is the immediate context of the development to the south-east.



Figure 15: Neighbouring modern flat buildings on the eastern side of Dangar Street. This is the immediate context of the development to the east.



Figure 16: Modern Flat Building 88-98 King Street. This is the immediate context of the development to the west.

Discussion

The proposal is located adjacent to the southern boundary of the North Randwick Heritage Conservation Area, and the new buildings will have minimal impact on the heritage conservation area. The majority of the development will be concealed from view by the existing buildings and vegetation located in the street. The siting of the proposal, together with the proposed open public space will ensure that views to and from the conservation area to the north and at the junction of Dangar Street and Tramway Lane will be substantially retained.

In regards to the proposal being a sympathetic transition, the new buildings are setback from the street frontage along King Street which has provided an opportunity to develop a new open public space with seating and landscaping and to provide additional carparking on site. Building D will retain the existing setbacks along Dangar Street and landscaping is proposed, which will maintain a domestic scale frontage and residential appearance to the streetscape. The height of the buildings respond to the neighbouring buildings in the streetscape, and the mass and scale of the proposal is in keeping with the existing development on the Montefiore site.

The treatment of the building facades will be of natural materials, textures and tones. This is a similar treatment to that used on the existing Montefiore building. It is also representative of the types of building materials used in other developments in the immediate vicinity.

Conclusion

The proposed buildings will have only minor impacts on the heritage values of the place. The buildings are set back from the site boundaries, built to a similar height as surrounding development and the site is landscaped to be in keeping with the character of the locality.

The streetscape value of the North Randwick Heritage Conservation Area in Dangar Street will not be compromised.

Primary views of the former Boiler House Chimney Stack will be preserved. Views of the top of the former Boiler House Chimney Stack from Dangar Street will be lost.

Consequently, in the view of this firm the proposal has a minimal impact on the significance of the site and the adjacent heritage conservation area, and **should be** approved by the consent authority in regard to heritage matters.

Suggested Conditions of Approval

In spite of the above some aspects of the proposal need further clarification, and the following condition of approval is recommended should the consent authority decide to approve this proposal:

- If a subsurface deposit is revealed during construction works, an historical archaeologist should be consulted.



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